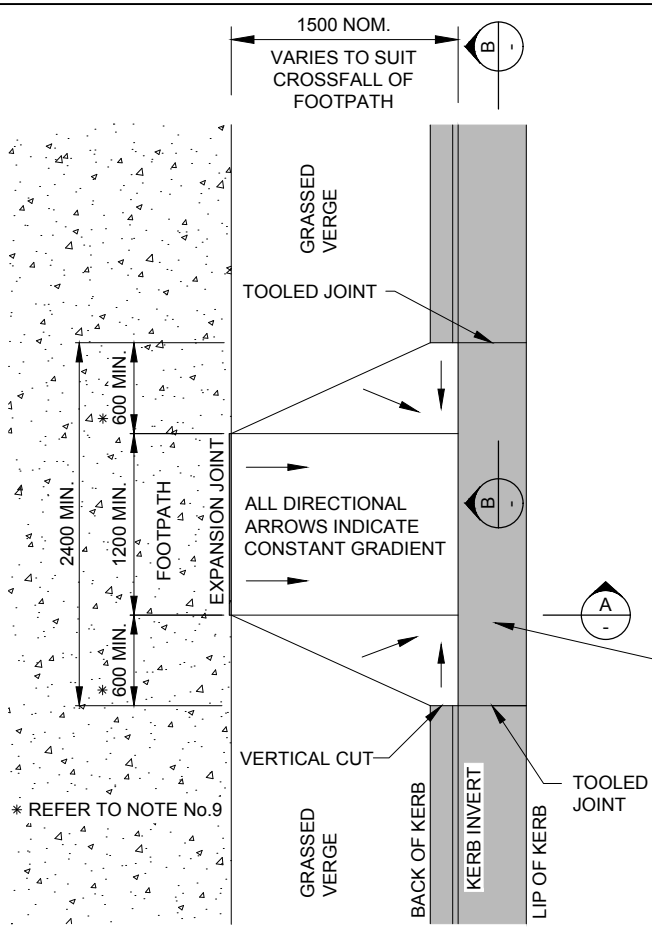
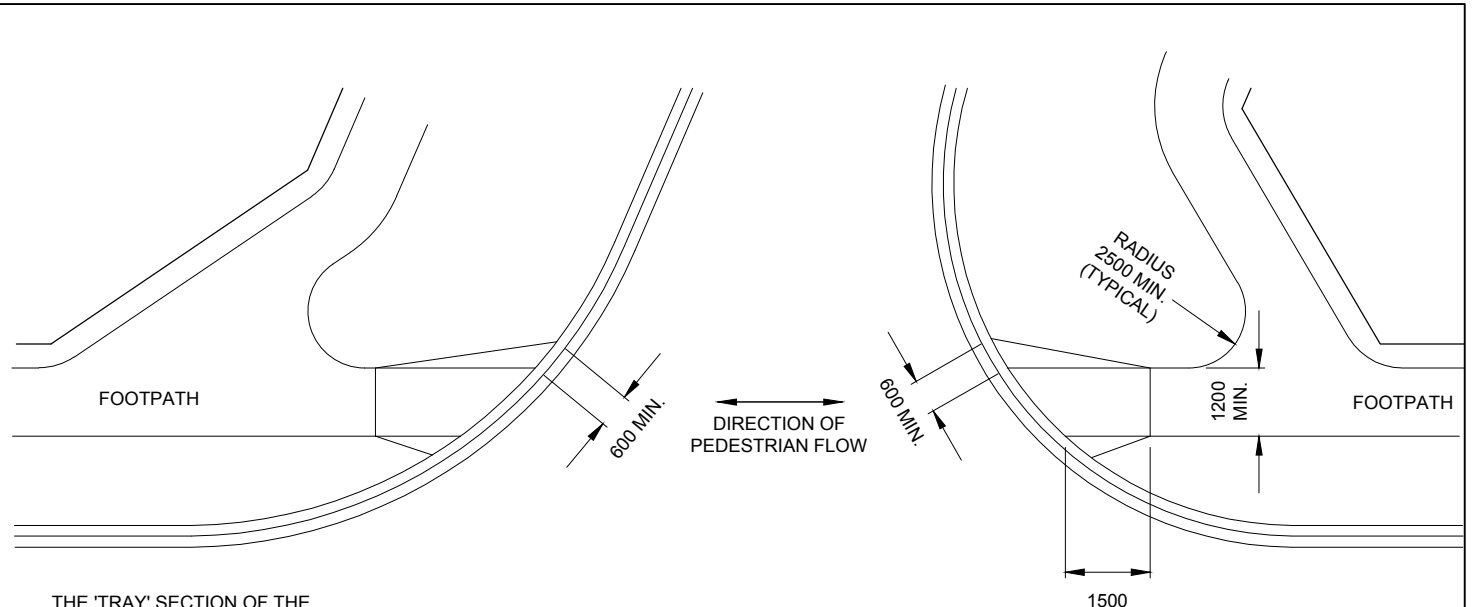


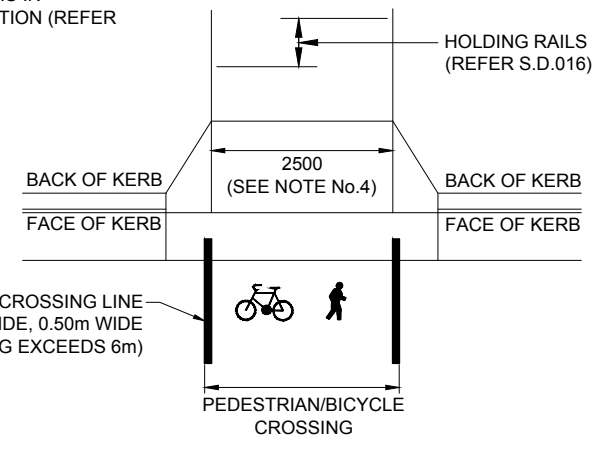
**PLAN 'A'**  
**STANDARD 3600 KERB RAMP**  
**DETAIL**  
NOT TO SCALE



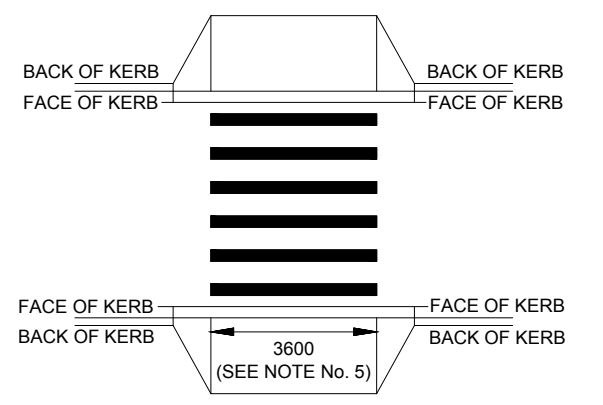
**PLAN 'A'**  
**STANDARD 2400 KERB RAMP**  
**DETAIL**  
NOT TO SCALE



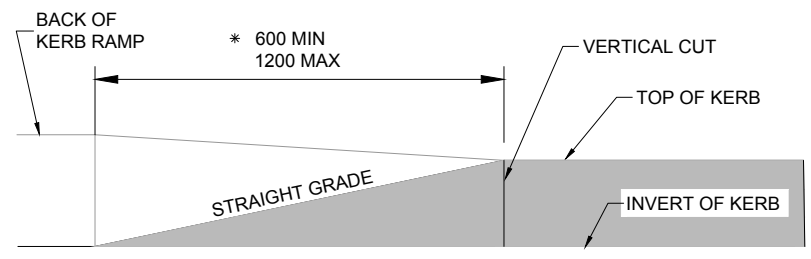
**OBTUSE & ACUTE SITUATIONS**  
NOT TO SCALE



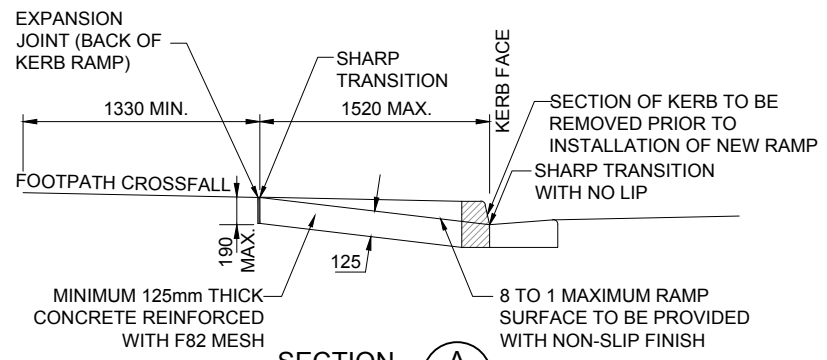
**PLAN 'B'**  
**MARKED FOOT CROSSING (SHARED**  
**PEDESTRIAN / BICYCLE CROSSING)**  
NOT TO SCALE



**PLAN 'C'**  
**AT PEDESTRIAN CROSSING (ZEBRA)**  
NOT TO SCALE  
(REFER TO NOTE No.5)



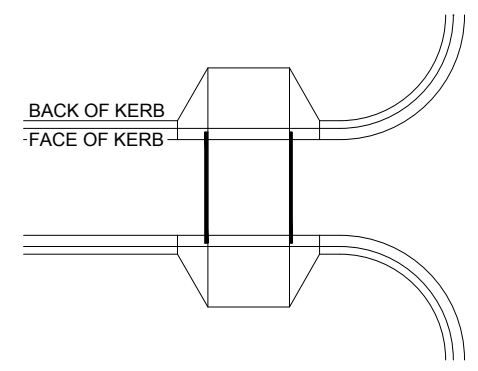
**ELEVATION B**  
NOT TO SCALE



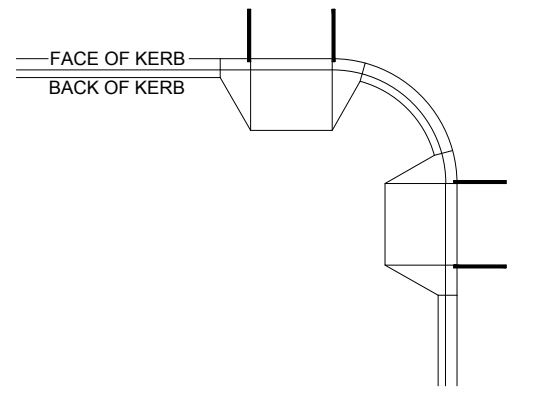
**SECTION A**  
NOT TO SCALE

**NOTES:**

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL KERB RAMPS ARE TO BE ALIGNED WITH THE DESIRED DIRECTION OF PEDESTRIAN FLOW AND BE A MINIMUM OF 1200 WIDE.
3. AT PEDESTRIAN CROSSINGS AND MARKED FOOT CROSSINGS RAMPS MUST LINE UP WITH CROSSING AND CORRESPONDING CROSSING ON OTHER SIDE OF ROAD.
4. FOR A MARKED FOOT CROSSING (SHARED PEDESTRIAN/BICYCLE CROSSING) SLOPING FACE OF RAMP SHOULD BE AS WIDE AS THE INSIDE OF THE PAINT LINES AT A PRACTICAL WIDTH NO LESS THAN 2500. CYCLE DEFLECTION BARRIERS ARE TO BE INSTALLED AS PER S.D.016.
5. AT PEDESTRIAN CROSSINGS (ZEBRA) THE SLOPING FACE OF THE RAMP SHOULD BE AS WIDE AS THE OUTSIDE EDGE OF THE ZEBRA STRIPES (3.6m WIDE). WHERE THIS IS NOT POSSIBLE, THE RAMP SHOULD BE AT A PRACTICAL WIDTH NO LESS THAN 1200 PLACED CENTRAL TO CROSSING (REFER TO PLAN C).
6. EXPANSION JOINT TO BE PROVIDED WHERE KERB RAMP ABUTS FOOTPATH.
7. SEPARATE KERB RAMPS MUST BE USED FOR ADJACENT CROSSINGS AT INTERSECTIONS (REFER TO PLAN E).
8. ALL WORKS TO CONFORM WITH AS1428 [ACCESS FOR DESIGN & MOBILITY (2009)].
9. WHERE THE FOOTPATH AREA IS CONCRETED FROM THE BACK OF THE KERB TO THE PROPERTY BOUNDARY & PEDESTRIAN MOVEMENTS ARE GENERALLY PARALLEL TO THE KERB THE MAXIMUM WING DIMENSION MUST APPLY.
10. EXPANSION JOINTS TO BE AN APPROVED PRE-FORMED EXPANSION KEY (e.g. CONNOLLY JOINT). PROVIDE AT ALL GREEN POUR JUNCTIONS AND JOINTS



**PLAN 'D'**  
**ALIGNED KERB RAMPS**  
NOT TO SCALE  
(REFER TO NOTE No.4)



**PLAN 'E'**  
**ADJACENT CROSSINGS**  
NOT TO SCALE  
(REFER TO NOTE No.7)

F	NOTES ADDED & RAMP WING DIMENSIONS ADDED	A.R.	10.2015
E	NOTES AMENDED & ADDED, SECTION 'A' AMENDED & SECTION 'B' DELETED	G.P.C.	04.2015
D	PLAN FORM UPDATED, DIMENSIONS & NOTES AMENDED	G.P.C.	07.2013
C	VARIOUS AMENDMENTS	N.R.	03.2007
	PREVIOUS AMENDMENTS NOT SHOWN		
ISSUE	AMENDMENT DETAILS	INITIALS	DATE



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DESIGN ENGINEER	<i>W. Boyd</i>	DATE 19.10.15
DESIGN MANAGER	<i>P. Hogg</i>	DATE 19.10.15
DRAWN	ENGINEERING & OPERATIONS DESIGN UNIT	
SCALE	NOT TO SCALE	

PROJECT:	ROAD WORKS STANDARDS
PLAN TITLE:	STANDARD KERB RAMP DETAILS

DRAWING NUMBER:	<b>S.D.014</b>
	<b>OCT 2015</b>