



**Kingscliff Locality Plan  
Development Control Plan**

## Amendments

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| 1.0     | –         | 03/08/2017 | Draft Public exhibition document | Council    |
| 2.0     | –         |            | Draft Public exhibition document | Council    |
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Prepared by:  
Strategic Planning & Urban Design Unit

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## PART 01 INTRODUCTION

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## 1.1 Vision for Kingscliff

*“Kingscliff is a vibrant north coast town celebrated for its active community life, beach, creek, natural environment and agricultural surrounds. The town centre will continue to be the commercial and community heart of the locality meeting the needs of the locals, surrounding smaller villages and tourists alike. Future development within the locality will strengthen the coastal character and achieve a balance between meeting increased housing needs, fostering healthy community life, providing for economic and employment growth whilst protecting the highly valued environmental areas and working agricultural hinterland.”*

### Creating a Sense of Place

The existing Kingscliff town centre will continue to serve as the primary retail, commercial, health, social and cultural hub for the settlement and broader sub-regional catchment. The distinctly low rise building scale and character will be retained along the Beach Front with additional public domain improvements to facilitate greater pedestrian priority, to define the heart of the town and improve pedestrian connectivity. There will be opportunities for the town centre to expand west along Turnock Street to accommodate future retail and commercial growth and diversity. All buildings will be designed to take advantage of the subtropical context to strengthen and enhance the Kingscliff coastal character set within a landscaped and greened streetscape.

The Kingscliff town centre will be supported by other retail and business opportunities across the locality including an additional smaller local centre within the Business and Knowledge Precinct expanded opportunities to meet the needs of both residents and tourists within the Salt precinct. Smaller businesses will also be encouraged within existing resident precincts with the intent of every resident being within a walkable or cycling proximity to day to day conveniences and open space.

Greenfield development sites adjoining the existing town centre present significant opportunities to expand the town centre business, retail and residential uses. Turnock Street is envisaged as a tree lined boulevard with its street edge activated with small scale retail and outdoor dining opportunities. Integrated basement car parking will liberate the ground plane from expansive hardstand areas and high quality landscaped pedestrian and public domain spaces will provide for pedestrian amenity and comfort. Shop top housing above these ground level retail and commercial uses will also provide additional housing variety within a walkable town centre catchment reducing car movements.



The development of key sites surrounding the town centre will provide additional housing diversity and supply which will meet demonstrated growing population need for the next 30 years. These new residential precincts will demonstrate a high standard of coastal architecture and urban design, and facilitate built form character which embraces the public domain and provide landmark developments for the Kingscliff community. The network of residential precincts will be supported by a public transport and passive movement network which connects existing and future residential areas to the town centre and key areas of open space. Planning for community facilities to support a growing population must consider shared or multi-purpose use, be located in easy to access locations, appropriately sized and universally designed to be activated and respond to future community needs.

### **Best Practice Design and Sustainability for New Development**

The overall concept for greenfield development sites within the Kingscliff Locality is to produce a high quality mix of residential development which offers a range of housing types, connected network of open space and community services. In step with population increases experienced across the Tweed Coast generally, there is also a need to significantly strengthen, business, knowledge and employment opportunities. Each of the existing and future residential precincts have their own distinctive characters where the built form responds and takes advantage of precinct specific topographic, climatic, visual and landscape characteristics.

Within new greenfield development sites, street layout and allotment configuration will incorporate best practice subdivision design, embedding passive design principles to optimise solar access, capture prevailing breezes and take advantage of views and view lines where available. Increasing residential densities around the activity centres will increase housing choice to meet a range of affordability and lifestyle demands within highly walkable and well connected catchments. By increasing density and diversity of housing types in well located areas will reduce the pressure on developing the agricultural and environmental protection land edges of the settlement.

### **Health and Wellbeing**

The open space and recreation strategy for Kingscliff is to provide a diverse range of quality open spaces to meet the active and passive recreational needs of the existing and future population. These areas of open space also provide a greening edge to the built environment which contributes significantly to the existing visual character of the Kingscliff settlement. Ready access to these areas of open space provide opportunities for health and recreation pursuits essential to general health and well being.

The relatively flat topography of Kingscliff provides opportunities for excellence in design of recreational space and allows for defined active and passive open space to be effectively linked together with grade accessible walking and cycle paths. Each of the residential precincts will be connected to the broader network of well-designed public open spaces. Securing strong north-south and east west passive movement connections within the subdivision and master planning processes will ensure high levels of universal access to the town centre, places of employment, community facilities, open space and other public spaces throughout the settlement.

# Vision



## Kingscliff locality vision

The future of Kingscliff will achieve a balance of meeting increased housing needs, fostering healthy community life, providing for economic and employment growth whilst protecting the highly valued environmental areas and working agricultural hinterland

## Environment and heritage

Facilitate the protection of aboriginal and non-aboriginal cultural heritage and management of land identified as environmentally and/or ecologically important through appropriate land use zoning and provisions for ongoing fauna, habitat and heritage management.

## Town centre

Provide opportunities for the growth of the town centre core as a sub-regional retail centre fulfilling a diverse range of retail, commercial, cultural, recreation, social, health and accommodation uses with a focus on improving the connectivity, streetscape and pedestrian amenity whilst effectively managing traffic and car parking.

## Economy and employment

Expand employment generating land uses by providing land use opportunity for larger employment generating developments such as a business park, health and/or university campus, commercial and retail uses, as well as a range of student, tourist and residential accommodation types to build upon the existing industry pillars of tourism, agriculture, health and local small business.

## Housing

Provide a planning framework which will deliver a diverse range of housing types to appeal to a broad demographic and housing affordability profile with a strong focus on subtropical subdivision and housing design, connectivity between residential areas with tree lined streets and well located and embellished open spaces and public domain.

## Open space and community

Build upon Kingscliff's diverse network of active and passive open space areas including sportsfields and courts, parks, civic open space, bushland, coastal foreshore areas, cycle and walking paths with a strong focus on achieving a greater level of connectivity and user amenity. Integrate new community facilities to meet future community needs and services within the town centre.

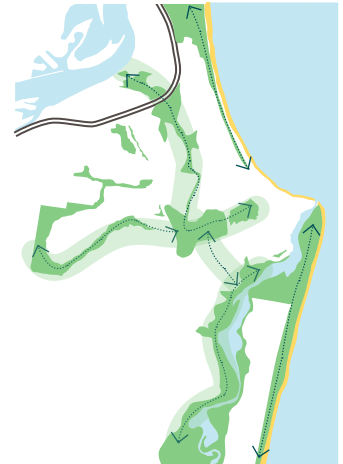
Figure 1.1 Key Vision Statements and Strategies

# Strategies

|  |   |
|--|---|
| Protecting the environment through land use planning.                                      | Facilitate employment and economic growth opportunities.                                |
| Meeting housing and community needs for a population of approx 12,000 by 2036.             | Implement design and planning guidelines for existing and future development precincts. |
| Enhance and strengthen coastal character through architecture, design and public domain.   | Ensuring adequate provision of civil and social infrastructure.                         |
| Zone lands of environmental and ecological significance.                                   | Manage aboriginal and non-aboriginal cultural heritage.                                 |
| On going enhancement and management of environmental protection areas.                     | Ongoing flora and fauna management.   |
| Ongoing coastal and estuary management.  | Enhanced Koala habitat.   |
| Retain low scale character along Marine Parade.  | Increase density and housing diversity in and around the town centre.                   |
| Facilitate town centre expansion along Turnock Street.                                     | Improve pedestrian connectivity and public domain.                                      |
| Architecture and design guidelines to reflect coastal character.                           | Improve car parking and traffic management.   |
| Facilitate development of a new business and knowledge precinct.                           | Promote and support agriculture including value add industries.                         |
| Promote and support tourism.   | Facilitate additional business development and light industrial land.                   |
| Additional smaller retail centre within business and knowledge precinct.                   | Support and facilitate local cultural, sports, market events.                           |
| 2500 additional new dwellings which could accommodate an additional 4000 people.           | Mix of dwelling types and lot sizes to meet different demographic need.                 |
| Additional low rise medium density housing types.  | Focus on subtropical subdivision and house design.                                      |
| Additional shop top housing within and surrounding the town centre.                        | Improved connectivity and access to and between open space areas.                       |
| Further embellish Kingscliff Sports and Recreation Complex.                                | Opportunity to expand community facilities within Kingscliff town centre location.      |
| New active and passive open space including a district park and family and youth precinct. | North south and east west passive movement corridors.                                   |
| Continue to embellish coastal foreshore areas and beach access.                            | Embellish existing parks and the provide new parks in future greenfield development.    |

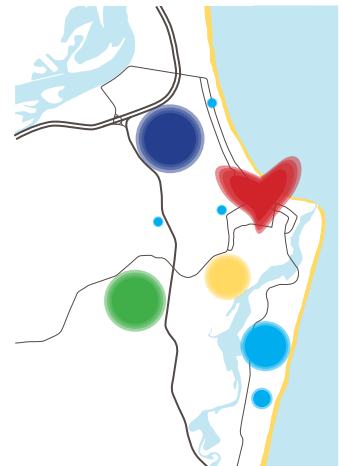
### 1 Protected ecological areas

Protect and enhance areas of environmental significance and strengthen connections between them. Natural areas frame and contain areas of urban settlement with connected open space embraced as key community spaces for recreation, connection and enjoyment; the soft green community space behind the beach.



### 2 Promote economic development

Facilitate the growth and development of economic and employment opportunities through land use planning including expansion of the existing Town Centre, the development of a future Business and Knowledge Precinct, expanded industrial areas and providing opportunities to grow the retail, health, education, tourism and agricultural industries.



### 3 Connected residential precincts

Create 'village scaled' precincts which are discrete with their own identity and character, framed by natural areas and interconnected by a necklace of green spaces for environmental quality, recreation, walking and cycling.

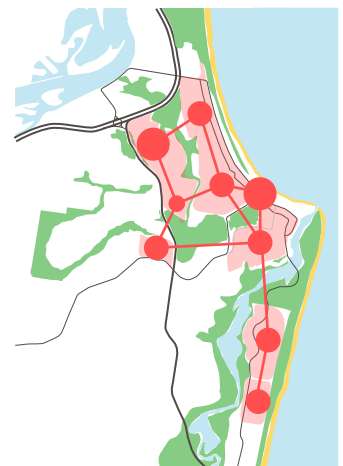
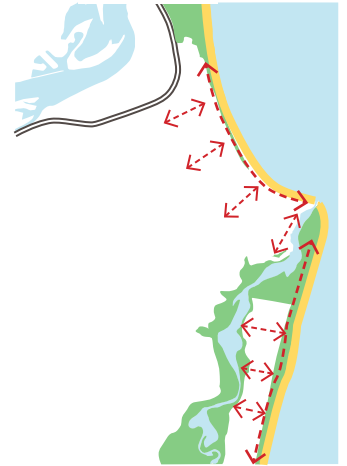


Figure 1.2 Key Planning and Design Principles

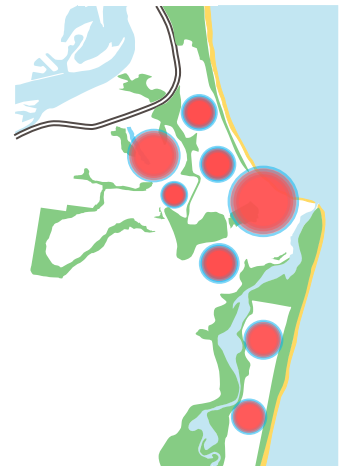
#### 4 Embrace the coastal edge

Strengthen and enhance connections and access to the coastal and creek edge which can be enjoyed by all. New development and public domain network of parks and pathways will provide and enhance experience, amenity and universal access connections to this edge.



#### 5 Smarter density

Provide opportunities for an increased housing density and diversity supporting a broad demographic profile and housing affordability needs. Density around the town centre will facilitate the provision of additional green spaces, improving resident amenity and enhance connected areas of environmental protection.



#### 6 Improved connectivity and permeability

Leverage new development to create important north-south and east-west road, pedestrian, bike and public transport links. Enhance landscape and ecological links linking each of the precincts within the centre, the coastal edge, open space areas and broader settlement.



## 1.2 Purpose of the Kingscliff Locality Plan & Development Control Plan

The purpose of the Kingscliff Locality Plan and DCP (the 'Plan') is to provide a 30 year vision and planning framework to guide the future growth and expansion of the Kingscliff locality. The stated visions and strategies within the plan aim to preserve local character and natural environment, facilitate business and employment opportunity, encourage housing supply and diversity and recommend measures to improve public domain to create a more contemporary place to live, work and play. The Kingscliff locality Plan and development control plan consists of two interrelated documents including:

- **Kingscliff Locality Plan** - Provides a locality wide context and precinct specific strategies which defines the future planning vision and direction for Kingscliff.
- **Kingscliff Development Control Plan** - Provides design principles, objectives and development controls to guide the preparation and determination of development applications.



Kingscliff Locality Plan and Development Control Plan Structure

## 1.3 Aim of this Section of the DCP

The aim of this **Development Control Plan** (the 'DCP'), is to guide planning and design development within the Kingscliff Locality through the application of planning and design principles, objectives and development controls.

This Section of the Tweed Development Control Plan has been prepared in accordance with Part 3 of the *Environmental Planning and Assessment Act 1979* ('the Act') and Part 3 of the Environmental Planning and Assessment Regulation 2000.

## 1.4 Land to which this DCP applies

This Section of the Tweed DCP applies to development on land within the area illustrated in Figure 1.3 of this DCP.

The DCP includes lands bounded by the Pacific Highway to the north extending to the locality boundary with Fingal Head, the Tweed Coast Road to the west and south to the locality boundary with Casuarina. The study area also includes Cudgen Village given the proximity and strong historic relationship with Kingscliff the need to more holistically consider the broader locality context, particularly in relation to land use, desired future character and traffic management considerations. The study area includes part of Chinderah (south of the Pacific Highway) as it marks the interface between the existing light industrial estate and those lands to the south.

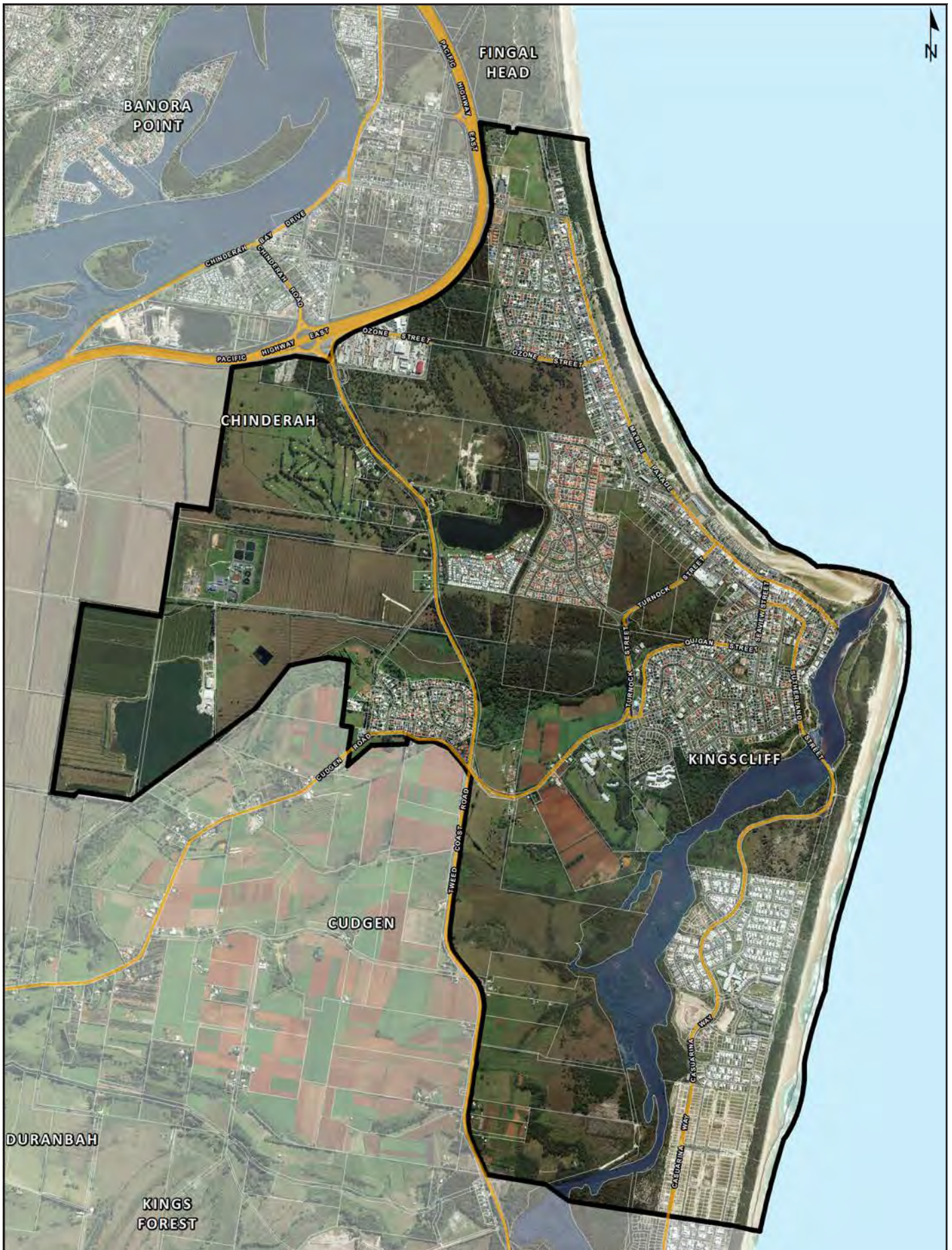


Figure 1.3 Locality Plan and DCP Study Area

## 1.5 Objectives of this DCP

The objectives of this DCP are to:

1. Provide a strategic planning and development control framework for the Kingscliff locality that details land use strategies, development and design principles and development controls relating to existing precincts as well as greenfield development sites.
2. Ensure protection, enhancement and ongoing management of natural bushland areas, waterways and land of high ecological value.
3. Facilitate increased opportunity for employment generating land uses.
4. Facilitate increased opportunity for housing diversity to meet a broad demographic and socio-economic profile including an increase of density around centres.
5. Provide quality open space and public domain areas that meet the needs of the local and regional community.
6. Co-ordinate and facilitate infrastructure provision including community and service infrastructure to ensure efficient use of the land and efficient infrastructure supply and provision.

## 1.6 Kingscliff Development Control Plan Structure

This Kingscliff Development Control Plan comprises 4 sections:

1. **Introduction** – Identifies the purpose, the aims, the area of application and the objectives of this DCP.
2. **Master planning and Subdivision** – Identified subdivision and master planning provisions as they relate to the broader locality and provides specific principle considerations for the structure and master planning of identified key greenfield development sites.
3. **Town Centre** – Details design principles and controls for the each of the defined precincts. This part specifically covers Place making and Urban Design, Built Form Design, Community Infrastructure, Commercial & Retail Facilities and Fostering Walkability & Integrated Movement.
4. **Residential Precincts** – Details actions and implementation strategies.

Within each of the DCP sections each head of consideration is structured as follows:

**Preamble** – Provides an explanation of the issue and the need for specific objectives and controls.

**Objectives** – State desired outcomes to be achieved by development.

**Controls** – Prescribe the requirements for achieving outcomes and the desired future character identified for the site and its integration with adjoining land uses.

**LEP Implications** – Prescribe the required amendments to TLEP development standards and land use zoning to achieve desired strategic planning outcomes and will form the basis of future amendments to the TLEP.

**Additional advice** – Provides additional supporting information, further reference material and linkages to other relevant information.

**Diagrams and Illustrations** – Illustrations provided in this section are indicative only and are provided to illustrate certain provisions in this section.



## 1.7 How to use this Section of the Tweed DCP

In preparing an application for development, there are a number of specific steps that should be followed:

**Step 1:** Check the zoning of the site under the Tweed Local Environmental Plan to ensure that the proposed development is permissible and to determine what related provisions apply. Where a proposed development is inconsistent with the land use provisions of the Tweed LEP, refer to Step 6.

**Step 2:** Establish what other Sections of this DCP or Council Policies apply to the site.

**Step 3:** Familiarise yourself with the context of the Kingscliff locality and determine which precinct the site is located within.

**Step 4:** Understanding the 'Vision', 'Character Statements', 'Statement of Intent', 'Objectives' and be guided by 'Development Control' for the particular development form. Note: Where a proposed development is consistent with the Strategy for a particular precinct, but inconsistent with the land use provisions of Tweed LEP, refer to Step 6.

**Step 5:** Follow the applicable design guidelines and refer to other applicable Sections of the Tweed DCPs and related policies. It is these components that will be used by Council to assess any development proposal.

**Step 6:** Where a development proposal is consistent with the Vision, Objectives and Development Control for a particular development type or area, but is inconsistent with the provisions of Tweed LEP, the applicant will need to request a rezoning of the land in accordance with the provisions of the *Environmental Planning and Assessment Act, 1979*.

## 1.8 Variations to this Section of the Tweed DCP

Variations to the provisions within this Section may be considered where the variation meets the relevant objectives. Any application seeking a variation to a control must be in writing and:

1. Identify the specific control which is proposed to be varied, and the extent and reason of the proposed variation.
2. Identify how the proposed variation satisfies the objectives of that section and the aims of that part of the plan.
3. State the benefits to the design of the development which will result from the proposed variation.
4. Include, where applicable, detailed diagrams, plans, specialist studies/reports sections, and photomontages indicating the benefits of the proposed variation.
5. Identify the impact of the proposed variation on the proposed development, and surrounding properties and outline how adverse impacts on surrounding properties have been minimised.



## **PART 02 MASTER PLANNING & SUBDIVISION**

- 2.1 Urban structure**
- 2.2 Site and context analysis**
- 2.3 Environment**
- 2.4 Landscape character and views**
- 2.5 Land forming**
- 2.6 Road layout traffic and transport**
- 2.7 Open space**
- 2.8 Dwelling and allotment mix**
- 2.9 Urban design, streetscape and public domain**
- 2.10 Solar orientation and passive design**
- 2.11 Hazards and resilience**
- 2.12 Infrastructure**
- 2.13 Key Greenfield Site – Turnock Street Precinct**
- 2.14 Key Greenfield Site – West Kingscliff Precinct**
- 2.15 Key Greenfield Site – North Kingscliff Precinct**
- 2.16 Key Greenfield Site – Business and Knowledge Precinct**
- 2.17 Key Greenfield Site – Cudgen Village Precinct**



## 2.1 Urban Structure

### 2.1.1 Preamble

#### Statement of Intent

*Kingscliff's greenfield sites will deliver additional housing supply, allocating additional land area to enable the town centre to grow, initiating a new business and knowledge precinct to facilitate economic and employment opportunities and protect areas of environmental significance. Preservation of natural areas will frame and define the limits of urban growth and enhance ecological links throughout the settlement. Each of the existing and future precincts will have their own distinctive character derived from topographic, environmental and landscape characteristics informing land use and design led development outcomes. A network of pathways and ecological corridors will link each of the precincts with activity centres within a network of open space.*

#### Urban Footprint Methodology

The overriding future urban development direction is to consolidate and develop existing urban and developable areas to achieve projected land use and density targets rather than contributing to 'sprawl' at the settlements peri-agricultural and natural environment edges.

Based on a review of demographic trends and population projections and a review of development potential over identified key greenfield development sites, there is currently sufficient zoned greenfield development land available for development to meet a growing population for the next 30 years. Accordingly, development is only supported in the nominated urban footprint areas (Figure 2.2). Council will seek to apply appropriate environmental zoning over areas identified as ecological significance forming the conservation footprint (Figure 2.3).

Kingscliff has approximately 165ha of greenfield development land comprising a mixture of residential, agricultural, industrial, special uses and environmental protection land uses. It is noted that whilst a significant proportion of these key greenfield development sites have an R1 General Residential zoning, not all land zoned will be suitable for development in consideration of development constraints (Figure 2.1 Kingscliff Locality Constraints Overlay) including areas of ecological significance (Figure 2.6 Areas of Ecological Significance).

In defining the Kingscliff Urban Footprint (Figure 2.4), land identified as ecologically significant and potential offset planting areas have been removed from defined developable footprint in keeping with the overriding vision and guiding principles of the Kingscliff Locality Plan. There are however some instances where smaller and less connected areas ecological significant land have been nominated for potential clearing within the urban development footprint to enable a more efficient and orderly urban structure. These cleared areas however are require to be offset with new planting within the local area calculated using a contemporary biodiversity calculator. Candidate offset planting areas have been nominated within Figure 2.3 Conservation footprint.

The next highest order constraint is flooding where a significant proportion of greenfield development sites within Kingscliff are currently subject to the 100 year ARI flood inundation. For the purposes of defining an urban footprint, these flood affected lands have not been discounted in recognition of the ability to fill these sites above design flood levels subject to satisfactorily meeting the provisions of Tweed DCP A3 – Development of Flood Liable Land including addressing flood modelling criteria and flood studies as required.

It is critical to acknowledge that the diagrammed constraints within this Code (including Volume 01 Context and Locality Wide Strategies, Volume 02 Precinct Plans) are not exhaustive of the constraints that apply to the individual greenfield development sites. Additional constraints such as drainage, acid sulphate soils, soil stability, slope and bush fire prone land, for instance, have the capacity to influence the type, style and scale of development achievable. Further discussion on these matters is contained throughout this Code and should be the subject of more extensive developer/land owner led site investigations.

## Key Greenfield Development Sites

In order to guide new greenfield development sites across the locality, this Code contains design principles applicable to the greenfield development sites (Section 2.13-2.18 of this DCP). These site specific greenfield design principles align with key strategies in the Kingscliff Locality Plan. The prepared indicative structure plans illustrated within this DCP represent one potential designed outcome based on an understanding of the site constraints and application of desired urban design and urban structure planning principles. Given the need for more in depth site specific analysis and master planning design processes, these diagrams do not necessarily represent the required final designed outcome. The final designed outcome will be the outcome of a design led process in consultation between developer/land owner(s), Council and the broader community.

### 2.1.2 Objectives:

1. Achieve the orderly and efficient use of land and focus future urban development on land that is suitable for the development and capable of supporting proposed uses in accordance with strategies, objectives and intent of the North Coast Regional Plan, Tweed Local Environmental Plan and Kingscliff Locality Plan.
2. Strengthen the character and hierarchy of settlement by maintaining strong multi- functional business centre(s), capitalise on well located infill development opportunities, minimise urban sprawl at the localities edges and maximise infrastructure and service efficiencies.
3. Facilitate the protection and management of land identified as environmentally and/or ecologically important through appropriate land use zoning and provisions for ongoing habitat management.
4. Plan new subdivisions as connected communities framed within the hierarchy of settlement where consideration is given to the ultimate geographical extent and population target for each community that is to be formed or built upon, and the staging/timing by which it is proposed to reach that position.
5. Provide for a variety of dwelling types and choice which will meet the broad demographic and affordability needs of the future resident population.
6. Promote high quality design and environmental stewardship that integrates the Design Principles of this Code.

### 2.1.3 Development Controls:

- C1. Any planning proposal, master plan, concept development application and or subdivision application over land(s) within the identified urban footprint (Figure 2.2), including over land already zoned for settlement but not yet developed, must undertake a design led master planning process in consultation with Council prior to the submission of a planning proposal or development application. This process will consider the interrelationship with the existing settlement, all site constraints, greenfield site specific planning and design principles (see Section 2.13–2.18), all other relevant principles and controls within this Code as well as Council's other applicable development control and policy instruments.
- C2. This Code does not support additional urban development outside the identified urban footprint unless for critical/essential infrastructure or environmental works or land uses consistent with permissible uses prescribed within the TLEP.

### 2.1.4 Additional Advice:

Refer to the North Coast Regional Plan.

Refer to Kingscliff Locality Plan: Volume 1 – Context and locality wide strategies and Volume 2 – Precinct plans.

Refer to Tweed DCP A5 – Subdivision Design Manual.

Refer to Open Space Strategy 2019-2029.

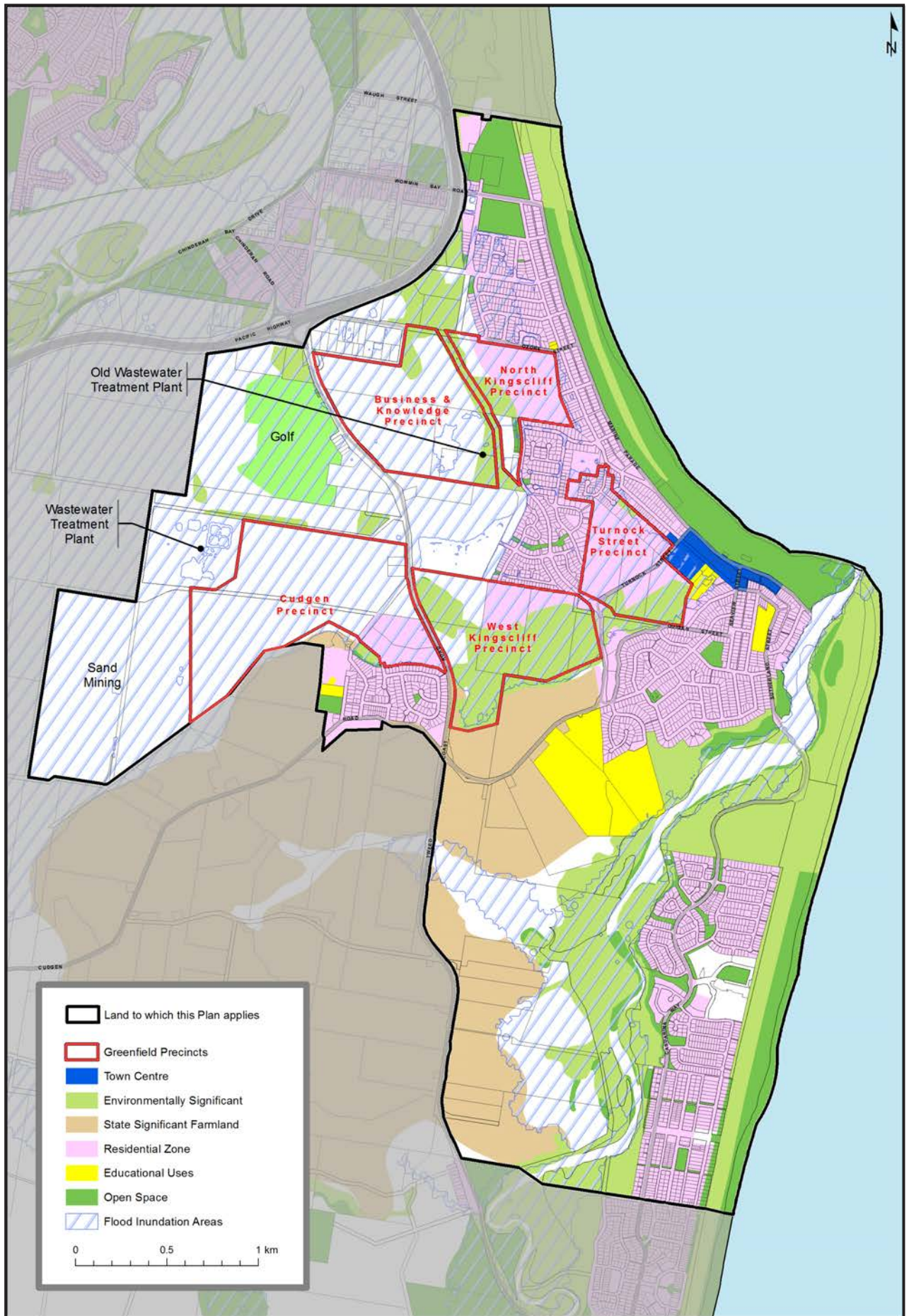


Figure 2.1 Kingscliff Locality Constraints Overlay

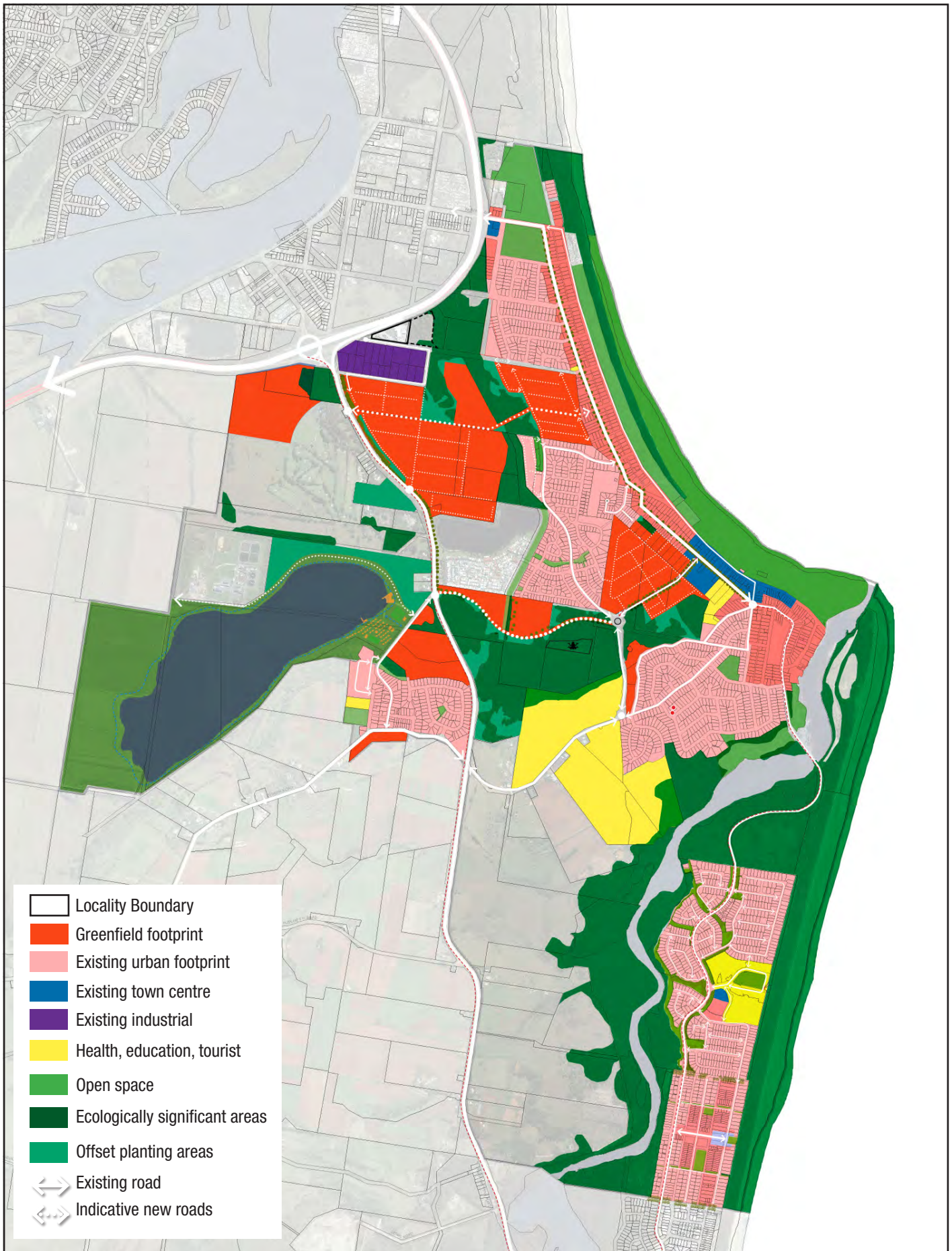


Figure 2.2 Kingscliff Locality Urban Footprint

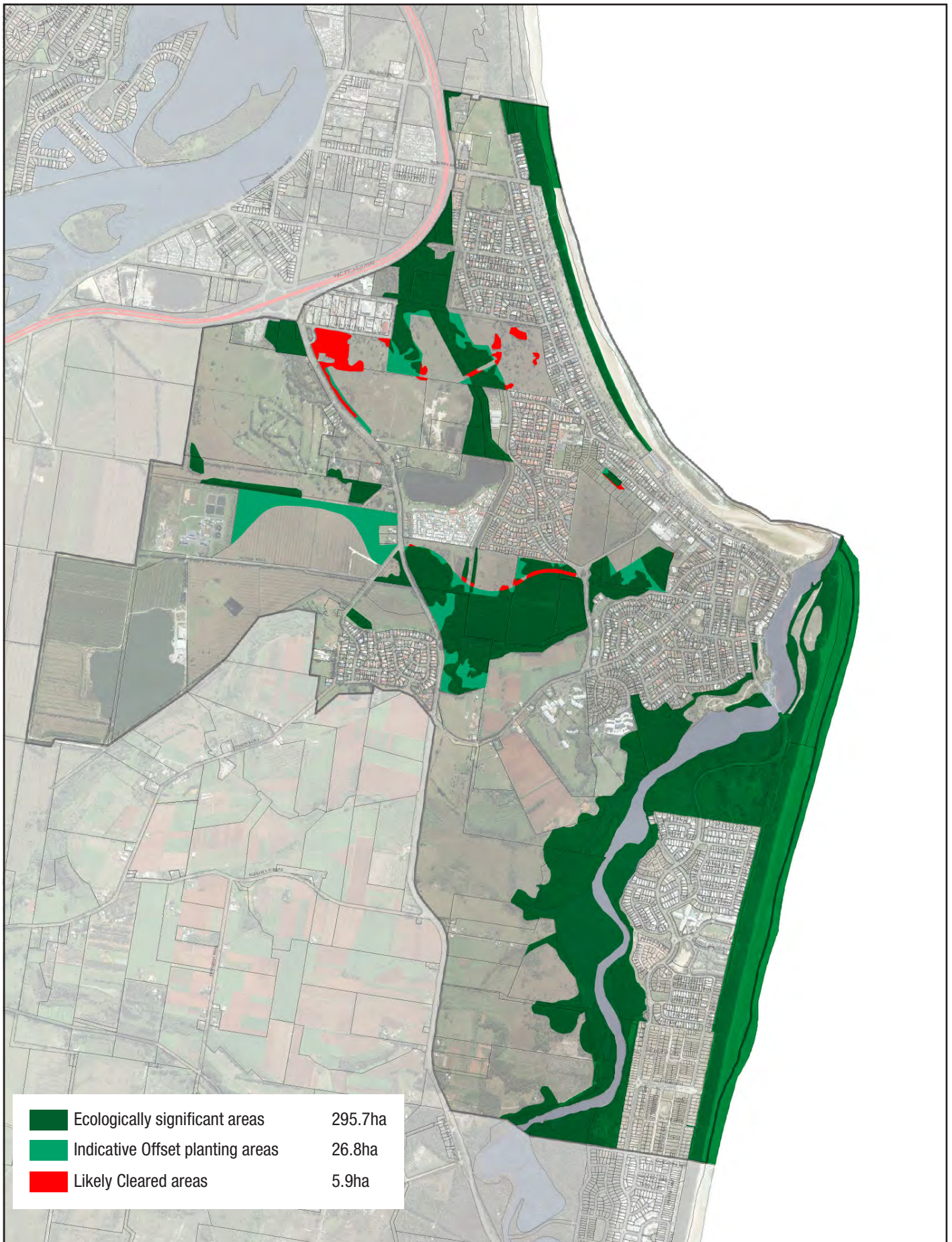


Figure 2.3 Kingscliff Locality Conservation Footprint



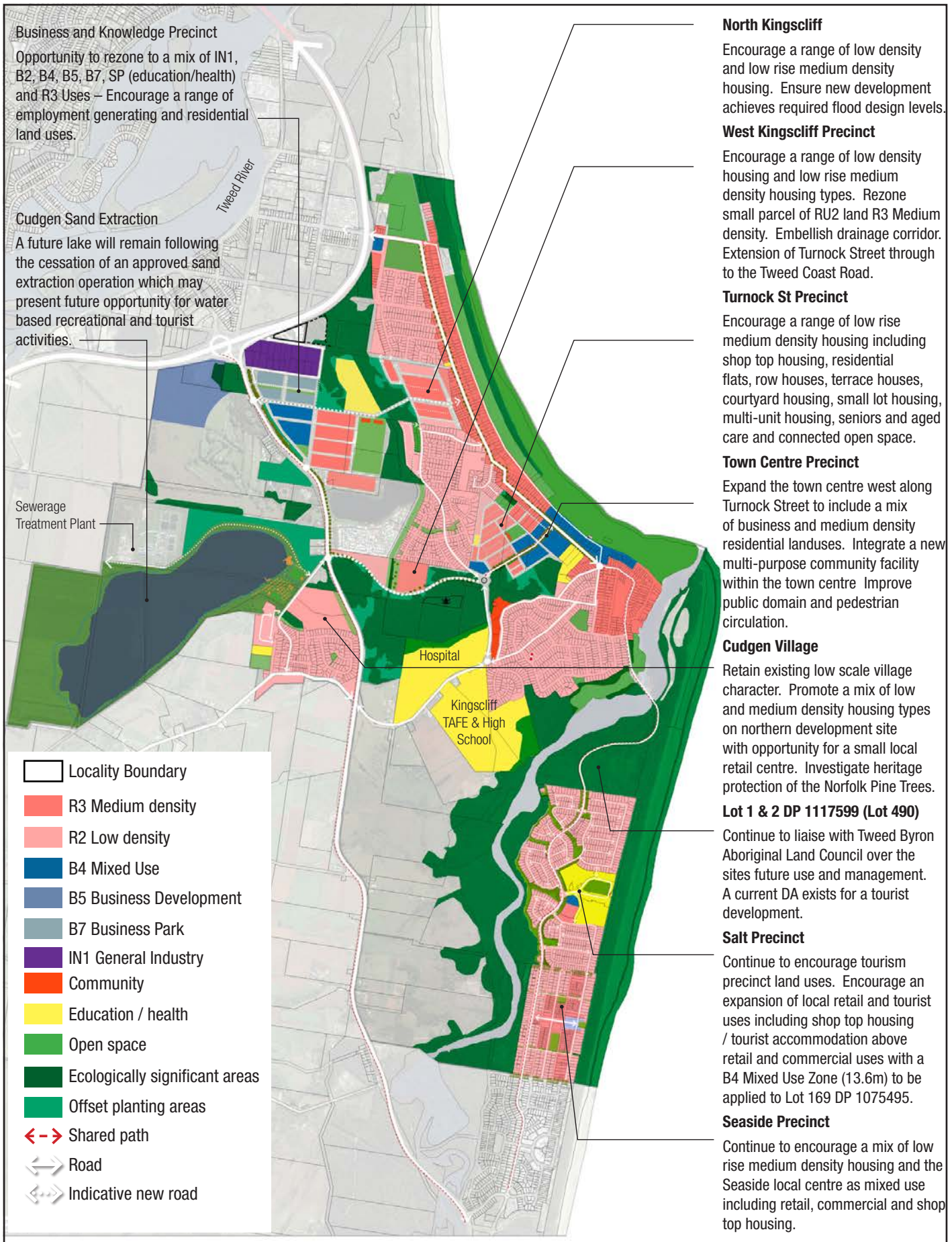


Figure 2.4 Indicative Kingscliff Master plan Plan

## 2.2 Site and context analysis

### 2.2.1 Preamble

#### Statement of Intent

*All new development is to undertake a site and context analysis. Understanding a detailed site and context assessment of a site will enable the development of planning and urban design options which 'work with' the sites natural systems, opportunities and constraints early in the design process. This results in more cost effective and environmentally responsive design which balances development opportunity with environmental, natural and cultural protection. This site design led process embeds a sense of place and character within future neighbourhood developments.*

Detailed site and context analysis ensure that all constraints including but not restricted to climatic considerations, topographic and hydrological condition, buffers, easements, setbacks, areas of environmental protection, connecting open space areas, streets and infrastructure are integrated in a way that maximises land efficiency and minimises the overall cost and impact of development. Figure 2.5 illustrates the cyclic relationship between site and context analysis, structure planning and neighbourhood character.

### 2.2.3 Objective:

1. To undertake a site and context analysis as the first stage of a subdivision design to inform subdivision planning and design decisions.
2. To achieve a sustainable site responsive subdivision design through detailed consideration of site natural systems, opportunities and constraints.
3. To use site and context analysis as a preliminary site design stage to inform structure and subdivision design outcomes.
4. To retain the sites natural, environmental and landscape qualities into future structure design outcomes which underpins a sense of place and character.
5. Enhance local character, identity and sense of place by working with the natural features of a site, retaining key topographic features, understanding climatic influences, landscape elements, view lines and other significant natural, cultural and heritage assets.
6. Use the site and context analysis process as part of a development sites risk management by preventing unforeseen outcomes based on site conditions which could have significant financial and community implications as well as mitigate the need for complex engineering solutions including bulk earthworks.
7. Use the site and context analysis to understand the development sites interface and adjoining land use considerations including but not limited to existing and planned adjoining development, existing street connections, operational buffers, natural hazards, infrastructure locations and requirements, environmental areas, landscape assets, scenic landscape and view lines, public open space networks, topographic and catchment drainage.

### 2.2.4 Controls:

- C1. Any proposed planning proposal, master plan, concept development application and or subdivision application over land(s) within the identified urban footprint (Figure 2.2) must submit a site and context analysis. The site and context analysis should be appropriately documented with 'layered' drawings and a report which documents the site constraint and development opportunities which will inform the subsequent development and design stages.
- C2. The site and context analysis is to identify a development footprint based on an overlay of all known environmental, buffer, topographic, hydrological, landscape, heritage, cultural, infrastructure, hazard and climatic considerations.
- C3. The site and context analysis is to demonstrate how the proposed site design responds to the identified site opportunities and constraints.

### 2.2.5 Additional Advice:

Refer to Tweed DCP A5 – Subdivision Design Manual

Dependant on the size and scale of the proposed subdivision, the site and context analysis is to be forwarded to Council for appraisal and consideration as part of a 'pre-DA' meeting prior to the submission of a development or planning proposal application.

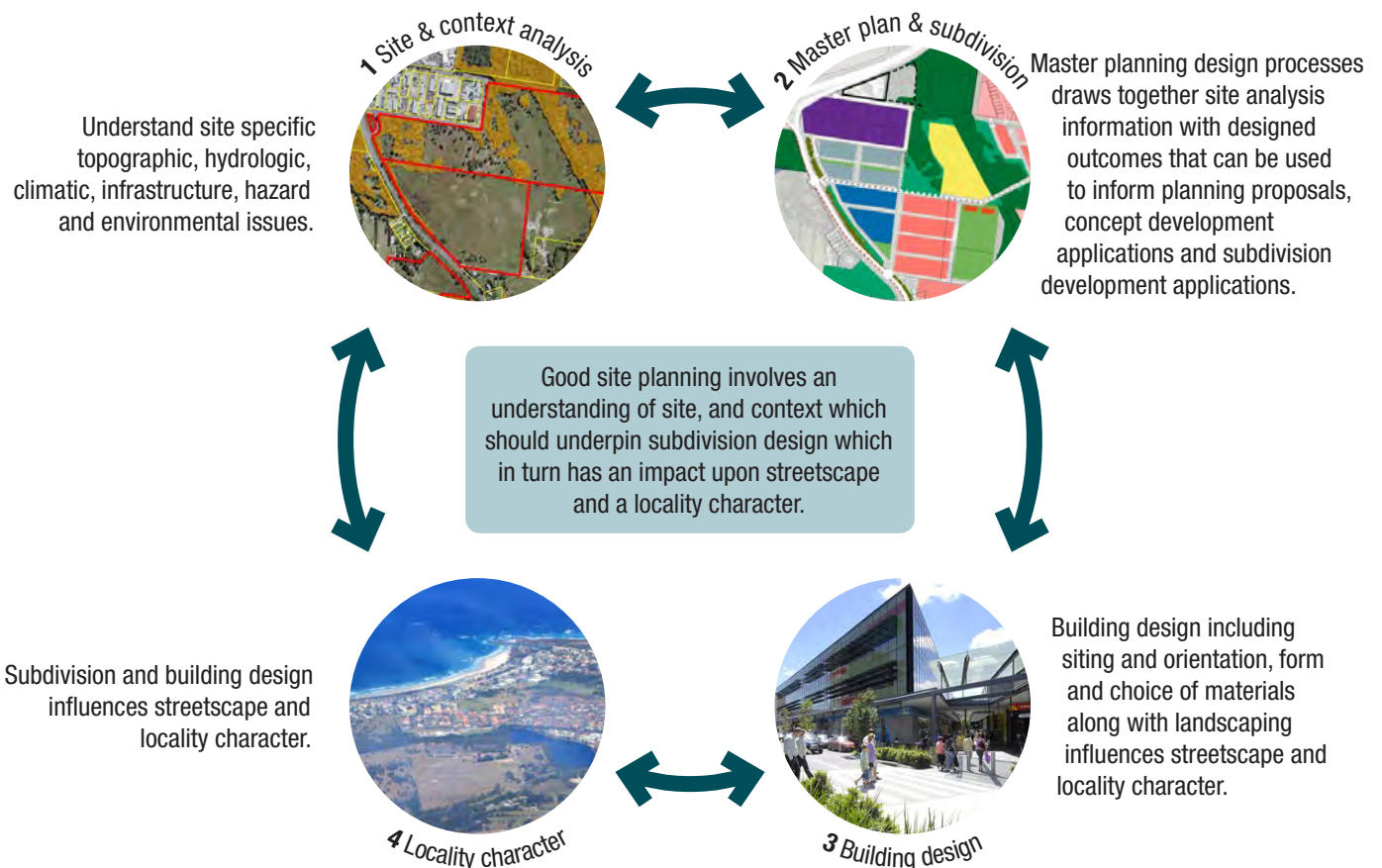


Figure 2.5 Subdivision Site and context analysis

## 2.3 Environment

### 2.3.1 Preamble

#### Statement of Intent

*To facilitate the preservation and enhancement/rehabilitation of areas of environmental significance and provide management for the conservation of threatened and endangered species. This includes the preservation of native bushland vegetation and recognising areas of high environmental amenity value which underpin the ecological, visual and landscape character of the Kingscliff locality. Ensure that proposed uses adjacent to environmental protection zones do not have significant adverse impacts.*

#### Preserve

Environmental surveying has identified significant tracts of lands as having environmental significance and a number of endangered ecological communities (EECs) across the locality. As part of the preparation of design led master planning and subdivision processes over development sites, flora and fauna assessments will be required to acknowledge land of environmental significance and make provision for appropriate buffering and ongoing management. In reviewing protection and vegetation management, which may eventuate in some localised clearing and vegetation offsetting, a whole of locality environmental evaluation which strategically enhances and strengthens ecological links is required. Urban development within land zoned as Environmental Protection or identified as being environmentally significant will not be supported by this Code, as indicated by the Urban Footprint illustrated in Figure 2.2.

#### Enhance

Kingscliff greenfield development sites contain diverse vegetation communities. These communities include coastal heath along the coastline, riparian and wetland communities along the estuarine, Melaleuca and She Oak forest in the low lying flats and Sclerophyll forest across the northern precincts. Koala habitat, including areas mapped as Preferred Koala Habitat, is also present throughout the planning area. Development proponents must explore opportunities to enhance connections between these areas of bushland. The master planning process will play a significant role to provide visual 'breaks' to the built form by way of suitably vegetated corridors permeating throughout the Kingscliff locality. Figure 2.6 provides examples of green corridors/links that must be investigated within future applications both for their environmental and urban design/visual value.

#### Rehabilitate

As part of the future subdivision and development of greenfield development areas of Environmental Protection are to be restored, rehabilitated and managed. The interface between and key habitat areas for protected, threatened and endangered species is a key issue which requires careful management. Urban development within a greenfield context can often reduce the amount of available habitat, fragment retained habitat and sever wildlife corridors. As discussed in other areas of this Code the desired future subdivision road pattern is to include road interfaces with natural areas rather than private land tenure, allowing an improved public interface with these areas, easier management and rehabilitation.

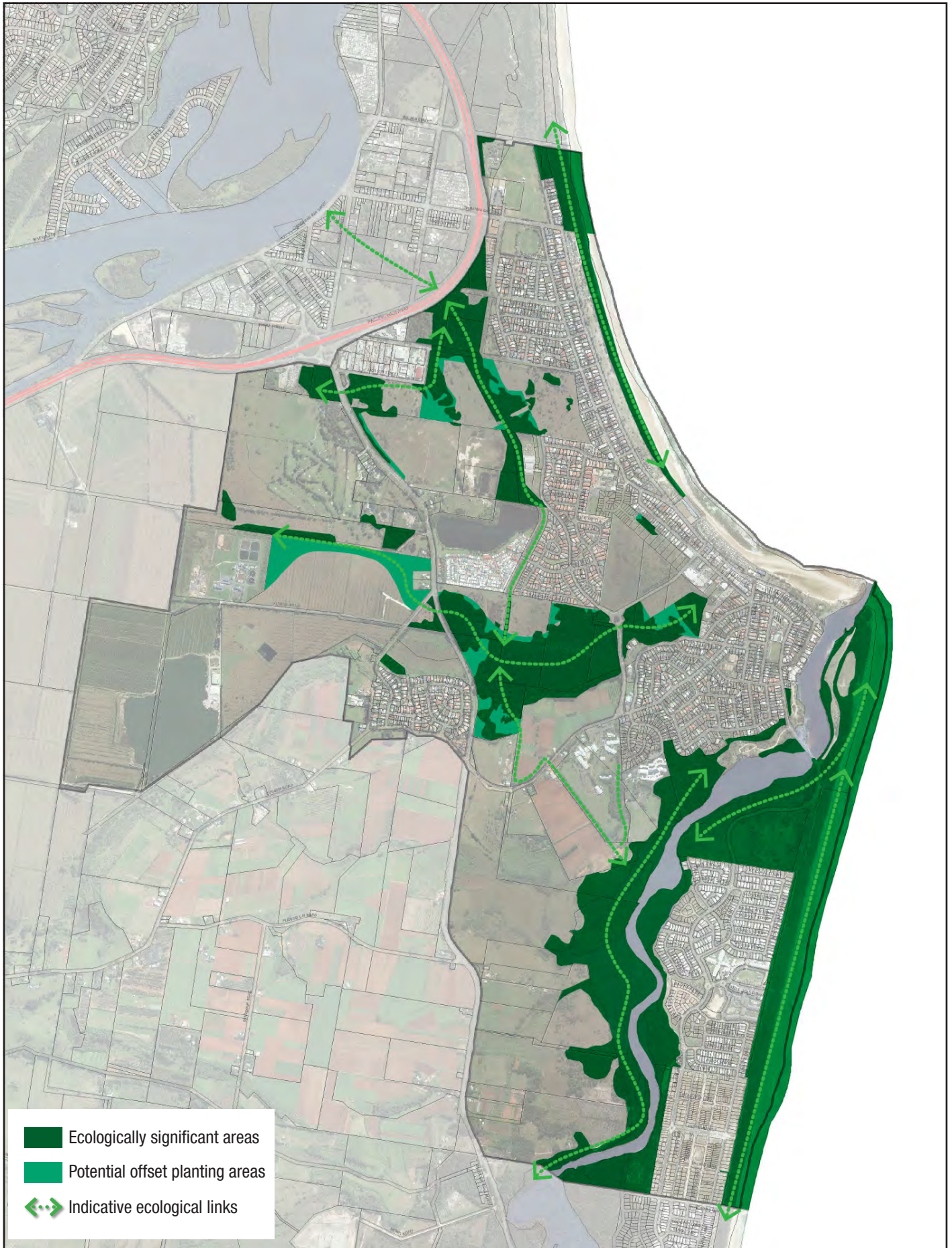


Figure 2.6 Areas of ecological significance

### 2.3.2 Objective:

1. Lands identified as being ecologically significant within the conservation footprint (Figure 2.3) are protected through land use planning.
2. To ensure ongoing management of land of ecological significance from the impacts of urban development.
3. To ensure an integrated approach to establishing any habitat offset areas in the KLP area.
4. To provide a natural growth boundary to residential development and visual relief for the proposed urban environment.
5. To provide for the rehabilitation and enhancement of degraded habitat, strengthen ecological links ensure that comprehensive rehabilitation plans form part of any future development applications or master planning processes.
6. To provide for the protection and improvement of existing hydrological conditions into receiving drainage corridors.
7. To create new and strengthen existing wildlife corridors.

### 2.3.3 Controls:

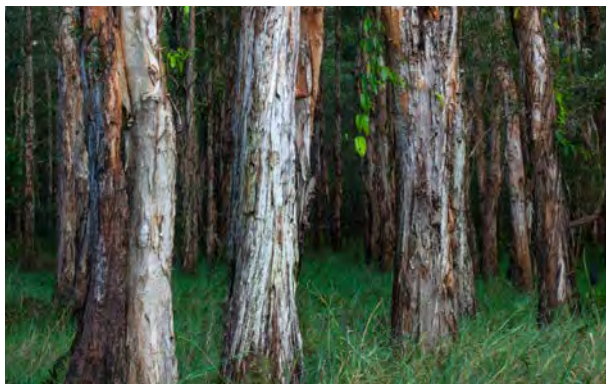
- C1. Any planning proposal, master plan, concept development application and or subdivision application over land(s) within the identified urban footprint (Figure 2.2) must submit:
  - Flora and Fauna assessments prepared in accordance with DCP A19 Biodiversity and Habitat Management will be required to identify the presence of land of high environmental quality, suitable buffering and ongoing management.
  - A habitat Restoration Plan incorporating any proposed habitat offset areas must be prepared in accordance with DCP A19 Biodiversity and Habitat Management within the conservation footprint and any required ecological buffers.
- C2. Except as otherwise noted in C3 below the provisions of DCP A19 Biodiversity and Habitat Management apply to development in the KLP area.
- C3. The clearing of areas included in the Urban Footprint (Figure 2.2) may be approved despite DCP A19 Biodiversity and Habitat Management providing appropriate habitat offsets are established in suitable locations within the KLP area.
- C4. The long term protection and management of land within the Conservation Footprint shall be achieved through appropriate land use zoning, legal agreements (e.g. covenants, planning agreements, stewardship agreements) and potentially land dedication to a public authority such as Council.
- C5. The Conservation Footprint (Figure 2.2) may only be varied through an amendment to this KLP or by a resolution of Council supported by an overarching master plan (or similar document) that demonstrates a no net loss ecological outcome within the KLP area. The following conditions are the minimum requirements to meet this outcome:
  - The total area of any proposed clearing shall not exceed 7.0ha.
  - The quantum of any proposed offsets are calculated using a contemporary biodiversity offset calculator.
  - The structural condition (as calculated using the NSW Biodiversity Assessment Method) of the tallest growth form at the proposed offset site shall be not more than the following proportions of the structural condition benchmarks for the relevant community type: 20% for forests, 30% for shrublands, and 50% for treeless wetlands.
- C6. Through urban structure and master planning processes ensure existing wildlife corridors and vegetative links including those illustrated within Figure 2.6 will be maintained and enhanced. This could be by way of enhancing existing continuous vegetation or providing linking native street trees, verges, corridors and buffers where vegetation is disconnected by roads.

- C7. Demonstrate appropriate buffers between developable areas and areas of ecological significance are established along with ongoing management of buffer land (see DCP A19 Biodiversity and Habitat Management). Infrastructure including roads should form the interface between development and ecological areas and be contained within developable footprint rather than buffer areas.
- C8. Demonstrate appropriate buffers between developable areas and areas of ecological significance are established along with ongoing management of buffer land (see DCP A19 Biodiversity and Habitat Management). Infrastructure including roads should form the interface between development and ecological areas and be contained within developable footprint rather than buffer areas.
- C9. Demonstrate how the works identified within any draft Habitat Management Plan(s) will be responsible for and the intended method of addressing the works required.

**2.3.4 Additional Advice:**

It is acknowledged that land requiring restoration works across the locality may be in fragmented ownership. To this extent, Council is open to discussion with applicants regarding delivery methods for the restoration work identified to ensure equitable distribution across the landowners and development of greenfield sites within Kingscliff. The developer will be responsible for the restoration works of the area of environmental protection to Council’s satisfaction.

Where environmental areas are proposed to be dedicated to Council in any subdivision or other development, Council may enter into an agreement for a maintenance period and contribution for ongoing management prior to handover and all restoration works must be completed to Council’s satisfaction. DCP A19 Biodiversity and Habitat Management contains details or habitat management periods and other arrangements.



**Kingscliff Environment and Ecology** – Kingscliff has a unique and varied ecology ranging from sensitive SEPP 14 wetlands and saltmarshes along estuary areas, melaleuca forests over low lying areas, eucalyptus forest to the north and pockets of remnant rainforest which supports significant endemic flora and fauna species.

Photo Credits: Wetlands Restoration

## 2.4 Green Edges Landscape Character and Views

### 2.4.1 Preamble

#### Statement of Intent

*The landscape and visual character of the locality including greenfield development sites should be recognised and enhanced. Existing significant landscape features will be retained and integrated into the structure and master plan designs. There is an opportunity for new development sites to take advantage of landscape character elements in terms of retaining topography, overland flow paths, watercourses, native vegetation, view fields and other significant stands of vegetation. Outside of existing settlement areas and areas identified as greenfield development sites, non-agricultural uses which may impact the scenic landscape character are discouraged.*

#### Recognise

The unique attributes of the Kingscliff locality has a varied yet strong collective landscape and visual character. In consideration of the areas coastal context, agricultural hinterland, areas of ecological significance, topography and historical land uses the key landscape and visual character components include:

- Dominant ridge line from Cudgen Plateau along Cudgen Road to Cudgen Creek and Sunderland Point.
- Varied vegetation communities including the melaleuca and swamp she-oak forest within the West Kingscliff precincts, the open sclerophyll Forests within the Business and Knowledge Precinct and the sedgeland and remnant rainforest pockets within the Turnock Street precinct. Agricultural, farmland and hinterland views experienced heading north and south along Tweed Coast Road and east and west along Cudgen Road.
- Elevated north and east views from various aspects on Kingscliff Hill to Fingal Headland, Cook Island and the Pacific Ocean and south west elevated views from various aspects on Kingscliff Hill to Cabarita Headland and west around to Mt Warning and the border ranges.
- Views north and south experienced from the beaches, coastal foreshore areas and break walls and views along the creek.
- Long views north towards the Terranora ridge and west towards the Border Ranges.
- Green break or vegetated buffer (Lot 1 and 2 DP 1117599) between the northern extents of Salt and the Cudgen Creek Bridge.
- Large street trees including the town centre Fig Trees and Cudgen Norfolk Pine Trees.

Many of Kingscliff's greenfield development sites are visible from higher vantage points both from within and outside of the locality. Some of these view fields are experienced from the northern side of the Kingscliff Hill precinct looking north over the Turnock Street precinct, the south side of Kingscliff Hill looking south towards Salt and Seaside precincts and the northern side of Cudgen Village which has a vantage looking north-east and north-west. Other vantages over greenfield development sites are experienced along the northern side of Duranbah Road and the southern side of Terranora Road. The potential impacts on these view fields over time will be the replacement of undeveloped agricultural views interspersed with vegetation with future urban development footprints. Figure 2.7 illustrates the key landscape character zones and prominent views and vantage points experienced in and around the locality.

#### Enhance

There is an opportunity through future subdivision design over the lower set flat greenfield development sites to strengthen and enhance landscape character. This is achieved by retaining and strengthening existing stands of vegetation and retaining key topographic features. Retention and strengthening of vegetation in particular has the effect of creating a green edge to new precincts and green buffers between precincts. Subdivision landscape design together with WSUD opportunities should integrate strong ecological links through precincts and where possible be combined with connecting pathways and cycle routes. Street trees should be specified to suit the local landscape character and streetscape context. Within subdivision areas street trees should be located to be clear of potential driveway cross overs and to enhance streetscaping and shade opportunities.



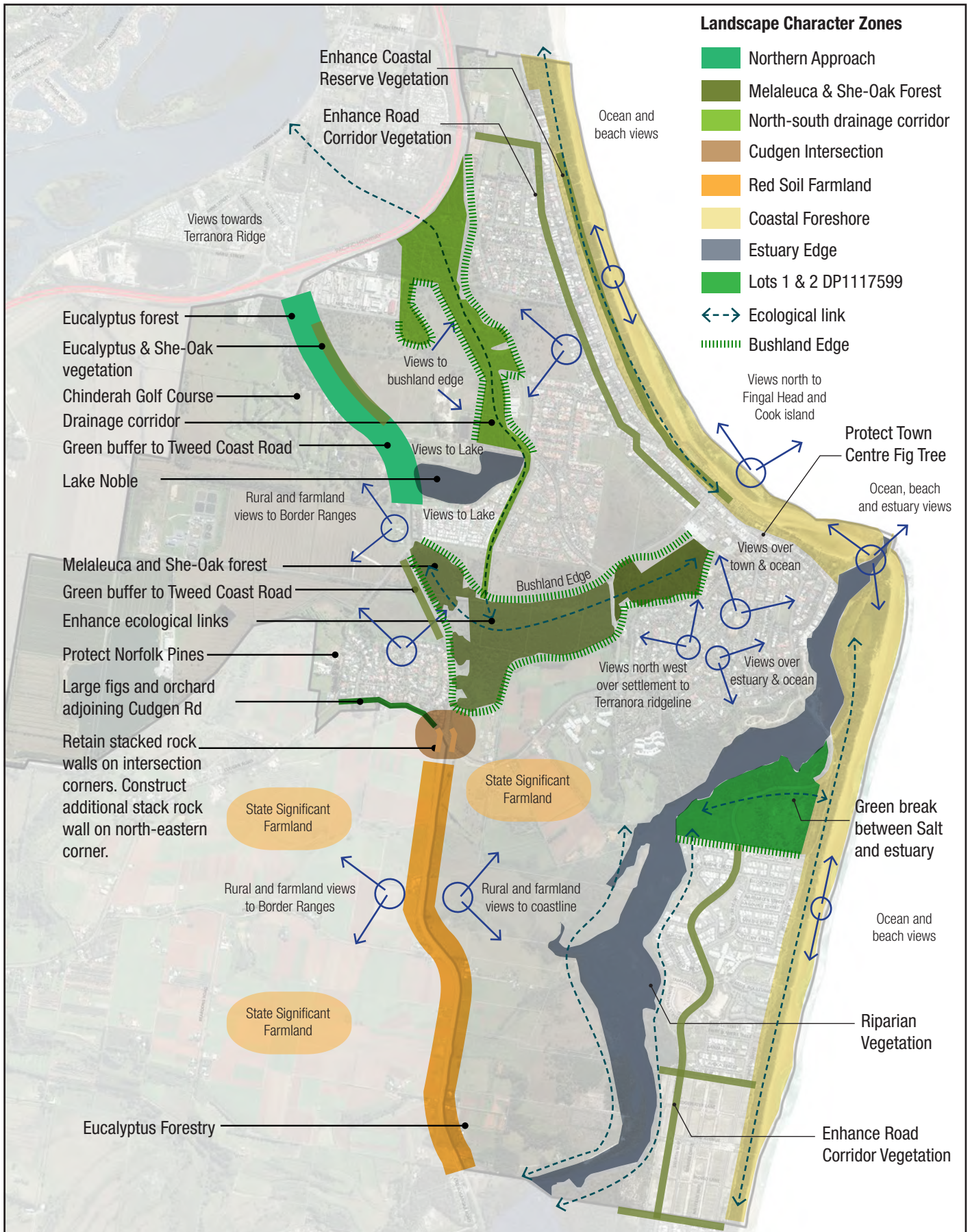


Figure 2.7 Landscape Character Zones



**The northern approach** – The business and knowledge precinct forms part of the northern approach defined by sclerophyll forest on the western side and scattered She-Oak Forest on the eastern side.



**Melaleuca forest** – The melaleuca forest within the West Kingscliff Precinct defines the eastern and western side of Tweed Coast Road has a canopy height of between 5–15m and forms part of a larger 36ha vegetative area (to the east) which has a high ecological value.



**North Kingscliff greenfield** – The development and subdivision of the north Kingscliff greenfield site will require filling above the design flood level. Notwithstanding this required change in landform, the site is framed by vegetation which provides a strong contained landscape character and green backdrop to the precinct. The subdivision design should strengthen this character with consideration of terminating views lines to the bush, creating edge roads rather than back fences to the bush as well as drawing connected landscape areas through the precinct.

### 2.4.2 Objectives:

1. Maintain the integrity of vegetation, watercourses and natural topographic features as important features of the environmental, landscape and visual character.
2. Ensure site modifications, excavation, fill, retaining walls and engineered elements do not adversely impact on the visual and landscape character of a precinct and broader locality.
3. Identify and retain key landscape and visual character features of the site and seek to integrate those landscape features through a contemporary urban structure and built form outcomes.
4. The identification, retention and strengthening of green breaks, important feature trees and or stands of trees within master planning and subdivision processes.
5. Ensure view sharing and maintenance of view fields including maintaining important regional and local views.
6. Discourage non-agricultural development outside of the defined urban footprint which will impact the landscape and visual character.

### 2.4.3 Controls:

- C1. Any planning proposal, master plan, concept development application and subdivision application over the identified greenfield development sites must submit a visual impact assessment. Using Figure 2.7 as a reference the visual analysis should:
  - i. Provide an assessment of the existing visual and landscape character including an assessment of any impacts from the proposed subdivision development.
  - ii. Identify significant landscape features including overland flow paths, dams, native vegetation and other significant stands of vegetation across the subdivision site and seek to retain or interpret these important elements of the sites visual character. Suggested means of embodying these components include adapting existing vegetated wind break lines as street trees, to create more visually attractive streetscapes; utilising overland flow paths as ecological corridors and passive movement links (walking and cycling), maintain the presence of existing mature trees to assist in visually defining the identified character zones and preserving ecological habitat.
  - iii. Identify landscape and character zones within the greenfield development precincts as a means of defining and containing the urban structure as well as contributing to the overall principle of creating a network of precincts connected and framed by green space.
  - iv. Demonstrate how the resultant urban structure, subdivision landscape design and road layout has been informed and takes advantage of landscape and visual character conditions and principles.

### 2.4.4 Additional advice:

Refer to the Kingscliff Locality Plan Vol 1 & 02 and the Tweed Scenic Landscape Strategy.

## 2.5 Land forming

### 2.5.1 Preamble

#### Statement of Intent

*Maintaining the integrity and intrinsic landscape visual character by minimising bulk earthworks in co-ordination with implementing required flood and drainage mitigation measures.*

Given the low lying and flood prone nature of much of the identified greenfield development sites, land forming including bulk fill earthworks will be required as will be establishing an evacuation route via rising roads from lower lying greenfield areas. In this regard, a strong design focus must be employed to ensure that subdivision design meets required design flood levels and evacuation requirements whilst designing to mediating potential level differentials between existing, developable and undevelopable areas.

To address the land forming subdivision considerations, any planning proposal, master plan, concept development application and subdivision application are required to clearly identify developable envelopes as an outcome of the site analysis process and indicate required site fill levels. Where development sites adjoining existing residential precincts and areas of environmental protection documentation is to indicate all required buffers and setbacks and adjoining site levels. Where level differentials occur, design measures to mitigate risk and amenity based issues are to be addressed.

### 2.5.2 Objectives:

1. Maintaining and respecting the landform over the entire balance of greenfield development sites including both the developable and undevelopable areas.
2. To carefully design and integrate flood mitigation and drainage through appropriate land forming and drainage works in a way which is compatible with the overall landscape character of the development sites.
3. Given the relatively flat landform of the identified greenfield development sites promote subdivision and building design outcomes which reduce the need for significant post subdivision land forming.
4. Adopt an overall bulk earthworks strategy that seeks to:
  - i. Limit modification of site levels at boundaries to maintain amenity to adjoining properties.
  - ii. Integrate flood mitigation and drainage works within the overall land forming and subdivision design.
  - iii. To ensure site modifications, retaining walls and engineered elements do not adversely impact on adjoining existing settlement areas or the streetscape character.
  - iv. Ensure that fencing on top of retaining walls does not adversely impact amenity of neighbouring properties or de-stabilise retaining walls.

### 2.5.3 Development Controls:

- C1. Any planning proposal, master plan, concept development application and subdivision application over the identified greenfield development sites must submit:
- Plans displaying compliance with the development controls outlined in the Tweed Development Control Plan Section A5 – Subdivisions Manual.
  - Accurately represented and documented detail of all proposed site works including cut, fill and all retaining and interface walls which demonstrate minimisation of earthworks and integration of land forming design with flood mitigation works including flood evacuation routes and drainage works.
  - Land forming plans are to detail the location, management and final placement of Class 6 Soils in order to preserve and productively utilise this soil.
  - Acid Sulphate Soil Management Plans over low lying sites where land forming is proposed.
- C2. Maintain the integrity of natural topographic and landscape features as an important part of the locality and precinct character by including the following:
- As part of the subdivision and land forming design use the road layout to define the developable envelope where it adjoins natural areas.
  - Step and landscape retaining walls and batters to filled areas at developable area edges rather than a single vertical retaining wall.
  - Utilise WSUD drainage swales where appropriate improve water permeability across development sites, reduce the overall quantity of fill required to provide adequate drainage, mitigate flood impacts and strengthen landscape character.
- C3. Where greenfield development sites directly interface with existing settlement areas, fill levels shall be consistent where both are above design flood level. Where existing settlement sites are below design flood level heights, new development areas are to be constructed at design flood levels. Interface retaining walls/ batters are to be stepped with the integrated landscape at boundary interfaces to reduce the visual impact of retaining walls and level differential.

### 2.5.4 Additional advice:

Refer to the Tweed DCP A5 – Subdivision Manual and DCP A3 – Development of Flood Liable Land.

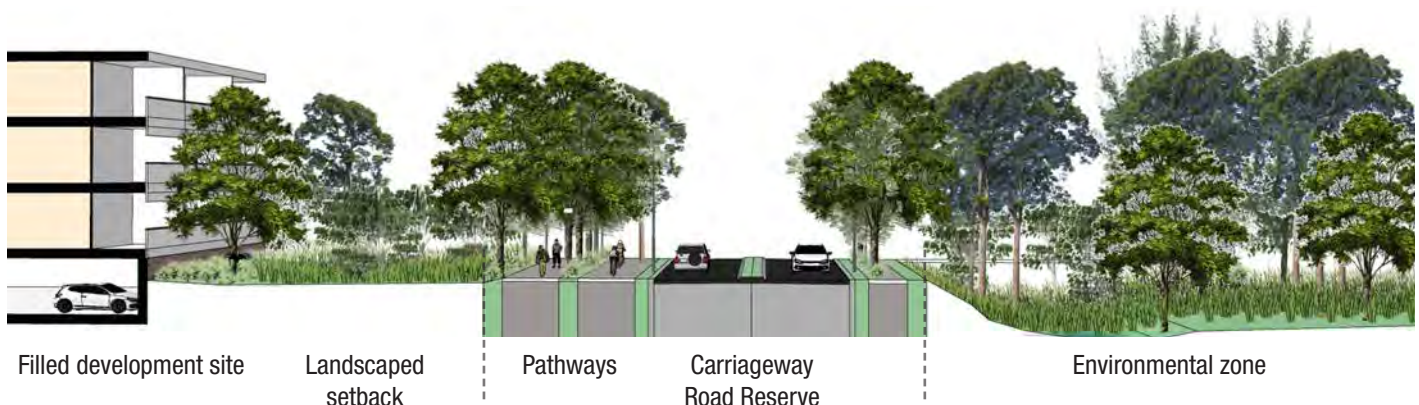


Figure 2.8 Land forming – Roads serve as the interface between filled development areas and environmental areas providing a physical buffer as well as good landscape and drainage opportunities.

## 2.6 Traffic Access and Movement

### 2.6.1 Preamble

#### Statement of Intent

*The Tweed Coast Road is to be reinforced as the key connector road with new east-west connections through the West Kingscliff Precinct (Turnock Street extension) and through the Business and Knowledge Precinct improving access to the town centre and residential precincts. The road network is to be supplemented by an embellished passive movement network of pedestrian and cycle paths along the existing north-south drainage corridor and along Turnock Street. New subdivision roads are to generally be aligned in a north-south and east west orientation to maximise the opportunity for best solar orientation for allotments. The curvilinear streets and cul-de-sacs which result in reduced connections, movement legibility and less regular shaped lots are discouraged.*

#### Road Network

The Tweed Coast Road is the connector and key movement corridor which services the Kingscliff locality. The Tweed Coast Road provides direct access onto the Pacific Highway allowing ease of movement north to Tweed Heads and beyond to the Gold Coast where many of the local residents commute each day to work. The stretch of Tweed Coast Road between the intersection with the Pacific Highway to Casuarina is planned to be widened to four lanes in the future as part of the Tweed Road Development Strategy. The general road network has been illustrated at Figure 2.9.

Despite the relative proximity to the highway, there is only one point of access between Tweed Coast Road and Kingscliff township via the Cudgen intersection. To improve the connector options into the township from Tweed Coast Road, an extension to Turnock Street intersecting with Tweed Coast Road near the Altona Road intersection and the Ozone Street link has been included within the Tweed Road Development Strategy. The timing and alignment of this new connector road will be dependent on the development of the West Kingscliff precinct in consideration of traffic generation from these release areas as well as the resolution of the environmental and flooding constraints.

To the south, Casuarina Way connects to the Tweed Coast Road at Casuarina Town Centre. It connects South Kingscliff across Cudgen Creek to Sutherland St. This traffic can then enter the town centre at Pearl St/Moss St or use the local access streets (Viking St and McPhail Ave) to connect with Cudgen Rd and the Tweed Coast Road beyond.

#### Road Layout

Road layouts in new subdivisions must seek to provide an efficiency of vehicular, pedestrian and cycle movement as well as work to optimise solar orientation for future development sites. Given the relatively flat topography of the greenfield development sites, new subdivision roads are to generally be aligned in a north-south; east west orientation to maximise the opportunity for best solar allotment orientation. The orthogonal street pattern enhances through connections, legibility and results in more regular shaped building lots rather than curvilinear streets and cul-de-sacs.

The road layout is to reflect and buffer urban development from environmental land and hazards by forming the boundary between urban development to environmental land, (including informal open space such as wildlife corridors and significant overland flow paths). A road layout that avoids a private interface to environmental land and green corridors will assist in the ongoing maintenance, retains public access to environmental land or informal open space as well as provide high levels of amenity to pedestrians and road users.

Kingscliff greenfield development sites should seek to integrate with the wider residential fabric of the locality. The creation of a logical street network that acknowledges and respects its wider context and contributes to improved connectivity and accessibility throughout the locality is encouraged. Road networks which result in dead end or enclave development is generally discouraged.

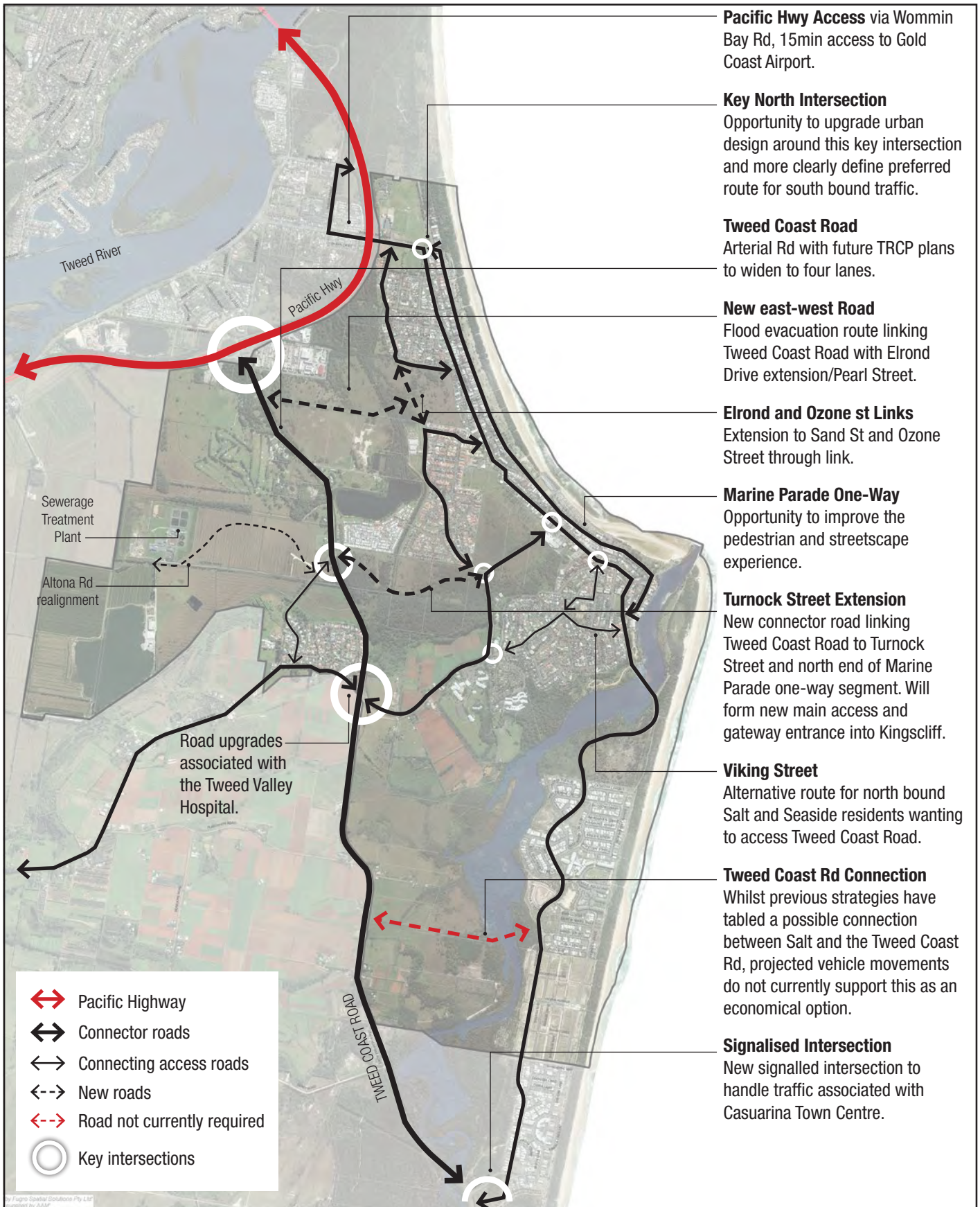


Figure 2.9 Kingscliff Locality Road Network Strategy

## 2.6.2 Objectives:

1. Road layouts in new subdivision must seek to provide an efficiency of vehicular, pedestrian and cycle movement as well as work to optimise solar orientation for future development sites.
2. Integrate the principles of WSUD into street and open space design where practical.
3. Progressively implement the construction of the Turnock Street extension.
4. The design of Turnock Street as the primary neighbourhood connector into the Kingscliff township is to create a sense of place through a range of public domain treatments and address pedestrian movement and comfort, efficient vehicle movement, and establish a key entry statement and journey to the overall character and appearance of Kingscliff.
5. A road layout and design that provides integration between the existing urban fabric, particularly to the East and West, for an efficient bus transport option. Suitable locations and attractive bus shelter designs should be determined to further encourage this sustainable mode of transport.
6. A road network and layout that establishes a clear and legible configuration contributing to way finding and establishing a strong streetscape character in terms of carriage widths, verge, street trees and implementation of water sensitive urban design principles.
7. Adequate integrated bicycle facilities (parking and on/off street routes). Particular consideration should be given to providing North-south and East-West links throughout Kingscliff to further encourage this healthy and sustainable form of transport.
8. Encourage convenient and safe pedestrian access to all facilities whether it is for work or recreational purposes via a comprehensive pedestrian network.
9. Ensure that there is sufficient road capacity to support future traffic growth and that the existing intersection methods of control are adequate to accommodate future traffic levels.
10. Encourage that the access between residential precincts and the town centre is as direct and convenient as possible and avoids unnecessary circuitous routes.



**Streets for people as well as cars** – New streets should be designed to provide opportunities for safe pedestrian and cycle connectivity, WSUD treatments, appropriate street and pedestrian lighting and tree lined streets to achieve a high level of visual and user amenity.



### 2.6.3 Development Controls:

- C1. Any planning proposal, master plan, concept development application and subdivision application over the identified greenfield development sites must submit:
  - Traffic Study.
  - Road network plan including flood evacuation routes.
  - Street sections.
  - Passive movement strategy (Cycle and walking paths).
- C2. Any subdivision application over the identified greenfield development sites (Figure 2.2) seeking development consent prior to the construction of the Turnock Street extension must be accompanied by a traffic study demonstrating the ability for the proposal to be accommodated by existing or alternative proposed road networks to the satisfaction of Council.
- C3. The design of Turnock St extension is to include a range of public domain treatments which address efficient traffic movement as well as pedestrian movement, landscape and streetscaping, WSUD treatments where possible and establishment a key entry statement at the intersection with Tweed Coast Rd.
- C4. A passive movement strategy is to be submitted with any subdivision development and illustrate how the proposed subdivision will integrate pedestrian and cycle movements within and beyond the development site and link to key facilities.
- C5. Where identified on Figure 2.10 shared pathways should be integrated and developed as part of the road and passive movement network.
- C6. Applicants must investigate integration of public transport services in consultation with the local public transport provider and ensure those considerations are incorporated into any planning proposal, master plan, concept development application and subdivision application. This includes routes suitable for a buses, a central bus interchange and bus stops, suitable pavement widths and appropriate bus stop locations.
- C7. Suitable locations for bus shelter should be determined as part of the road layout planning to ensure ease of access to further encourage this sustainable mode of transport. Bus shelters must be designed to a universal access design standard and include lighting.
- C8. Road layout design and street sections are to complement the relatively flat landscape character of the greenfield sites with a preference for a north-south and east-west grided road layout to optimise passive design opportunities for resultant built form development.
- C9. Ensure that a road forms the edge to the natural and environmental protection areas providing a public rather than private interface to the buffers and areas of environmental protection for bushfire and ecological management access.
- C10. Road layout and design is to be clear and legible and provide opportunities for landscaping, street trees and consideration of potential long or cross site view opportunities.

### 2.6.4 Additional advice:

Refer to the Tweed DCP A5 – Subdivision Manual and DCP A3 – Development of Flood Liable Land.



**New Bus Interchange** – The adopted Tweed Public Transport Strategy is to develop a Rapid Bus Transit Corridor Plan from Gold Coast Airport Transport Hub to Pottsville through Kingscliff with links to Murwillumbah.



**Encourage cycling and walking** – A network of pathways and cycleways should connect all residential areas with key open space and activity centres.



**Universal design** – New bus shelters, pathways and public domain areas should be designed to meet universal access design standards.



**Turnock Street Montage** – Turnock Street will form the main access road into Kingscliff township from the Tweed Coast Road, and presents a significant opportunity to be designed as a tree lined boulevard with dedicated pedestrian and cycle path.

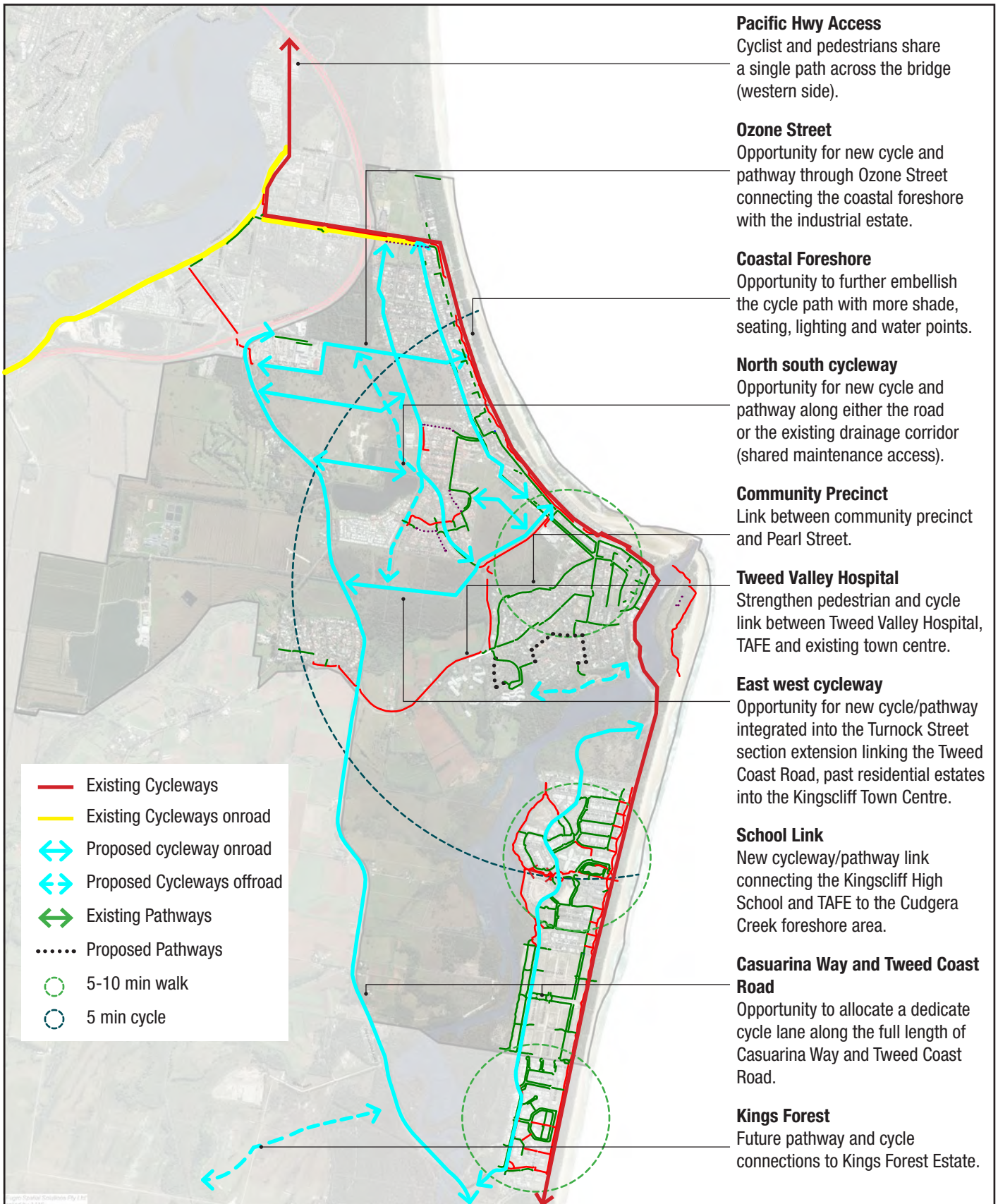


Figure 2.10 Kingscliff Pedestrian and Cycle Network

## 2.7 Open Space

### 2.7.1 Preamble

#### Statement of Intent

*The Kingscliff locality will have a connected network of open space offering a diverse range of open space types. This includes active open space such as sports fields, passive open space including neighbourhood parks, walkways, cycle paths as well as nature based recreation opportunities. New areas of open space will be well located and easy to access with the target that all residential precincts will be within 500m walking proximity. There is opportunity to integrate existing north-south and east-west drainage corridors as passive movement links connecting residential precincts with key activity centres such as the town centre, employment areas, schools, community landuses. An extensive walking and cycling path network which is shaded, well lit and with adequate seating and drinking water will improve passive movement amenity and may lead to reduced car based movements.*

#### Integrated Open Space

Figure 2.11 illustrates the existing provision of active and passive open space within Kingscliff. Figure 2.12 illustrates the potential future active and passive open space which includes the following embellishments and additions:

- Continued embellishment of the Kingscliff Sport and Recreation Complex which provides 13.4ha of active open space serving the existing sports clubs and residential population.
- Continued embellishment of the coastal foreshore areas which is a highly accessible lineal passive open space area linking the northern residential precincts with the town centre as well as providing access to the beach.
- An additional 9.5ha of open space within the Business and Knowledge Precinct to meet future active and passive open space need.
- Embellishment of parks in existing residential precincts and the provision of new parks throughout the future greenfield development sites ensuring all residents are within a 500m walkable catchment to open space and contributing to the overall network of open space.
- Embellishment of a future north-south passive movement link (pedestrians and cycle) following the existing drainage corridor.
- Improve the amenity of the east-west pedestrian and cycle link along Turnock Street to be extended through to Tweed Coast Road.
- Provision of a future east-west passive movement link (pedestrian and cycle) across the Business and Knowledge Precinct and North Kingscliff.
- Potential for 16.6ha of land within the Cudgen Precinct (west of Tweed Coast Road) as a long term opportunity to provide additional regional active open space at the cessation of sand extraction.

Both active and passive open space must integrate strongly with the localities urban structure and greenfield development design principles. Where increased densities are proposed, there will be a requirement for proportionally more public open space. New open space areas will be integrated with the wider open space network. In the case of greenfield development sites there is a good opportunity to utilise the existing drainage corridors as passive movement corridors between residential and open space areas.

#### Alternate Open Space Provision

Whilst there is currently a shortfall of planned and funded active open space servicing Kingscliff now and into the projected future, there is a surplus of passive open space on account of the extensive coastal foreshore reserves. The coastal reserves currently provide good proximity and access to many existing and planned residential precincts. As such Kingscliff is uniquely positioned to investigate and promote alternative open space and recreation options. Accordingly, where it can be demonstrated that a surplus exists within a walkable proximity of residential precincts Council is open to greenfield development master plans to investigate alternative passive open space outcomes.

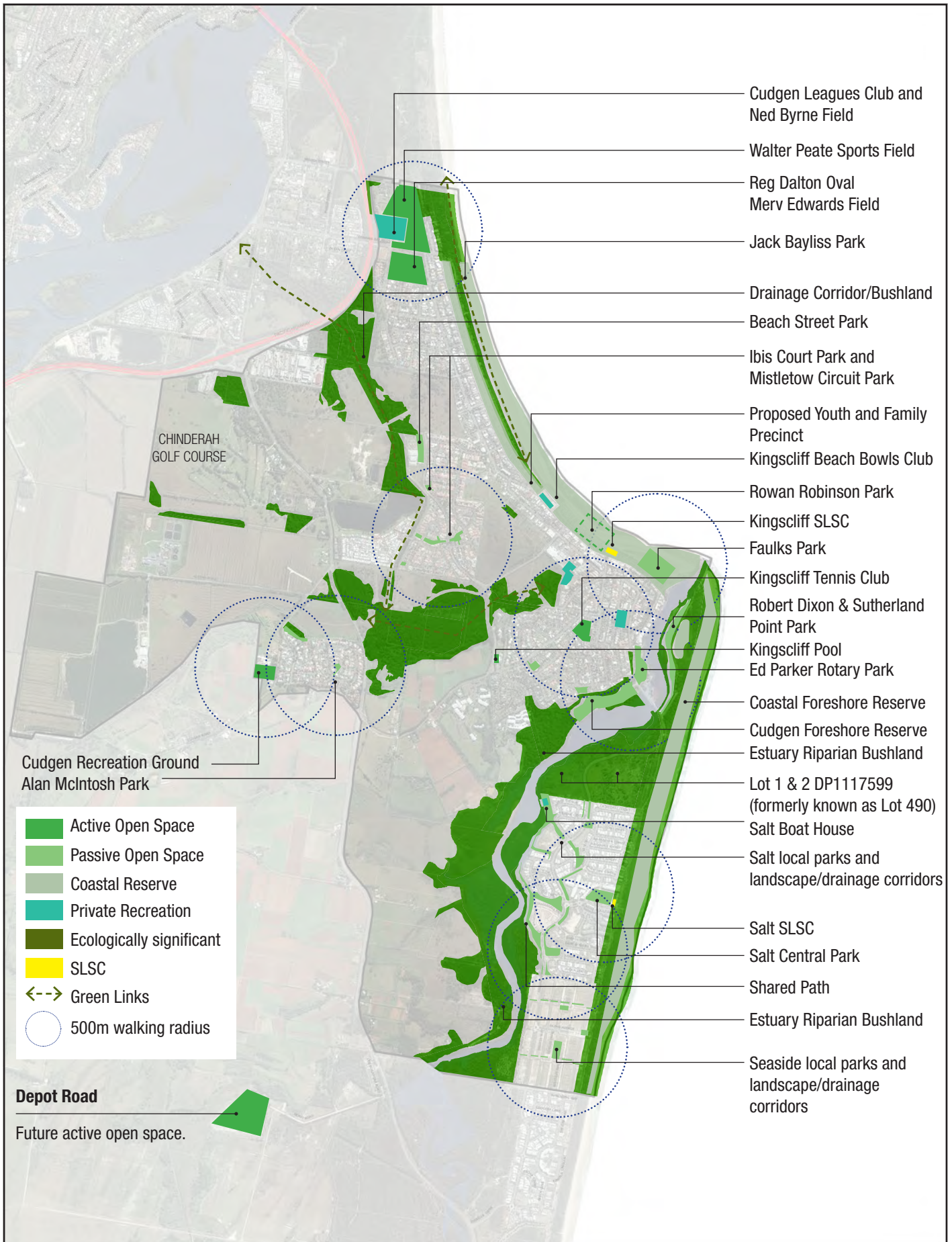


Figure 2.11 Existing Distribution of Open Space

Investigations into alternate forms of open space provision should seek to harness the attributes of the site and broader locality context and open space need. For example, embellishing both existing and new passive movement corridors (pedestrian and cycle) could be considered in lieu of passive open space where there is a demonstrated surplus of passive open space to a particular precinct and reflect community need, with due consideration of changing leisure patterns and demographic changes. Other alternatives could include monetary contributions towards the further embellishment of existing open space areas, provision of an off leash dog park, development of a family and youth precinct (including skate park) or contribution towards streetscape and public domain projects within the town centre.

The possibility of nature based recreation may be present in areas of ecological significance, open space would not be the prime use and as such could not be considered as such for the purpose of provision of open space quantities. Scenic and environmental values often conflict with recreational uses that may lead to the degradation of these areas. Whilst Council encourages investigations into the delivery of alternate forms of open space these uses must be balanced against preserving and protecting the environmental qualities of the Kingscliff locality.

### 2.7.2 Objectives:

1. Ensure the provision of additional active and passive open space within the Kingscliff locality is consistent with future demand as identified within the Open Space Strategy (2019-2029) and build upon the existing network of open space across the locality and beyond.
2. Ensure a diverse range of open space to support a variety of active and passive recreational activity improving community health and well-being as well as reflecting expressed community need.
3. Open space to be integrated with the urban environment and designed to provide an identity and a sense of place for each precinct by responding to important landscape features, scenic qualities, cultural or historic characteristics of the undeveloped site.
4. The open space network and the road network will be fully integrated to provide a network of pedestrian, shared paths and cycleways to achieve good access and improve connectivity and site permeability.
5. Ensure commitment to the design of high quality multi-functional open spaces that are welcoming, attractive accessible and safe, incorporating the principles of Universal Design, social inclusion and sustainability.
6. Provide opportunity for developer investigations into alternate open space types, uses and facilities in lieu of standard open space requirements in areas where there is a demonstrated surplus of passive open space.
7. Within new subdivisions ensure open space is conveniently located, of suitable dimensions, is fit for intended use, unencumbered and is safely accessible to all users.
8. Ensure green linkages are provided through residential precincts (existing and new) to connect the open space system into the greater (external) network to form an integral part of the open space network, improve amenity, encourage walking and cycling and improve biodiversity links.
9. Integrate the principles of water sensitive urban design (WSUD) principles into street and open space design.

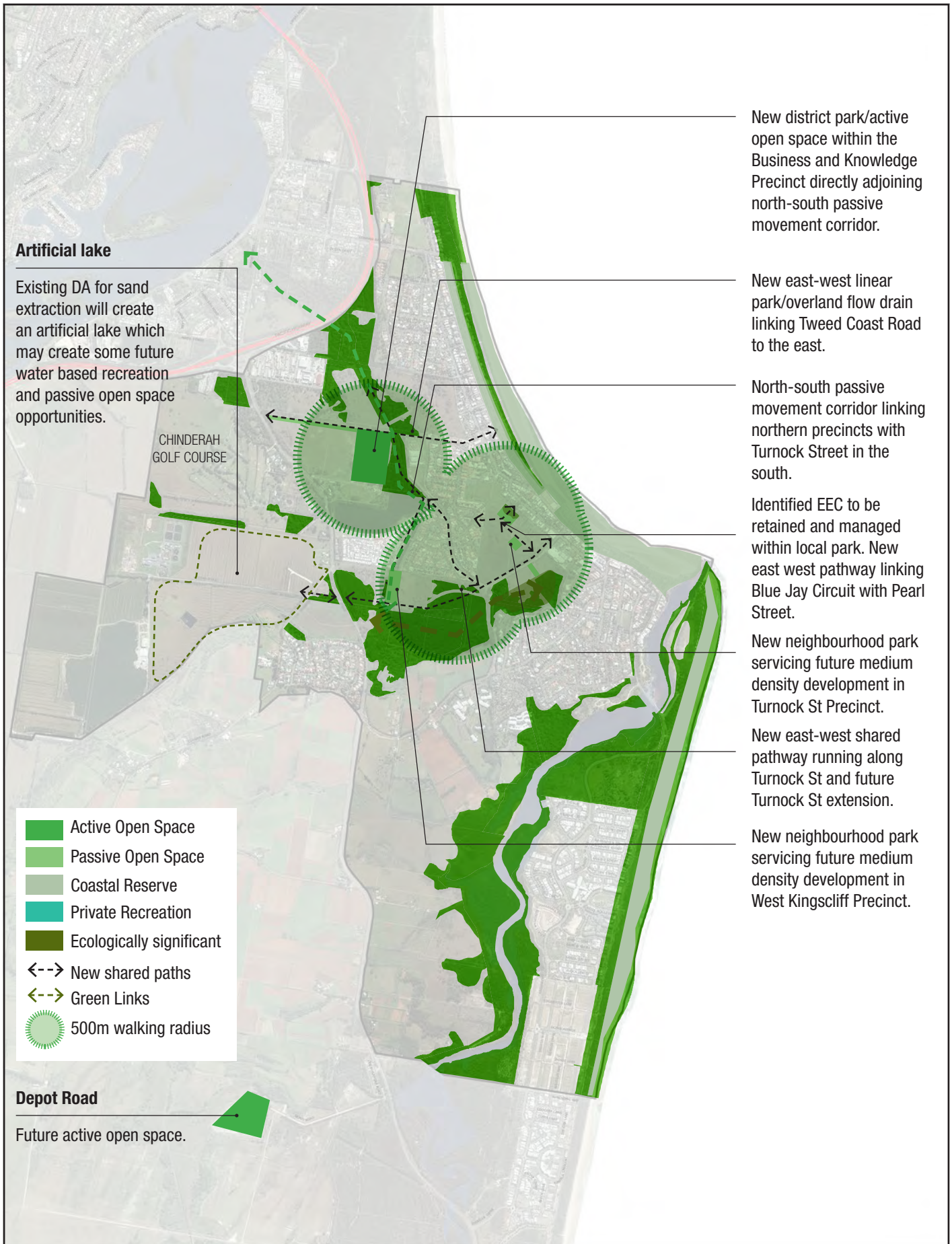


Figure 2.12 Potential new greenfield open space area

| <b>Kingscliff – Existing</b>           |                      |                      |                        |
|--|----------------------|----------------------|------------------------|
| <b>Population (2016) – 9611 people</b> |                      |                      |                        |
| <b>Open Space Rates</b>                | <b>Required (ha)</b> | <b>Existing (ha)</b> | <b>Difference (ha)</b> |
| DCP A5 – Active<br>1.7ha/1000          | 16.33ha              | 13.4ha               | -2.93ha                |
| DCP A5 – Passive<br>1.13ha/1000        | 10.86ha              | 47.93ha              | +37.07ha               |
| <b>Total Open Space Required</b>       | <b>27.19ha</b>       |                      |                        |
| <b>Total Open Space Existing</b>       | <b>61.33ha</b>       |                      |                        |
| <b>Difference</b>                      | <b>+34.14ha</b>      |                      |                        |

| <b>Kingscliff – Projected</b>            |                      |   |                        |
|--|----------------------|---|------------------------|
| <b>Population (2036) – 10 998 people</b> |                      |   |                        |
| <b>Open Space Rates</b>                  | <b>Required (ha)</b> | <b>Existing/Indicative Master Plan (ha)</b> | <b>Difference (ha)</b> |
| DCP A5 – Active<br>1.7ha/1000            | 18.69ha              | 22.9ha                                      | +4.21ha                |
| DCP A5 – Passive<br>1.13ha/1000          | 12.4ha               | 52ha  | +39.6ha                |
| District Park                            | 2ha                  | 2ha   | 0                      |
| <b>Total Open Space Required</b>         | <b>31.09ha</b>       |   |                        |
| <b>Total Open Space Provided</b>         | <b>76.9ha</b>        |   |                        |
| <b>Difference</b>                        | <b>+45.81ha</b>      |   |                        |

| <b>Kingscliff – Combined existing and Greenfield Development Sites</b> |                      |   |                        |
|--|----------------------|---|------------------------|
| <b>Population (2036) – 14 000 people</b>                               |                      |   |                        |
| <b>Open Space Rates</b>  | <b>Required (ha)</b> | <b>Existing/Indicative Master Plan (ha)</b> | <b>Difference (ha)</b> |
| DCP A5 – Active<br>1.7ha/1000  | 23.8ha               | 22.9ha                                      | -0.9ha                 |
| DCP A5 – Passive<br>1.13ha/1000  | 15.82ha              | 52ha  | +37.11ha               |
| District Park  | 2ha                  | 2ha   | 0                      |
| <b>Total Open Space Required</b>                                       | <b>41.62ha</b>       |   |                        |
| <b>Total Open Space Provided</b>                                       | <b>76.9ha</b>        |   |                        |
| <b>Difference</b>  | <b>+35.28ha</b>      |   |                        |

**Table 1 Existing and Greenfield Open Space Provision** – Combined existing and potential new greenfield open space area to 2036 based on both population rate increases (source: profile id) and population increases based off indicative greenfield development structure plans. Areas derived from indicative structure plan illustrated within this DCP and thereby will be subject to variation as part of more detailed structure and master planning processes over key greenfield development sites. The high rate of passive open space relates to existing extensive areas of coastal reserves.



### 2.7.3 Development Controls:

- C1. Any planning proposal, master plan, concept development application and subdivision application over the identified greenfield development sites must submit:
- Open Space and Landscape Plan
- C1. Active and passive open space is to be provided as detailed within Figure 2.12 specifically:
- 9.5ha (gross) as active open space and district park (min. 2ha) within the Business and Knowledge Precinct.
  - 1ha (gross) passive open space within the West Kingscliff Precinct.
  - 1.8ha (gross) passive open space within the Turnock Street Precinct.
  - Consideration of future active open space over the lands west of the Tweed Coast Road which may be required to meet district open space requirements will be considered at the cessation of sand mining operations.
  - Provide open space within 500m of all residents living in medium and higher density housing (a walkable distance) without having to cross a major road or other physical barrier.
- C2. In lieu of providing additional passive open space in new greenfield sites where there is a demonstrated surplus (based on population yields against DCP A5 open space rates and 500m walkable proximity to existing passive open space) there is opportunity to vary DCP A5 passive open space requirements. This could include the development or provision of monetary contributing via a voluntary planning agreement in consultation with Council to the following or other agreed open space projects:
- Embellishment of a north-south passive movement corridor (pedestrian and cycle) adjoining the existing drainage corridor across the Business and Knowledge, North Kingscliff and West Kingscliff Precincts.
  - Embellishment or establishment of connecting pedestrian and cycle paths between open space areas.
  - Development of a family and youth precinct (including skate park) within an agreed location and in consultation with the community.
  - Public domain embellishment projects.
  - Embellishment of existing passive open spaces.
- C3. Detail design of each of open space areas including site context information and details of embellishments including lighting, viewing platforms, park furniture, landscaping, play equipment, shelters, bbqs and picnic areas are to be lodged with applications that include open space land.
- C4. Subdivision design shall integrate walking and cycling paths connecting to the key open space areas, residential precincts with the village centre and surrounding residential areas.

### 2.7.4 Additional advice:

The above controls are to be read in conjunction with the criteria and provisions set out within Tweed DCP Section A5 – Subdivision Manual and the Open Space Strategy 2019-2029 where this plan does not make specific reference to a control or open space quantity covering the same matter.

Refer to Sports Field Strategy 2015.

Refer to Kingscliff Sport and Recreation Complex Master Plan.

Refer to Open Space Strategy 2019-2029.

Refer Contribution Plan Section 7.11 – CP07 – West Kingscliff.

## 2.8 Dwelling and Allotment Mix

### 2.8.1 Preamble

#### Statement of Intent

*Greenfield development sites in Kingscliff will deliver a diverse range of housing types to appeal to a broad demographic and socio-economic profile with a strong focus on responding to the local character and subtropical climatic context. Greenfield development precincts will strengthen the urban structure of Kingscliff by increasing density within and around the activity centres, increase opportunity for low rise medium density housing as well as increasing the supply of suburban allotments, particularly where greenfield development site adjoining existing low density precincts.*

#### Urban Structure and Lot Sizes

A range of lot sizes and housing types across Kingscliff's greenfield development sites are to reflect the broader urban structure strategy (Figure 2.13) including:

- Larger development allotments to facilitate retail and commercial development as well as shop top housing and tourist accommodation within and adjoining the existing town centre (Town centre and Turnock St Precinct).
- Larger development lots interfacing Turnock Street and potentially the Turnock Street extension to facilitate residential flat building development.
- Providing a range of allotment sizes to facilitate the development of low rise medium density housing typologies including dual occupancies, multi-unit development, row houses, terrace houses, manor houses courtyard houses, seniors/aged care housing and small lot housing within walkable proximity of the town centre (Turnock St Precinct).
- Providing a mix of residential accommodation associated with the Business and Knowledge precinct which may include large allotments for residential flat buildings, shop top housing, tourist accommodation and student housing as well as allotments to facilitate both medium density and low density housing types where appropriate flood evacuation routes can be provided.
- Increasing the supply of suburban allotments providing lower density opportunities across infill and interface greenfield areas (North Kingscliff, West Kingscliff, Turnock St and Cudgen precincts).

#### Diversity of Housing Choice

In consideration of the existing and projected population age structures and household composition, greenfield development will need to respond to future housing needs. It is anticipated that there will be an increasing demand for housing for single and couple households, first home owners, down sizing elderly residents, seniors housing and dedicated aged care with aligned health services. The development of the regional Tweed Valley Hospital will also increase the need for short-term as well as long term accommodation to accommodate patients, families, employees and students.

To meet these future housing needs there needs to be a strong focus on promoting diverse housing types within the new greenfield development areas. More diverse housing types will include better designed residential flat buildings, shop top housing units, tourist accommodation, senior living and aged care housing, small lot housing, low rise medium density housing types (terraces, row housing, manor houses) as well as live /work housing types. The provision of additional one and two bedroom housing options for singles and couples, and integration of universal and flexible housing design principles will be important to meet the localities diverse demographic sectors.

It is imperative that appropriate housing types and density are planned within greenfield sites commensurate to a sites proximity and access to activity centres services. This means higher density is planned closer to the town centre and planned business and knowledge precinct with low rise medium density and lower density housing types further from the centres typically adjoining existing areas of low density. Improving access and connectivity between residential precincts, activity centres and open space are also key master planning and subdivision design considerations.

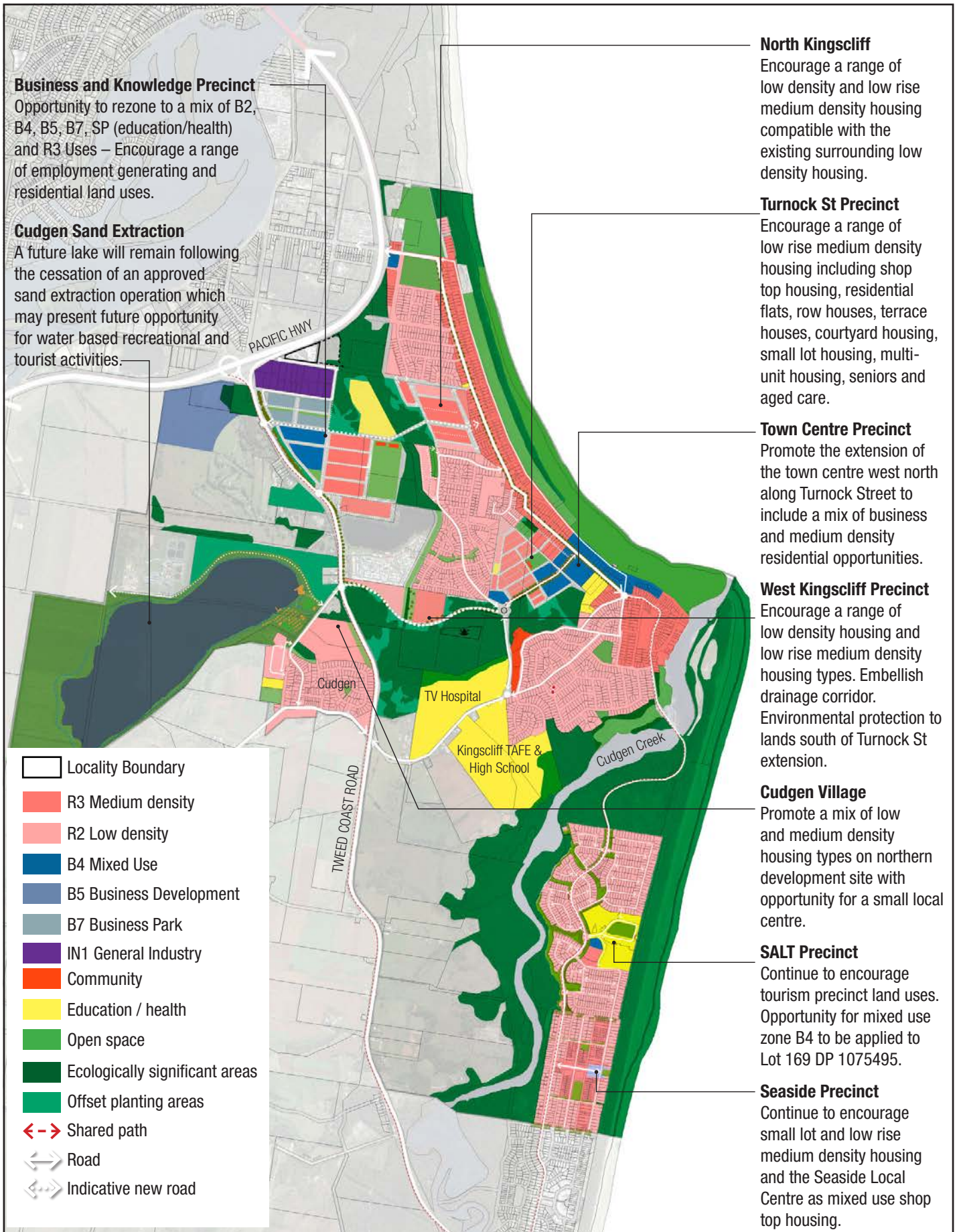


Figure 2.13 Key Kingscliff Locality Housing Opportunities

| Kingscliff Locality Greenfield Development Sites    |                                       |   |   |          |                            |
|---|---------------------------------------|---|---|----------|----------------------------|
| Greenfield Site                                     | Greenfield Developable footprint area | Approximate Dwellings/Population <sup>1</sup> | Indicative Land Use/Housing type  | Net Area | Target/Yield               |
| Turnock Street                                      | 19.78ha                               | 627 dw<br>1144 people                         | Shop top residential (12.2m)<br>(Density@ 30dw/ha)  | 3.84ha   | 115 dw<br>207 people       |
|   |                                       |   | Residential flat buildings (12.2m)<br>(Density@ 80dw/ha)  | 5.12ha   | 411dw<br>740 people        |
|   |                                       |   | Small lot and low rise medium density (9.0m),<br>Lots between 250 – 450m <sup>2</sup><br>Medium density @ 30dw/ha | 2.5ha    | 75 dw<br>135 people        |
|   |                                       |   | Low density residential<br>(450 – 600sqm) lots @ 16 dph)  | 1.68ha   | 26 dw<br>62 people         |
| West Kingscliff                                     | 8.78ha                                | 368 dw<br>662 people                          | Residential flat buildings (12.2m)<br>(Density@ 80dw/ha)  | 3.62ha   | 290 units<br>522 people    |
|   |                                       |   | Small lot and low rise medium density (9.0m),<br>Lots between 250 – 450m <sup>2</sup><br>Medium density @ 30dw/ha | 2.59ha   | 78 dw/units<br>140 people  |
| North Kingscliff                                    | 13.96ha                               | 400 dw<br>795 people                          | Small lot and low rise medium density (9.0m),<br>Lots between 250 – 450m <sup>2</sup><br>Medium density @ 30dw/ha | 7.66ha   | 230 dw/units<br>414 people |
|   |                                       |   | Low density residential<br>(450 – 600sqm) lots @ 16 dph)  | 4.57ha   | 73 dw<br>175 people        |
| Business and Knowledge Precinct <sup>2</sup>        | 76.65ha<br>(15.68 IN1)                | 592 dw<br>1094 people                         | Shop top residential (12.2m)<br>(Density@ 30dw/ha)  | 2 ha     | 60 units<br>108 people     |
|   |                                       |   | Residential flat buildings (12.2m)<br>(Density@ 80dw/ha)  | 4.4ha    | 352 units<br>633 people    |
|   |                                       |   | Small lot and low rise medium density (9.0m),<br>Lots between 250 – 450m <sup>2</sup><br>Medium density @ 30dw/ha | 4.4ha    | 132 dw/units<br>238 people |
|   |                                       |   | Low density residential<br>(450 – 600sqm) lots @ 16 dph)  | 3ha      | 48 dw<br>115 people        |
| Altona Road   | 10ha                                  | 144 dw<br>345 people                          | Low density residential<br>(450 – 600sqm) lots @ 16 dph)  | 9ha      | 144 dw<br>345 people       |
| Cudgen <sup>3</sup><br>(Currently being developed)  | 4.5ha                                 | 40 dw<br>94 people                            | Low density residential<br>(450 – 600sqm) lots @ 16 dph)  | 4.5ha    | 40 dw<br>94 people         |
| Seaside <sup>4</sup><br>(Currently being developed) | 32                                    | 409 dw<br>961 people                          | Small lot and low rise medium density (9.0m),<br>Lots between 250 – 450m <sup>2</sup>                             | 32       | 409 dw<br>961 people       |
| <b>Total</b>  | <b>165.67ha</b>                       | <b>2580 dw<br/>4065 people</b>                |   |          |                            |

1 Household occupancy assumes 2.4 persons per dwelling (Average occupants per household – North Coast-Kingscliff 2016 Source: profile id), 1.8 persons per unit.

2 Business and Knowledge combines a mix of industrial, business park, education campus, retail, open space and residential uses which may significantly vary depending on the final master plan outcome. 60% of site can be filled in accordance with the TFMP.

3 Based on DA13/0024

4 Allotments counted off TSC cadastre 2017.

**Table 2 Density and Housing/Allotment Type Targets** – Areas/density rates/targets in the above table have been derived from indicative structure plan illustrated within this DCP and are to be used as a benchmark as part of more detailed structure/master planning process over key greenfield development sites. As such the final density yields and targets for each site will vary in relation to the final structure plan/master plan.

### 2.8.2 Objectives:

1. Provide a planning framework which will deliver a diverse range of housing types to appeal to a broad demographic profile with a strong focus on subtropical subdivision and housing design, connectivity between residential precincts and embellished open spaces and public domain.
2. Strengthen the urban structure of Kingscliff by increasing density within and around the existing and future business centres, increase opportunity for medium density housing typologies as well as increasing supply of suburban allotments particularly adjoining existing residential precincts.
3. Promote opportunity for additional shop top residential and tourist accommodation within the town centre to further contribute to the range and diversity of housing and take advantage of access to retail goods and services as well as the coastal foreshore.
4. Facilitate a range of allotment sizes and scales of housing that will provide more housing affordability choice.

### 2.8.3 Development Controls:

- C1. Any planning proposal, master plan, concept development application and subdivision application over the identified greenfield development sites must submit:
  - Density and housing type plan.
  - Affordable housing strategy.
  - Architectural design guidelines.
- C2. Prepare a Density and Housing Type Framework Plan, including a structure plan and ancillary schedules of:
  - Differing allotment sizes including but not limited to development lots (greater than 1,200m<sup>2</sup>), integrated or large lots (greater than 800m<sup>2</sup>), suburban blocks (450-800m<sup>2</sup>), small lots (200-450m<sup>2</sup>).
  - Different proposed housing types which provide housing supply for a broad demographic and affordability profile including but not limited to shop top housing units, tourist accommodation units, residential flat buildings, integrated housing units, townhouses, row houses, plexes, detached dwellings and small lot houses.
- C3. Meet the density yield targets identified within this section of the Code as they apply to greenfield development sites. Where these yields cannot be met or exceeded, the justification for the departure or variation is required. Density and yield offsets around different parts of the precinct will be considered.
- C4. Prepare an affordable housing strategy over each of the identified greenfield development areas which allocates a proportion of housing types to specifically meet affordable housing need. The affordable housing strategy is to be prepared through co-ordinated negotiations between Council, the developer and affordable housing providers to set and meet agreed affordable housing benchmarks (housing type and quantity), procurement and management.
- C5. Architectural design guidelines should be formed as part of a subdivision application to provide design guidance to future developers and home builders and be reflective of the precinct desired future character. Design guidelines are to embody the principles of passive design as well as objectives, design principles and development controls within the residential section of this plan, other sections of the Tweed DCP and statutory requirements such as SEPP 65 and the apartment design guidelines.

## 2.9 Urban Design, Streetscape & Public Domain

### 2.9.1 Preamble

#### Statement of Intent

*The town centre, main street, overland flow paths and open space areas are to form the focus for public domain embellishments within the Kingscliff locality. The overriding strategy is to progressively implement a range of public domain and infrastructure improvements across existing and new greenfield development sites. These will address the key issues of pedestrian movement and comfort, providing areas of quality public domain reflective of local character, parking and efficient vehicle movement. This also improves to the overall character and appearance of the town and business centres as well as existing and future residential precincts.*

### 2.9.2 Objectives:

1. Use urban design, streetscape and public domain design to create 'village scaled' development precincts which are discrete with their own identity and character, but which are interconnected by a necklace of green spaces for environmental quality, recreation, walking, cycling and attractive tree lined streets.
2. Realise and retain key visual and landscape character components of the development site through a contemporary urban design, streetscape and public domain outcomes.
3. Public domain areas both within the town centre and residential areas are embellished to a quality standard capable of withstanding the marine environment as well as reinforcing the civic purpose, landscape and visual character of the locality.
4. Public domain areas and community facilities are to adapt principles of universal design to ensure safe and equitable access for all users.

### 2.9.3 Development Controls:

- C1. Any planning proposal, master plan, concept development application and subdivision application over the identified greenfield development sites must submit:
- Open space and landscape master plan.
- C2. Preparation of an open space and landscape master plan should including but not be limited to:
- A plan which nominates street tree planting, drainage corridors, buffer areas, passive open space and public domain areas. Street trees are to be nominated to relate to and strengthen the street pattern hierarchy. The street trees are to be positioned in a location where they are unlikely to conflict with the location of future driveways.
  - Integration of universal design and crime prevention through environmental design (CPTED) principles.
  - Application of Water Sensitive Urban Design (WSUD) principles to the streets, passive open space and drainage corridors where possible.
  - The inclusion of street plans and sections (one for each different street typology) illustrating the relationship between allotments (illustrate indicative front of buildings adjoining streets) and other street interfaces, verge and street tree planting, street lighting, pavement, footpaths and any other embellishments.
  - Incorporate universal design and CPTED principles in all public domain, open space and community infrastructure.
  - A schedule of finishes and materials which identifies key public domain improvement elements including but not limited to pavement, landscape, street lights, bollards, street furniture, bus shelters, shade structures. Materials and finishes should be of a quality standard capable of withstanding the marine environment.



**Town Centre Public Domain** – Public domain within the extended town centre greenfield development sites requires a high level of public domain improvements.



**Turnock Street** – The greenfield development sites along Turnock Street have the opportunity to create a shaded tree lined public domain areas with a widened pedestrian footpath embellished with street furniture and areas for outdoor dining.



**Open Space Embellishment** – Public domain areas incorporated within areas of open space should include bbq and shelters in strategic locations within new greenfield residential parks and public domain areas.



**Water sensitive urban design** – Integrate water sensitive urban design outcomes into the street, verge and pathway design to increase opportunities for stormwater infiltration and provide additional shade.



**Business and Knowledge Precinct Public Domain** – Public domain improvements within a Business Park context could include a series of smaller interconnected landscaped public domain areas fostering strong pedestrian connectivity, water sensitive urban design (WSUD) and a high level of outdoor amenity for workers and residents alike.



## 2.10 Solar Orientation and Passive Design

### 2.10.1 Preamble

#### Statement of Intent

*Achieve an optimised orientation of lots in all new subdivisions for good solar outcomes when designing subdivisions to maximise energy efficiency and integration of passive design principles when designing buildings.*

#### Maximise Energy Efficiency

Subdivision design and buildings are to be responsive to the climatic conditions of the site including solar path, solar exposure/shading, prevailing breezes, appropriate materials, internal and external living area configurations. Recognised solar orientation guidelines advocate the best orientation range within 20 and 30 degrees of a N-S or E-W (Amcord, 1997, p275).

Within a new greenfield subdivision allotment orientation is directly related to road structure planning. In this context regular rectilinear shaped allotments, formed by north-south and east west streets in a predominantly grided arrangement provide the best opportunity to design and build a house on. Housing allotments with an east-west lot orientation generally provide a good solar design opportunities with the northerly aspect aligned with the long boundary along which living and external living spaces are best located. North-south lot orientation result in allotments on the north side of a street having good solar access to the rear yard and allotments on the south side of a street having good solar access in the front yard. In all cases the configuration of habitable spaces with generally north facing living spaces, decks, courtyards and highlight windows all provide opportunity to draw natural light into the floor plan.

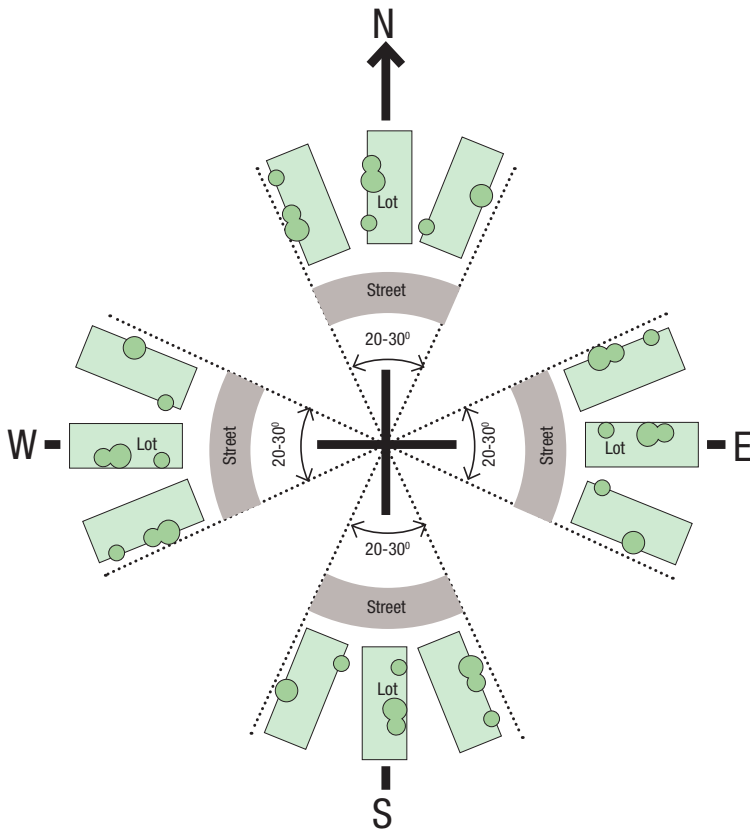
### 2.10.2 Objectives:

1. Encourage subdivision design which maximises opportunities for good solar orientation and access to prevailing breezes in terms of subdivision design, street layout and lot configuration.
2. To facilitate opportunities for future buildings integrate passive design principles and respond to the natural environment and climatic condition through optimising solar allotment orientation over new subdivisions.
3. Take advantage of the relatively flat greenfield development sites to utilise a grided road structure which will optimise a north-south and east-west allotment configuration.
4. Avoid curvilinear and cul-de-sac urban structures which result in a lower proportion of allotment with optimised solar orientation and results in awkward shaped allotments.

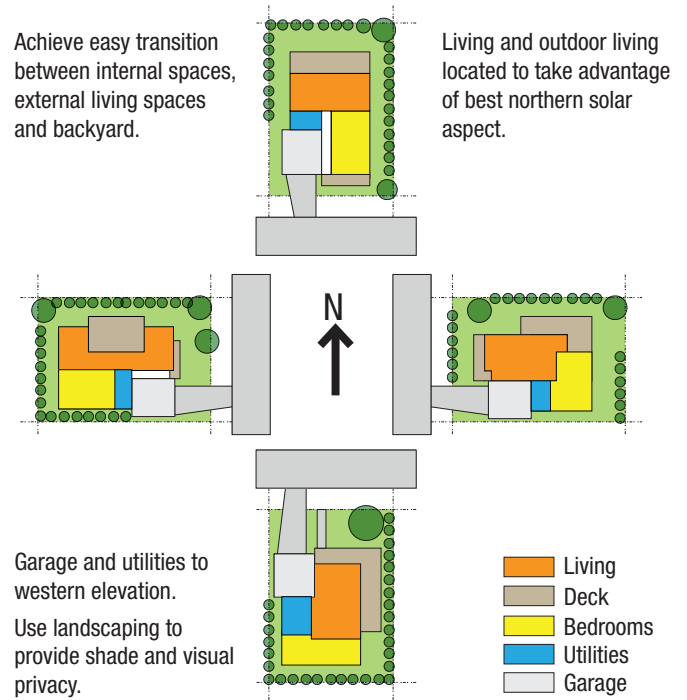
### 2.10.3 Development Controls:

- C1. Any planning proposal, master plan, concept development application and subdivision application over the identified greenfield development sites must submit:
  - Solar orientation plan and schedule of allotments with a breakdown of solar orientation.
- C2. Demonstrate by way of diagrams and or plans how a minimum of 75% of all new lots to meet the optimum solar orientation between 20-30 degrees of N/S or E/W and prevailing breezes have been considered in relation to orientation, size, width and depth to achieve optimised building blocks.
- C3. The urban structure of any subdivision development application shall avoid cul-de-sacs and road alignments which result in irregular shaped lots difficult to build on.
- C4. A diagrammatic site analysis is to be produced for each individual allotment prior to the release of a subdivision certificate which identifies contributing climatic and environmental factors (solar path, prevailing winds, key view lines) and optimised floor plan diagram illustrating habitable living, external living, non-habitable, utility and garage zones for consideration by future owner/occupants.

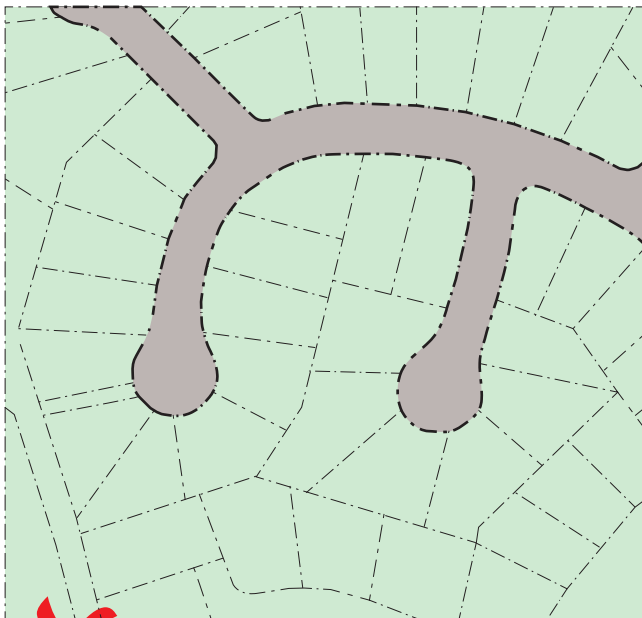




Site Orientation diagram - best orientation range within 20 and 30 degrees of a N-S or E-W



Building orientation- New buildings are to be responsive to the climatic conditions of the site including solar path, solar exposure/shading, prevailing breezes, appropriate materials, internal and external living area configurations.



Avoid curvilinear and cul-de-sac urban structures which result in a lower proportion of allotments with optimised solar orientation and lead to awkward shaped allotments which are harder to design to and build on.



Grided road network results in rectilinear allotments with favourable north-south or east-west orientation which are easier to design to and build on.

Figure 2.14 Passive Solar Design for Subdivision

## 2.11 Design Principle 11: Hazards and Resilience

### 2.11.1 Preamble

#### Statement of Intent

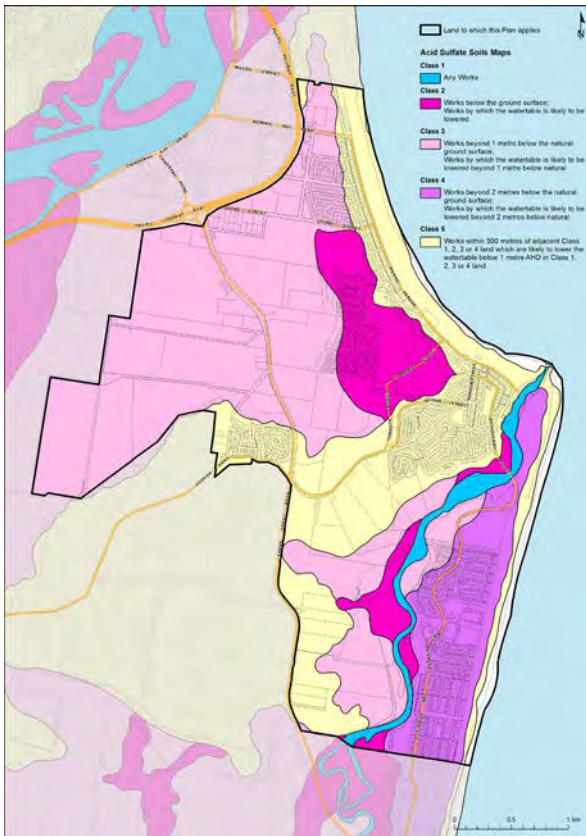
*All new greenfield development sites are to be appropriately designed to mitigate and manage various site constraints including flooding, stormwater runoff, land slip, acid sulfate soils and other hazards to ensure future resilience.*

### 2.11.2 Objectives:

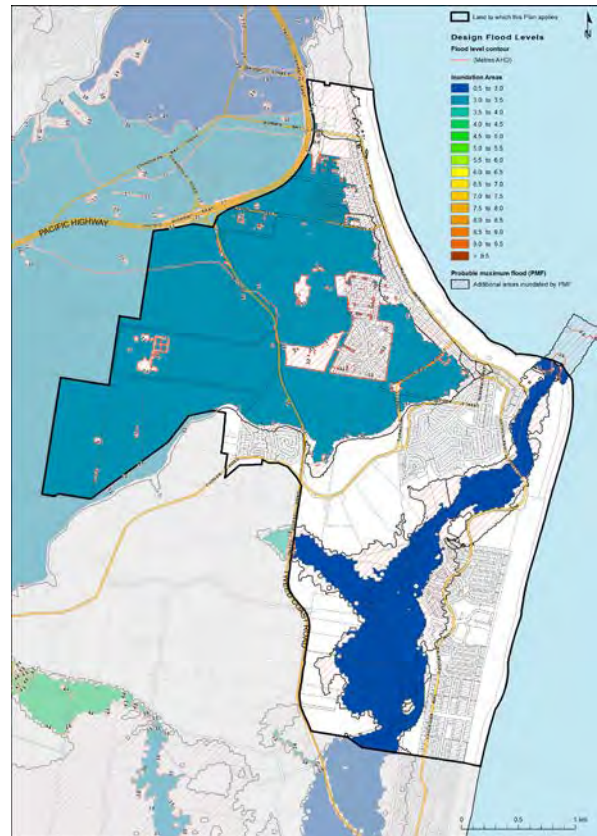
1. Provide only suitable development and land uses within land identified as affected by a Probable Maximum Flood.
2. Ensure that development is appropriately designed to accommodate for potential climate change impacts.
3. Ensure that any soil contamination is identified and suitably mitigated prior to the development of greenfield development sites.
4. Minimise the disturbance of acid sulfate soils.
5. Provide a subdivision layout that responds to and manages bushfire hazards.
6. Incorporate design elements and urban buffers, such as lot size and orientation, perimeter roads or overland drainage reserves, to enable the maintenance of existing vegetation and provide adequate separation of residential land uses from any hazard.
7. Ensure areas identified as 'currently unsuitable' are excluded from development for urban purposes or other purposes that are sensitive to soil stability.

### 2.11.3 Development Controls:

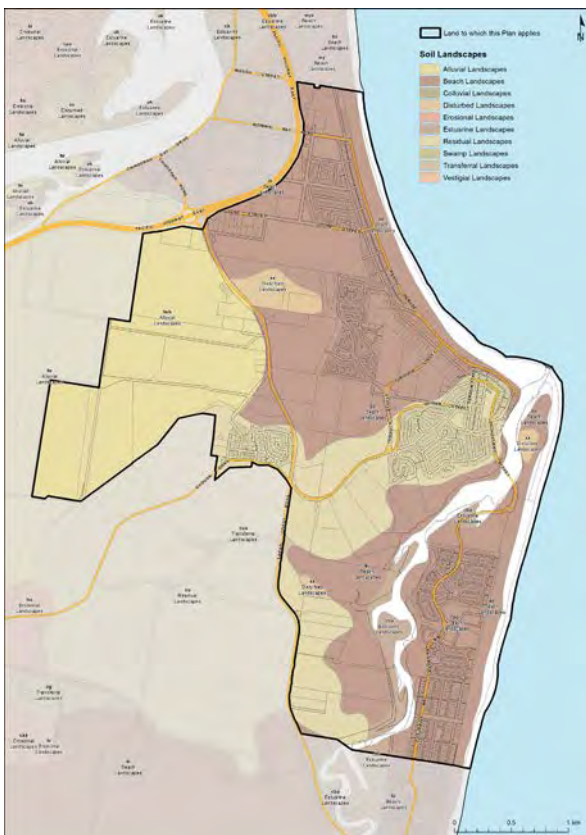
- C1. The following information is to be submitted with any planning proposal, master plan, concept development application and subdivision application over the identified greenfield development sites:
  - Demonstrated compliance with the provisions of the Tweed DCP A3 – Development of Flood Liable Land and Tweed Valley Floodplain Risk Management Study and Plan.
  - Detail of all site investigations (including underground and site boring) to provide adequate information to prepare designs and assess construction methods.
  - Detail of all necessary geotechnical investigation and analysis to ensure that the subdivision and all works associated with the subdivision are stable and will not be subject to subsidence, landslip, mass movement or significant erosion in the short and long term.
  - A Site Audit Statement (SAS) certifying the land is suitable for the proposed use. The SAS is to be prepared by an Environmental Protection Agency Accredited Contaminated Site Auditor under the provisions of the Contaminated Land Management Act, who is to be engaged to oversee the contamination investigation and any necessary remediation of the site.
  - On greenfield development sites adjoining the Kingscliff drain. In consultation with Council undertake studies to further understand the mechanisms for existing acid sulfate soils discharge events that result in significant iron floc discharges within the Kingscliff drain and the Tweed River. Further studies will be used to inform remedial actions and future works required to address chronic ASS issues in the Kingscliff Drain.
- C2. Required Asset Protection Zones must not be provided on public land (with the exception of roads) and are to be incorporated within development allotments.



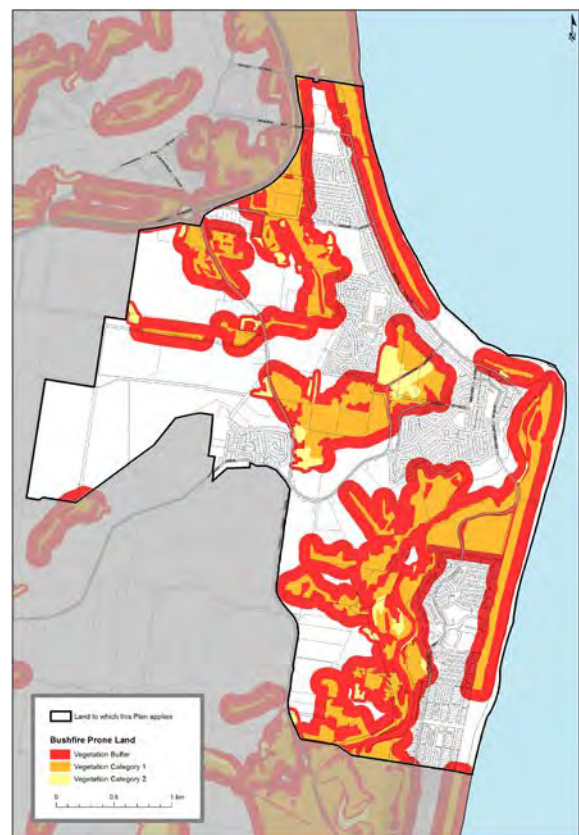
Acid Sulfate Soils



Flooding



Soil Stability



Bushfire Prone

Figure 2.15 Hazards and Resilience Constraints

## 2.12 Design Principle 12: Utilities

### 2.12.1 Preamble

#### Vision

*Progressively implement the design and construction of essential services for the Kingscliff locality and ensure the coordinated and efficient delivery of infrastructure across identified greenfield development sites.*

#### Infrastructure Planning

The future development potential of the identified urban release and employment generating lands combined with substantial infill development opportunities within Kingscliff and Cudgen will place an increased capacity on existing water sewer, telecommunication and electrical infrastructure. Development scenario's and expected population yields need to be considered in relation to the future planning of these infrastructure services as part of the master planning and subdivision design process.

The efficient delivery, roll out and considered integrated design of additional required infrastructure needs to be a key consideration within the early design phases of the greenfield development sites. In this regard, Council must consider how to most effectively service the area and provide for future development needs within Council's budget and works program, integrated with appropriate developer participation and contribution.

#### Water Infrastructure

The indicative water reticulation system is illustrated in Figure 2.16. The Kingscliff and Cudgen localities fall within five water zones including Duranbah, Cudgen, Cudgen Booster (Cudgen Village), Kingscliff and Kingscliff Booster (Kingscliff Hill).

The water supply to the Kingscliff District is sourced from the Bray Park Water Treatment Plant (WTP) and pumped to Hospital Hill Reservoirs in Murwillumbah before transported by different trunk main systems. The area north of Cudgen Creek receives water from the trunk main located along Tweed Valley Way which reticulates to a pumping station in Chinderah where it is pumped into the Kingscliff Hill Reservoirs then gravity fed to the urban area. The area south of Cudgen Creek receives its water from the trunk main located along Environ Road supplying the Duranbah Reservoirs, where it is gravity fed north to service Salt, Seaside City, Casuarina and south to other regions.

Future extra storage capacity will be required at the Duranbah Reservoir site and additional transfer capacity (ie: trunk mains) to cater for additional populations forecast to the south of Cudgen Creek. Additional storage capacity is not planned for future populations in Kingscliff to the north of Cudgen Creek, however additional transfer capacity is planned. Opportunities for water infrastructure expansion include the construction of a trunk main to connect the areas north and south of Cudgen Creek, which will provide an additional source of water during main breaks.

#### Wastewater Infrastructure

The indicative sewerage system is illustrated in Figure 2.16. The new Waste Water Treatment Plant (WWTP) was established to the west of the township off Altona Road which has a capacity to treat 6 megalitres per day, equivalent to 25,000 people. The WWTP which services Kingscliff, Fingal, Chinderah, Cudgen, South Kingscliff (Salt) and Casuarina utilises a chemically enhanced biological nutrient removal (CEBNR) process results in a high quality of the discharged effluent. A small percentage of the A Class treated effluent is reused for irrigation purposes at the nearby Golf Course. The remaining treated effluent is discharged into the Tweed River.

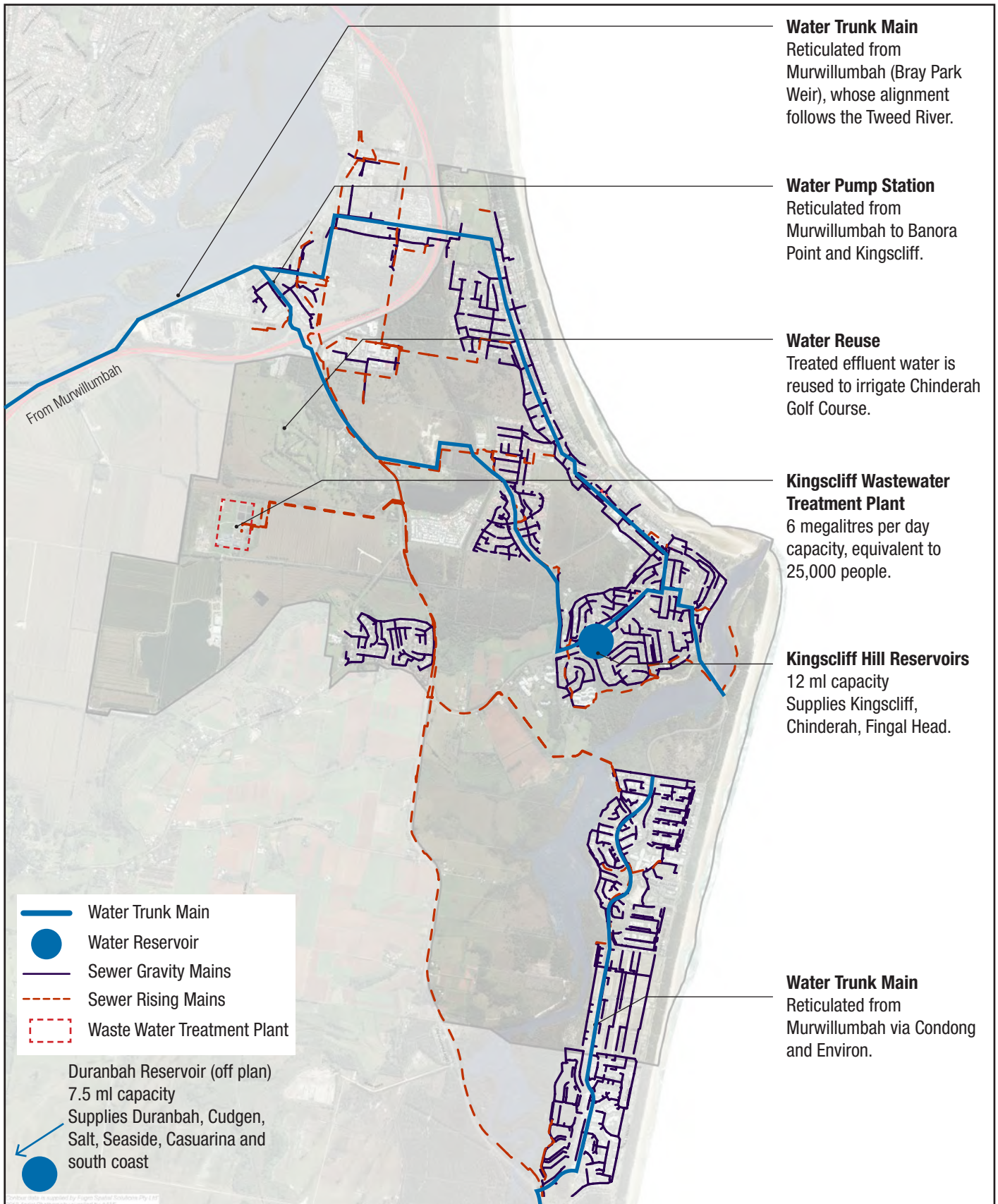


Figure 2.16 Kingscliff Water and Sewer Infrastructure

### **Stormwater Infrastructure**

Given the significant existing and future development in and around the study area, there is a need to update the Kingscliff Catchment and Drainage Management Plan (jointly by Council and stakeholder landowner) and investigate alternate options in terms of the longer term stormwater drainage infrastructure requirements, water treatment and management strategies. This may include the implementation of a wider north-south drainage corridor which would have the benefit of restoring a marine vegetative environment to treat receiving water, addressing potential acid sulfate soil issues as well as providing a linear open space area and passive movement corridor. It is particularly important at the master planning level to implement the principles of water sensitive urban design and onsite water treatment and reuse systems over future development sites.

### **Electrical Infrastructure**

Essential Energy is a NSW Government-owned corporation, with Delivery Partners for building, operating and maintaining the electricity network with numerous service providers facilitating individual connections. There are currently no planned service upgrades programmed for the Kingscliff locality in the immediate future. The further extension of electrical infrastructure as part of a staged release of future development sites will be designed and planned as part of the development approvals process.

### **Existing telecommunication and NBN Infrastructure**

Once a Government-owned corporation, Telstra which owns, builds, maintains and operates telecommunications networks is now a public company. This network includes Kingscliff's predominantly above ground pole infrastructure. Since the privatisation of Telstra and the deregulation of telecommunications service there are then numerous telecommunication and internet service providers facilitating individual connections.

The National Broadband Network (NBN) is a national open-access data network which is under development and roll out across Australia. It is based on the premise that fixed line and wireless broadband connections are sold to retail service providers (RSP), who then sell Internet access and other services to consumers. Currently, the NBN Co is planning to acquire and build up to 20 fixed wireless facilities across the Tweed Shire including Kingscliff. The fixed wireless system uses cellular technology to transmit radio signals to and from a small antenna fixed on the outside of a home or business, which is pointed directly towards the fixed wireless facility (antenna).

### **Service Infrastructure Issues**

The ultimate capacity will be to provide for a district population catchment of 50,000 people which will include the Kings Forest release area. The existing water and sewer systems will require increased capacities and staged future planning and upgrades to meet this projected population. In addition to new infrastructure servicing new release areas, the existing water and sewer infrastructure, particularly within older residential estates will also require staged upgrading and improvement to ensure efficient services capable of meeting increased demand.

### **Infrastructure Opportunities**

- The construction of a trunk water main to connect the areas north and south of Cudgen Creek, which will provide an additional source of water during main breaks. Opportunity to increase water supply capacity at Duranbah Reservoir and to meet future population needs.
- Opportunity to for developers to incorporate demand management into their developments, such as third pipe and other reuse strategies, to reduce water demand.
- Opportunity to expand the existing Kingscliff Wastewater Treatment Plant to meet future population needs.
- Opportunity for appropriate sites to reuse the Kingscliff Wastewater Treatment Plant A Class water and biosolids.
- Greenfield development sites to incorporate NBN ready infrastructure.
- Undertake an integrated stormwater drainage strategy which takes into account existing drainage regime and capacity and future need based on identified greenfield development sites.

### 2.12.2 Objectives:

1. Progressively implement the design and construction of essential services for Kingscliff's' existing settlement areas and greenfield development sites and ensure coordinated and efficient delivery.
2. Preserve existing catchment boundaries and utilise existing water courses and gully lines for conveyance where practical and environmentally sustainable.
3. Provision of minor and major stormwater collection and conveyance systems for the development land.
4. Provision of stormwater quality control devices to meet Council's stormwater quality objectives in accordance with Development Design Specification D7 – Stormwater Quality.
5. Provision of stormwater detention/retention devices and level spreaders to ensure that stormwater discharge from the development does not create significant adverse impacts on receiving water bodies, wetlands and environmental land.

### 2.12.3 Development Controls:

- C1. The following information is to be submitted with any Development Application for subdivision:
  - Water Servicing Plan.
  - Sewer Servicing Plan.
  - Stormwater Management Plan.
  - Erosion and Sediment Control Plan (ESCP).
- C2. Any proposal must comply with the Demand Management Strategy adopted by Council employing minimum sized rainwater tanks and connected roof areas as well as reduced infiltration gravity sewers and other measures to reduce demand on water supply and load on wastewater systems.
- C3. Land affected by potential water infrastructure, as depicted in Figure 2.16, shall not be used for any other purpose than for water supply infrastructure unless Council specifically determines that the land is no longer required for that purpose.
- C4. A stormwater management plan is to accompany all greenfield development which will take account of potential development scenarios and implement a more holistic and water sensitive urban design approach to managing stormwater drainage. This would include revisiting the preferred design of key drainage corridors particularly in dealing with the existing north-south drain and potential acid sulfate soil issues.
- C5. Demonstrate the location of a fibre ready, pit and pipe network (including trenching, design and third party certification) to NBN CO's Specifications, to allow for the installation of Fibre To The Home (FTTH) broadband services.
- C6. Demonstrate existing or propose new lawful point(s) of discharge from greenfield development sites into adjoining natural areas or receiving stormwater drains.

### 2.12.4 Additional advice:

Refer to the Tweed DCP A5 – Subdivision Manual.

## 2.13 Key Greenfield Site – Turnock Street Precinct

### 2.13.1 Character Statement

The Turnock Street Precinct will accommodate opportunities for town centre expansion as well as a mix of medium density housing types to take advantage of the walkable proximity to the existing town centre and coastal foreshore. The tree lined streetscape of Turnock Street will be befitting of its town centre gateway location and include pedestrian and cycle paths along its length. The protection of ecologically significant areas to the south of Turnock Street will link with lands further to the west strengthening the forming a 'green heart' to the locality as well as providing nature based passive recreation opportunities. The key planning and design opportunities include:

- Expansion of the town centre uses west along Turnock Street incorporating active ground floor retail/commercial uses adjoining the Kingscliff Shopping Village site and shop top housing mix uses extending further west along Turnock Street.
- New mixed use landuses fronting Turnock Street to have a public domain interface with the street including widened footpath, areas for outdoor dining and street trees.
- Encourage a mix of residential flat buildings fronting Turnock Street and low rise medium density housing opportunities across the northern portion of the precinct to take advantage of the sites flat topography and good walking proximity to the existing town centre.
- Design Turnock Street as a well landscaped visually attractive connector street integrating traffic movement, cycle and pedestrian paths, parking, shade, lighting and WSUD befitting of the primary access road from Tweed Coast Road into the Kingscliff township.
- Protection of areas identified as having high ecological significance and areas for offset planting.

### 2.13.2 Planning and Design Principles:

- P1. Undertake master planning process to underpin any future planning proposal, concept development application or subdivision development application(s) as required to achieve a balance of character defining town centre built form and public domain, medium density residential housing, network of open space, strong passive movement links (pedestrian and cycling) and areas of environmental protection. The master plan process should:
- Facilitate town centre staged growth and expansion along Turnock Street to include an expanded range of retail, commercial, community, open space, tourist and shop top residential uses through a B4 Mixed Use zoning with building height to 13.6 metres (post bulk earth works fill).
  - Facilitate residential flat building development immediately west of the town centre expansion area along Turnock Street through a R3 zoning with a building height to 12.2 metres (post bulk earth works fill).
  - Facilitate a mix of housing opportunities including residential flat buildings, low rise medium density housing, live work housing, affordable and aged housing to take advantage of the flat site and good proximity to the existing town centre with building heights to 12.2 metres (post bulk earth works fill). Land use and density targets based on the Turnock Street indicative structure plan (refer Table 3).
- P2. Reinforce Turnock Street as the principle connector road which will ultimately link the Tweed Coast Road with the Kingscliff township by:
- Designing Turnock Street as a tree lined boulevard to provide a high level of visual and user amenity.
  - Co-ordinated access to avoid multiple driveway and or building access points off Turnock Street.
  - Inclusion of a dedicated pedestrian and cycle lane linking areas of west Kingscliff with the town centre with shade, seating, lighting and water points at regular intervals.
  - Providing public domain interface for new town centre buildings fronting Turnock Street.
  - Integrated street lighting, parking, landscape bays and WSUD streetscape elements.



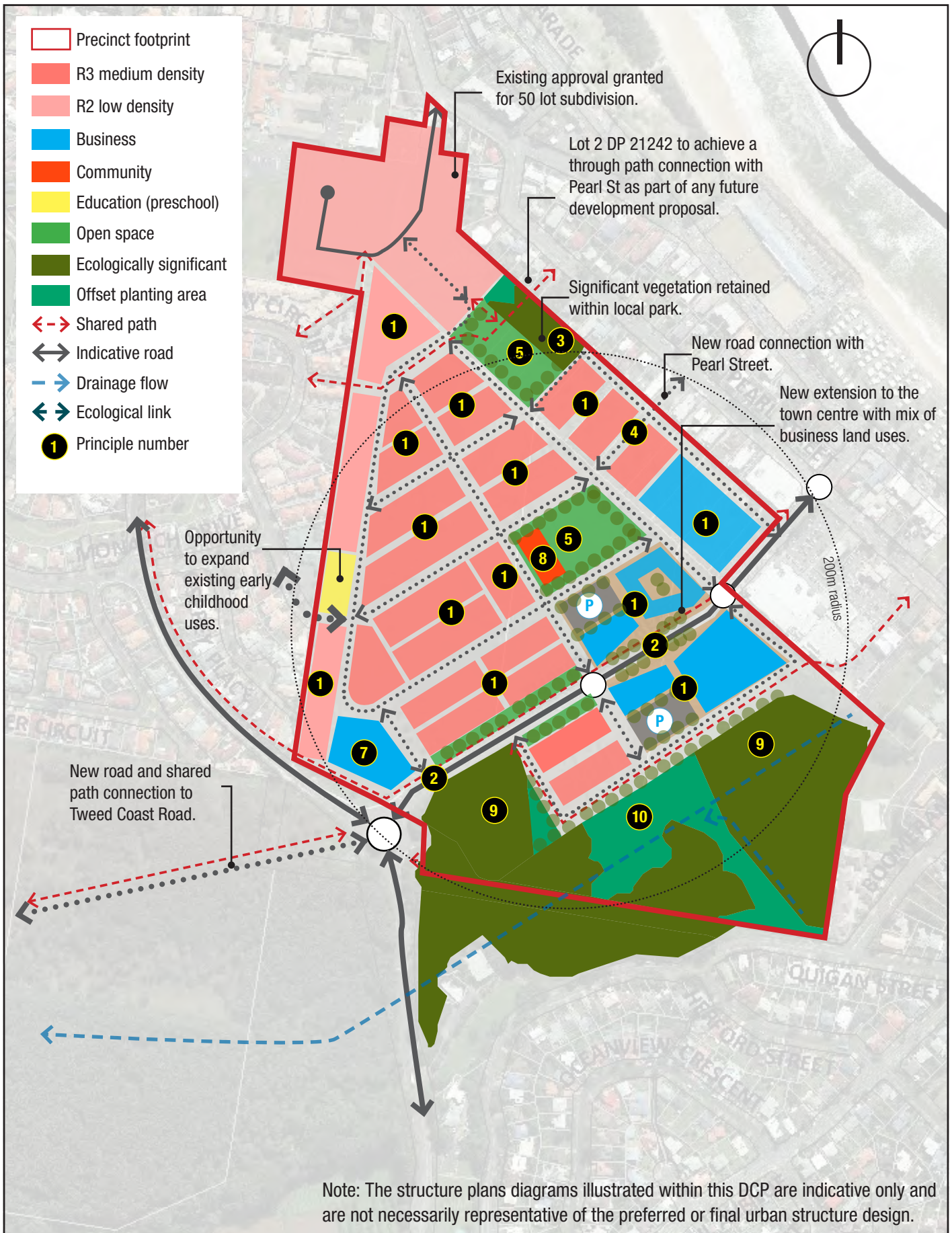


Figure 2.17 Indicative Turnock St Precinct Structure Plan

- P3. Retain the identified Endangered Ecological Community (EEC) within the northern portion of the precinct as an environmental zone set within an area of public open space RE1. Within this area of RE1 create a pedestrian connection which has the capacity to link through to Pearl Street across Lot 2 DP 21242 to achieve a through path connection with Pearl St as part of any future development proposal.
- P4. Utilise Lot 6 DP 21242 as a road reserve providing access from Pearl Street into the development site.
- P5. Development of local and neighbourhood parks within this precinct to cater for the passive recreational needs of future residents. At least 50% of the park perimeter is to have direct road frontage to encourage universal accessibility and visibility. The park should be landscaped and embellished to achieve a high level of user amenity and be a defining element of the future character of the precinct.
- P6. Integrate a pedestrian and cycle path network throughout the precinct and connect with surrounding existing and proposed residential areas and open space. The pathway network is to achieve user comfort and amenity in terms of co-location of street trees for shade, adequate lighting and regular waypoints with water points and seating.
- P7. Investigate opportunity to relocate town centre service station to a new site fronting the Turnock St roundabout which could be co-located with other retail tenancies or small scale commercial workspace.
- P8. In consultation with Council investigate opportunity within the Turnock Street Precinct for a new multi-purpose community building which could include a community centre, community meeting rooms, incubator workspace. Ideally, this building should be highly visible and accessible by adjoin either Turnock Street and/or the neighbourhood park (min. size 0.5ha). In addition there is a need for additional preschool and early childhood facilities within the locality.
- P9. Facilitate the protection and management of land identified as environmentally and/or ecologically important through appropriate land use zoning and provisions for ongoing habitat management.
- P10. Identify lands for offset planting and provisions for ongoing habitat restoration and management.



Turnock Street Precinct – Turnock Street will be a tree line boulevard with pedestrian and cycle pathways along its length. Land uses will be a combination of medium density housing types as well as opportunity for town centre expansion to the east.

**Turnock St Precinct – Land Use and Residential Density Targets**

| Land Use                    | Area (ha or sqm) | Building Height | Density Target (dph) | Dwelling/Unit No. | Projected Pop. (2.4 /1.8 per dw) |
|-----------------------------|------------------|-----------------|----------------------|-------------------|----------------------------------|
| R2 Low Density              | 1.68ha           | 9m              | 16                   | 26                | 62                               |
| R3 Medium density           | 5.12ha           | 12.2m           | 80                   | 411               | 740                              |
| R3 Medium density           | 2.5ha            | 9m              | 30                   | 75                | 135                              |
| B4 Mixed Use                | 3.85ha           | 12.2-13.6m      | 30                   | 115               | 207                              |
| Community Use               | 0.35ha           | 13.6m           | –                    | –                 | –                                |
| Open Space                  | 1.66 ha          | –               | –                    | –                 | –                                |
| Road Reserve/Infrastructure | 1 ha (25%)       | –               | –                    | –                 | –                                |
| <b>Total</b>                | <b>16.16ha</b>   | <b>9-13.6m</b>  | <b>16-80 dph</b>     | <b>627</b>        | <b>1144 persons</b>              |

**Open Space Required**

| Open Space Type | Rate        | Required | Existing | Structure Plan | Difference |
|-----------------|-------------|----------|----------|----------------|------------|
| Active          | 1.7ha/1000  | 1.86ha   | –        | –              | -1.86ha    |
| Passive         | 1.13ha/1000 | 1.03ha   | –        | 2 ha           | +0.97ha    |

Table 3 Turnock St Land Use and Residential Density Targets



Figure 2.18 Indicative Turnock Street Section – Mixed Use Interface

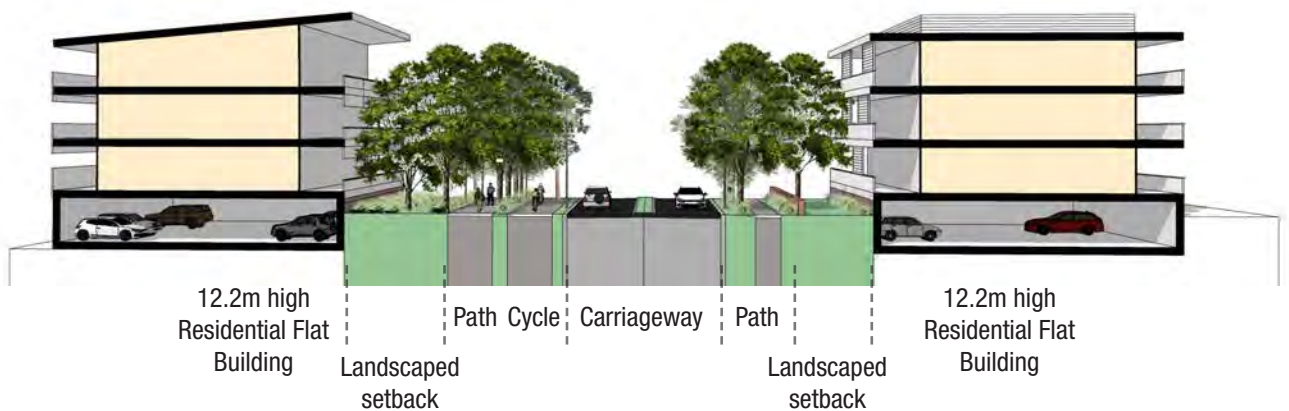


Figure 2.19 Indicative Turnock Street Section – Residential Interface

## 2.14 Key Greenfield Site – West Kingscliff Precinct

### 2.14.1 Character Statement

The greenfield development site within the West Kingscliff Precinct will be accessed from the Turnock Street (west) extension which will form the primary vehicular access from the Tweed Coast Road into the Kingscliff township. The greenfield development site presents an opportunity to develop a range of medium density, low rise medium and low density residential housing types with the southern development boundary defined by the Turnock St extension alignment and areas of environmental protection south of the road alignment. Well defined north-south and east-west strong passive movement links (pedestrian and cycling) will connect the greenfield development site to existing residential precinct areas to the immediate north and the Kingscliff township to the east. The protection of ecologically significant areas to the south of Turnock Street will link with lands further east strengthening the forming a 'green heart' to the locality as well as providing nature based passive recreation opportunities.

The key planning and design opportunities for this greenfield development site include:

- Construction of the Turnock Street extension which will form the new primary connector road and gateway from the Tweed Coast Road into the Kingscliff township to include a dedicated cycle and walking path.
- Maintaining a 'green edge' to Tweed Coast Road through the protection of environmentally sensitive vegetation which provides a strong ecological link through the precinct and separates the urban areas of West Kingscliff from Cudgen.
- Facilitate a mix of low rise medium density residential development heading west along the extended Turnock Street to take advantage of the close proximity to the existing centre and encouragement of higher densities along principle movement corridors.
- Pursue the embellishment of the north-south drainage corridor for combined drainage, vegetation, open space and passive movement (shared path) uses.

### 2.14.2 Planning and Design Principles:

- P1. Undertake a master planning process to underpin future planning proposal, concept development application or subdivision development application(s) as required to facilitate a range of low rise medium and low density residential housing types framed by the Turnock St extension and areas of environmental protection to the south. The master plan process should:
- Delineate areas of environmental protection from the developable portion of the site by the Turnock St (west) extension.
  - Integrate the development site with the existing West Kingscliff residential areas to the immediate north by way of the road network, pedestrian and cycling paths and compatible interface land use and housing types.
  - Integrate principles of passive subdivision design (solar orientation, prevailing breezes, landscape) into the designed outcomes and to generally provide a grided street network resulting in regularly shaped allotments with good solar and passive design opportunities for future buildings.
  - Integrate water sensitive urban design treatments and designed outcomes as part of the existing drainage corridor and overall subdivision design.
  - Address all potential flood impacts and mitigation strategies including design flood levels and flood evacuation routes east along Turnock St and up along Cudgen Road.
  - Embellishment of the north south drainage corridor for combined drainage, vegetation, open space and passive movement (shared path) uses.
  - Develop a neighbourhood park adjoining the north-south drainage corridor embellished to meet the passive open space needs to the local residents resulting in a green edge to the precinct.
  - Identification of lands to be dedicated for on site compensatory planting as a result of any vegetation clearing which may be nominated for removal from part of the identified development site as part of the master plan process.

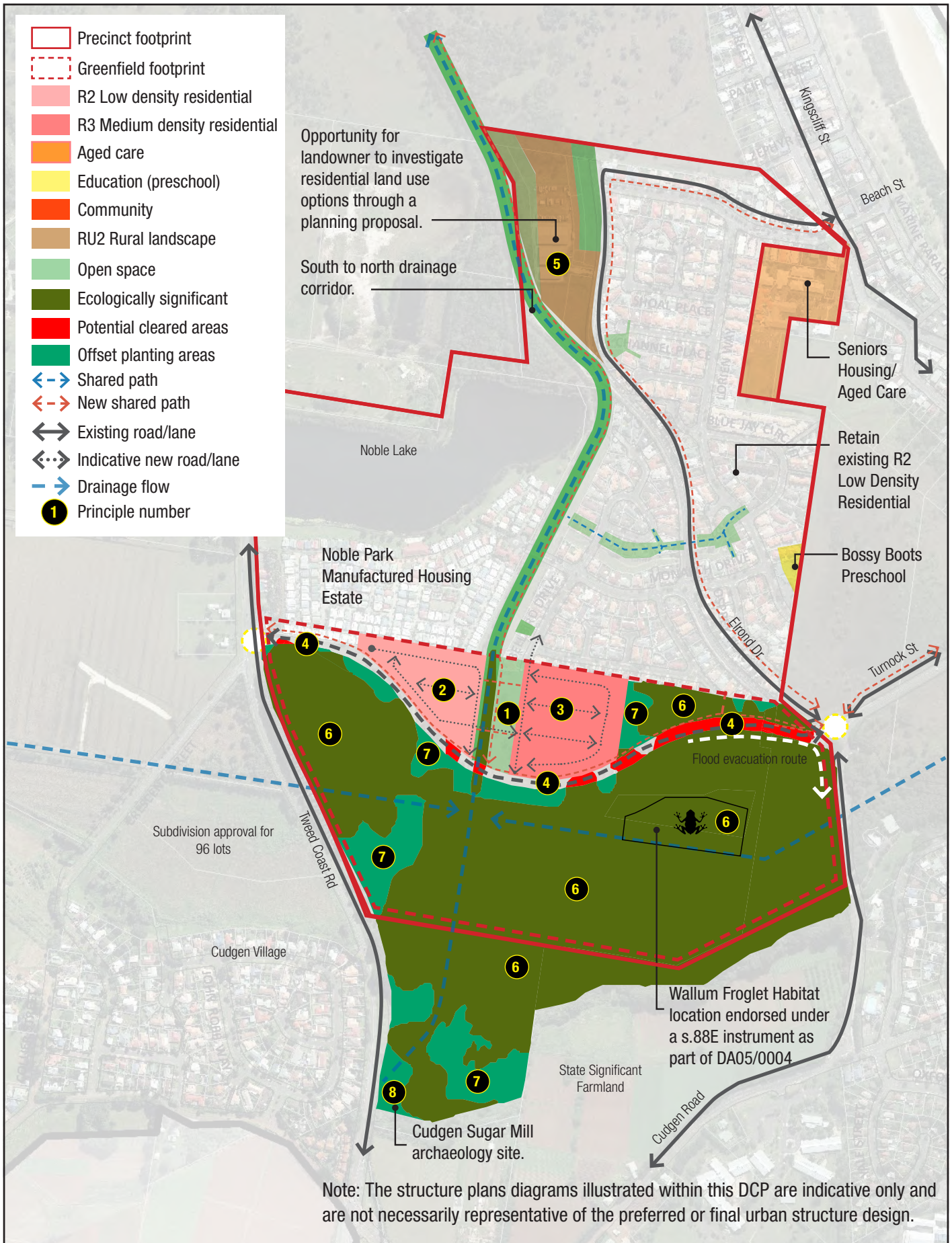


Figure 2.20 West Kingscliff Precinct Indicative Structure Plan

- P2. Facilitate the rezoning of land immediately south of the Noble Park Manufactured Home Estate from RU1 Rural Landscape to a residential land use. This land could form part of an extension to the manufactured housing estate, a small lot residential eco-village estate or form part of the broader West Kingscliff greenfield development subdivision.
- P3. Achieve a balance and mix of housing types including a mix of low rise medium density housing to 9.0m and residential flat buildings to 12.2m. Land use and density targets have been identified in Table 4.
- P4. Reinforce the extended Turnock Street as the principle connector road which will ultimately link the Tweed Coast Road with the Kingscliff township by:
- Designing Turnock Street as a tree lined boulevard to provide a high level of visual and user amenity.
  - Utilising the alignment of the Turnock Street extension to delineate between developable area (north of alignment) and area of high ecological significance/environmental protection (south of alignment).
  - Co-ordinated access points for development fronting Turnock Street to avoid multiple road/driveway access points off Turnock Street.
  - The inclusion of a dedicated shared pedestrian and cycle lane along the northern side of the Turnock St extension linking areas of West Kingscliff with the town centre with shade, seating, lighting and water points at regular intervals.
  - Integrated street lighting, parking, landscape bays and WSUD streetscape elements.
- P5. Investigate residential land use options in consideration of likely constraints including proximity to environmental protection areas, flooding, bushfire and community expectation to lands along Elrond Drive currently zoned RU2 Rural landscape (Lot 129 and 130 DP1039348).
- P6. Facilitate the protection and management of land identified as environmentally and/or ecologically important through appropriate land use zoning and provisions for ongoing habitat management.
- P7. Identify lands for offset planting and provisions for ongoing habitat restoration and management.
- P8. Undertake a heritage review of the old Cudgen Sugar Mill site.

**West Kingscliff Precinct – Land use and Residential Density Targets**

| Land Use                                      | Area (ha)      | Building Height | Density Target (dph) | Dwelling/Unit No. | Projected Pop. (2.4/1.8per dw) |
|---|----------------|-----------------|----------------------|-------------------|--------------------------------|
| R2 Low Density (existing)                     | 32.2ha         | 9m              | 16                   | 497               | 1168                           |
| R3 Medium density (greenfield)                | 3.62ha         | 12.2m           | 80                   | 290               | 522                            |
| R3 Medium density (greenfield)                | 2.59ha         | 9m              | 30                   | 78                | 140                            |
| Noble Park Estate (existing)                  | 21.9ha         | 9m              | –                    | 254               | 597                            |
| Open Space (Passive)                          | 1.2ha          | –               | –                    | –                 | –                              |
| Road Reserve/Drainage Corridor/Infrastructure | 19.3ha (16%)   | –               | –                    | –                 | –                              |
| <b>Total</b>                                  | <b>80.81ha</b> | <b>9m-12.2m</b> | <b>16-80 dph</b>     | <b>1119</b>       | <b>2427</b>                    |

**Open Space Required**

| Open Space Type | Rate        | Required | Existing | Structure Plan | Difference |
|-----------------|-------------|----------|----------|----------------|------------|
| Active          | 1.7ha/1000  | 4.12ha   | –        | –              | -4.12      |
| Passive         | 1.13ha/1000 | 2.74ha   | 1.76ha   | 1.2ha          | +0.22ha    |

Table 4 West Kingscliff Land Use and Residential Density Targets

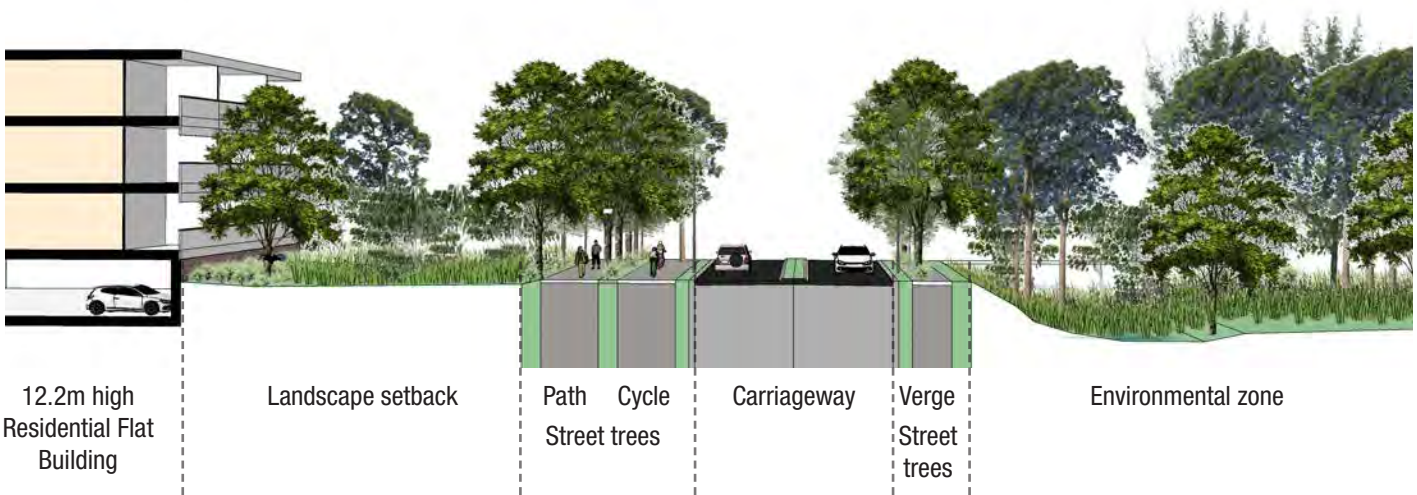


Figure 2.21 Indicative Turnock Street (West) Extension Section

## 2.15 Key Greenfield Site – North Kingscliff Precinct

### 2.15.1 Character Statement

The North Kingscliff Precinct greenfield development site adjoins existing low density residential dwellings north of Ozone Street and low density and low rise medium density dwellings south of Sands Street. The infill residential development site will facilitate additional low rise medium density housing types including small lots, terrace houses, row houses, courtyard housing, plexes and multi-unit development as well as additional suburban allotments adjoining the existing North Kingscliff residential areas. Given the location of the future business and knowledge precinct to the immediate west, the greenfield development site will provide additional east-west and north-south vehicle and passive movement connections.

The key planning and design opportunities for this greenfield development site include:

- Development of the north Kingscliff Precinct greenfield development site as a mix of housing types including low density residential housing, small lot housing and low rise medium density residential.
- Subdivision configuration of this site needs to consider the broader locality movement (vehicular and passive) opportunities and open space network. This may include; Ozone Road connection west to a future intersection with Tweed Coast Road; and a new north south road connecting Elrond Drive with Sands Street.
- Increasing residential density and building heights fronting Kingscliff Street which is the key north south connector road and public transportation route and western side of Sands Street with an R3 medium density zoning and building height of 9m to enable low rise medium density housing types.
- Reducing minimum lot size to encourage coastal small lot housing and additional dual occupancy infill development whilst maintaining a low density character.
- Encouraging secondary dwelling development within the existing residential areas providing affordable housing, aged housing in place and inter generational family housing opportunities.
- Embellish north south drainage corridor for combined drainage, vegetation, open space and passive movement (shared path) uses and pursue a dedicated on road cycle path the length of Kingscliff Street/Pearl Street.

### 2.15.2 Planning and Design Principles:

- P1. Undertake a master planning process to underpin future planning proposal, concept development application or subdivision development application(s) as required including facilitating a range of low density residential and low rise medium density housing types between Sands and Ozone Street and facilitate low rise medium density housing types along the western side of Kingscliff Street. The structure plan/master plan process should include:
- A neighbourhood park appropriately sized and embellished to meet the passive open space needs to the local residents.
  - A new north south road connecting Elrond Drive with Sand Street.
  - A new east-west road(s) connecting an Elrond Drive with the greenfield development site (business and knowledge precinct) to the west.
  - A continued east-west alignment of the local road network resulting in predominantly north-south allotments to achieve a predominantly grided street structure and optimum solar orientation for future buildings.
- P2. Given the known and potential occurrence of Aboriginal Cultural heritage sites within this precinct, there will be a requirement to undertake an Aboriginal Cultural Heritage Assessment in consultation with the Tweed Byron Aboriginal Land Council regarding the future planning, design and construction of development within this precinct.



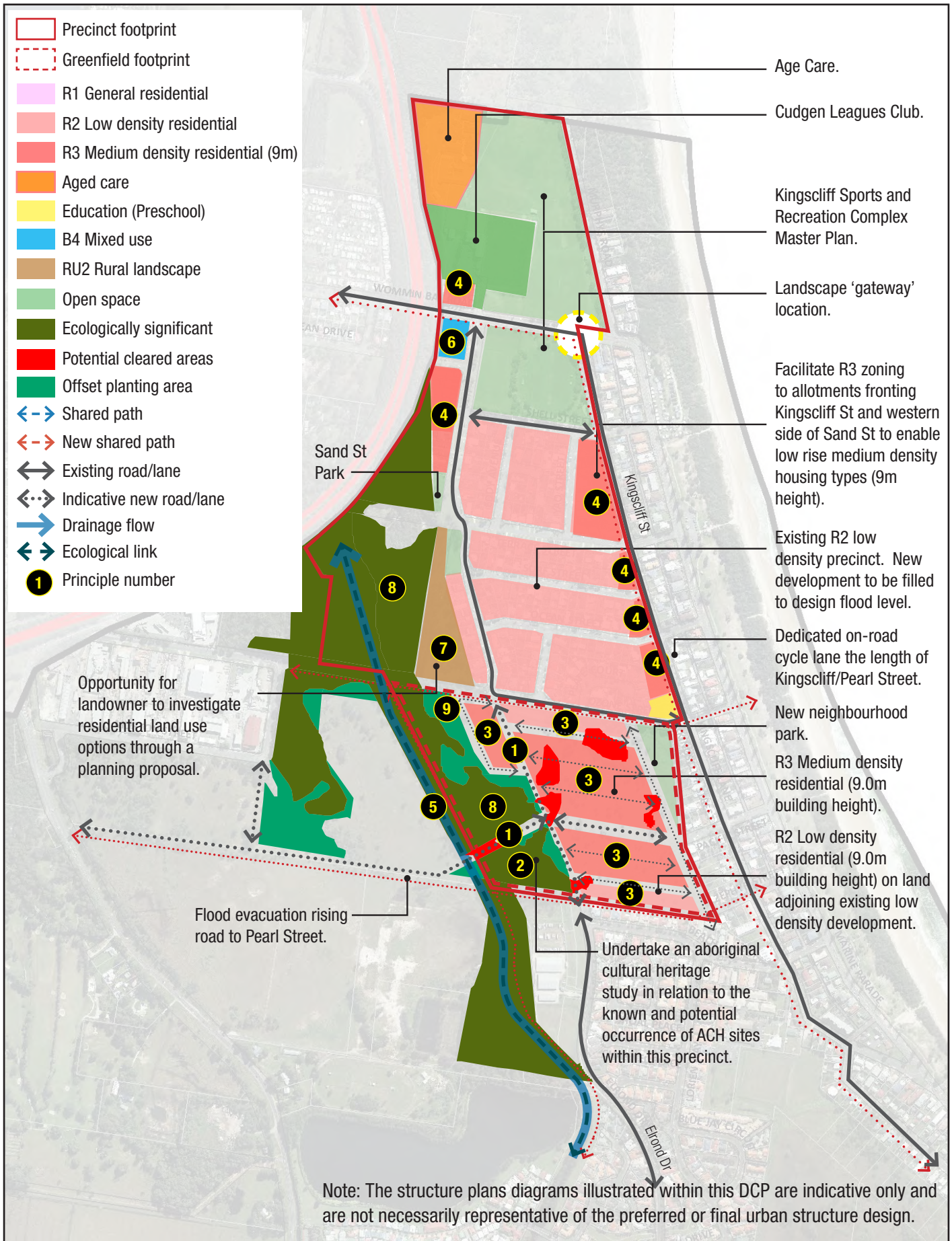


Figure 2.22 Indicative North Kingscliff Precinct Structure Plan

- P3. Achieve a balance and mix of housing types including a low density housing to 9.0 metres (post fill), low rise medium density housing to 9.0m (post fill). Land use and density targets have been identified in Table 5. Where green field development adjoins or interfaces with existing R2 low density development, new development shall similarly be R2 low density residential development.
- P4. Increase densities from R2 Low Density Residential to R3 Medium Density Residential with a 9.0m building height to:
- Allotments fronting Kingscliff Street (western side) which is the key north south connector road and public transportation route to allow low rise medium density housing types.
  - Allotments on the western side of Sands Street north of Terrace St.
  - Allotments on the north side of Wommin Bay Road adjoining Cudgen Leagues Club.
- P5. Integrate a pedestrian and cycling network of path throughout the precinct and connect with surrounding existing and proposed residential areas and open space. The pathway network is to achieve user comfort and amenity in terms of co-location of street trees for shade, adequate lighting and regular way finding with water points and seating. This includes:
- Strengthen pedestrian and cycling links towards the coastal foreshore reserve.
  - Lighting of pathways.
  - Embellishment of the north south drainage corridor for combined drainage, vegetation, open space and passive movement (shared path) uses.
  - Dedicated on-road cycle lane along the length of Kingscliff Street/Pearl Street.
- P6. Encourage the development of a local retail centre fronting Wommin Bay Road within the northern portion of this precinct which may also provide some live work opportunities, small scale food and beverage, small general store and/or relocated town centre service station site.
- P7. Investigate residential land use options over Lot 4 DP 1243907 accessed off Ozone Street against site considerations including proximity to environmental protection areas, potential for Aboriginal cultural heritage sites, flooding and bush fire.
- P8. Facilitate the protection and management of land identified as environmentally and/or ecologically important through appropriate land use zoning and provisions for ongoing habitat management.
- P9. Identify lands for offset planting and provisions for ongoing habitat restoration and management.

**North Kingscliff Precinct – Land Use & Residential Density Targets**

| Land Use                                      | Area (ha)      | Building Height | Density Target (dph) | Dwelling/Unit No. | Projected Pop. (2.35 per dw) |
|---|----------------|-----------------|----------------------|-------------------|------------------------------|
| R2 Low Density (existing)                     | 22.2ha         | 9m              | 10.9                 | 242               | 568                          |
| R3 Medium Density (existing)                  | 0.76ha         | 9m              | 28.9                 | 22                | 52                           |
| R2 Low density (greenfield)                   | 4.57ha         | 9m              | 16                   | 73                | 175                          |
| R3 Medium Density (Rezone)                    | 3.4ha          | 9m              | 30                   | 102               | 184                          |
| R3 Medium Density (greenfield)                | 7.66ha         | 9m              | 30                   | 230               | 414                          |
| Open Space (greenfield Passive)               | 0.8ha          | -               | -                    | -                 | -                            |
| Open Space (existing Active)                  | 12ha           | -               | -                    | -                 | -                            |
| Open Space (Private Recreation)               | 3ha            | -               | -                    | -                 | -                            |
| Road Reserve/Drainage Corridor/Infrastructure | 11.19ha (16%)  | -               | -                    | -                 | -                            |
| <b>Total</b>                                  | <b>66.29ha</b> | <b>9m</b>       | <b>10-50 dph</b>     | <b>669</b>        | <b>1393</b>                  |

**Open Space Required**

| Open Space Type | Rate        | Required | Existing | Master Plan | Difference |
|-----------------|-------------|----------|----------|-------------|------------|
| Active          | 1.7ha/1000  | 2.36ha   | 12ha     | -           | +9.6ha     |
| Passive         | 1.13ha/1000 | 1.57ha   | 0.86ha   | 0.78ha      | +0.07ha    |

\* North Kingscliff Sportsfields serves broader locality active open space need.

Table 5 North Kingscliff Land Use and Residential Density Targets

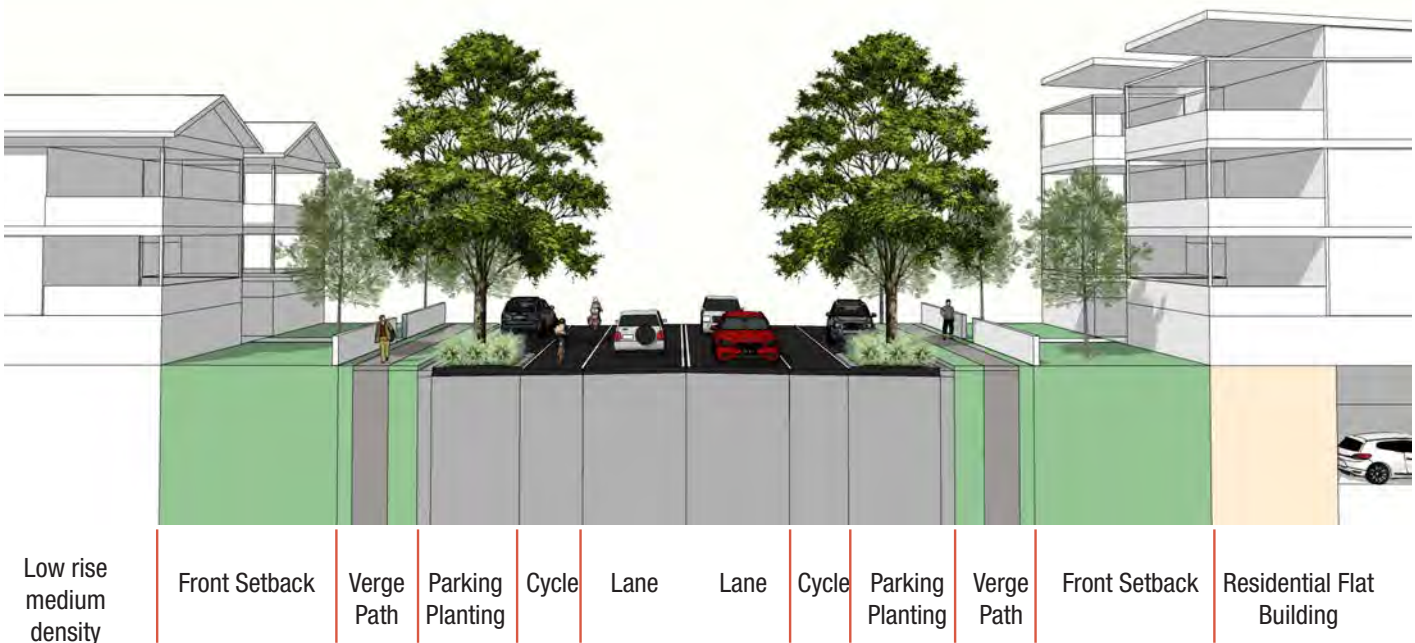


Figure 2.23 Indicative Kingscliff St Section

## 2.16 Key Greenfield Site – Business and Knowledge Precinct

### 2.16.1 Character Statement

The business and knowledge precinct will play a key transformative role to expand economic and employment generating land uses servicing both Kingscliff and broader sub-regional area. Given the large site area (47.5ha) and ready access to the Pacific Highway, the Business and Knowledge Precinct is strategically positioned to deliver a range of employment generating land uses including a business park, education campus, creative industries hub, cultural and entertainment uses, community facilities and a new local centre. These employment generating land uses will be supported by a mix of medium density residential housing types and embellished network of open space and public domain areas. The key planning and design opportunities for this greenfield development site include:

- The development of a regionally scaled business park (approx 75000sqm).
- Opportunity for the establishment of a regional education campus (University and/or TAFE expansion).
- Development of a new local retail centre to serve the immediate future resident population and complement the existing Kingscliff town centre (approx 10-15000sqm).
- Facilitate the transformation of the existing Chinderah Industrial Estate into a business park land uses including creative industries hub.
- Establish treelined mixed use main street with public domain.
- Providing opportunities for the development of a range of medium residential housing typologies.
- Provide open space including additional active open space (sportsfields) to meet the future need of the growing locality population.

### 2.16.2 Planning and Design Principles:

- P1. Undertake a master planning process to underpin future planning proposal, concept development application or subdivision development application(s) as required to facilitate the future development of the Business and Knowledge Precinct as a regionally significant employment generating precinct. The business and knowledge precinct will establish a mix of employment generating land uses including a business park (B4), a retail centre (B2), light and creative industries (IN1), cultural and entertainment uses, community facilities, opportunity for a health and/or education campus (SP), network of open spaces and a mix of residential housing types. The master plan process should include:
- A developer initiated and funded economic and retail centres feasibility assessment to evaluate proposed staged land use and floor area development scenarios/projections within the context of the broader Tweed retail and centres hierarchy.
  - A developer initiated and funded site specific flood study and mitigation strategy which identifies lands to be filled (65% of the overall site), evacuation routes and location of surface drains as integrated with the road and open space structure/networks with reference to the Tweed Valley Floodplain Risk Management Strategy (TVFPRMS).
  - A developer initiated built form character study reviewing the relationship between building heights and 3d massing study, FSR, site coverage, network of open space and public domain, view and visual impact analysis to underpin development standards within any future planning proposal.
  - A developer initiated traffic impact study based on envisaged land use and projected GFA/density outcomes with reference to the Tweed Road Development Strategy (TRDS).
  - A Site Contamination and Rehabilitation Plan over the decommissioned WWTP site to determine remediation requirements for future uses (open space).

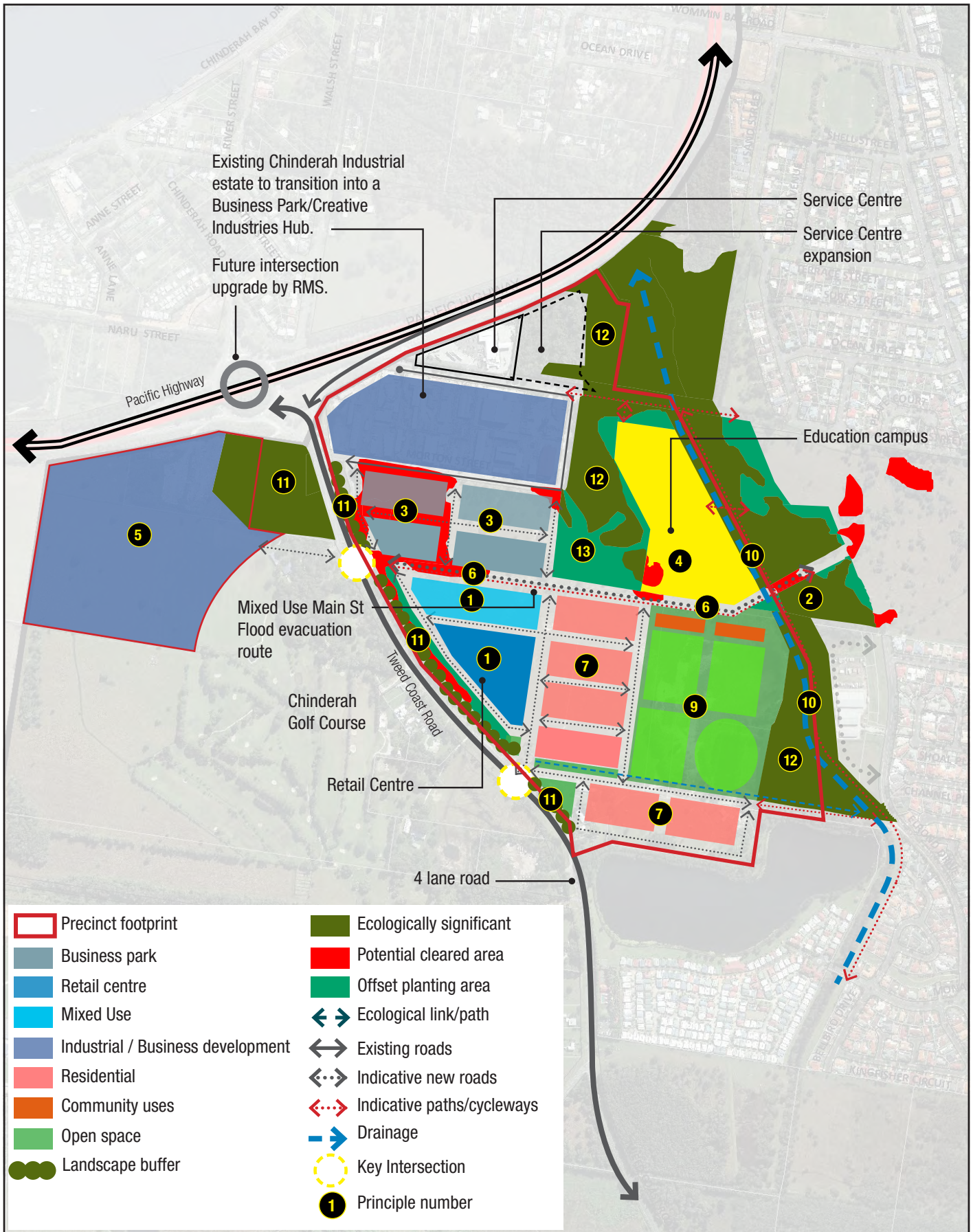


Figure 2.24 Indicative Business & Knowledge Precinct Structure Plan – Business & Education Campus Option

- P2. Given the known and potential occurrence of Aboriginal Cultural heritage sites within this precinct, there will be a requirement to undertake an Aboriginal Cultural Heritage Assessment in consultation with the Tweed Byron Aboriginal Land Council regarding the future planning, design and construction of development within this precinct.
- P3. Provide opportunity for the development of a business park within the precinct up to 13.6m in height to encourage more intensive employment generating land uses, commercial and business opportunities.
- P4. Investigate opportunity for the development of an education campus which may provide opportunities for a university campus and/or an expanded Kingscliff TAFE.
- P5. Facilitate a change of land use from IN1 General Industrial to B5 Business Development over lands to the west of Tweed Coast Road providing opportunity for larger floor plate and light industrial uses where it can be demonstrated that traffic access, environmental management, aboriginal cultural heritage and flood management related issues can be achieved through a master planning process.
- P6. Create a principle east-west connector road linking Tweed Coast Road across the precinct as a tree lined boulevard to the east to intersect with a future Elrond Drive extension. This connector road should:
- Be design as a flood evacuation route rising road to the east.
  - Facilitate the development of a mixed use/main street development over part of its length which may provide fine grain active retail street level with residential and or commercial uses above and public domain street edge.
  - Integrate street lighting, parking, landscape bays and WSUD streetscape elements.
  - Include a dedicated shared pedestrian and cycle lane network throughout the precinct with shade, seating, lighting and water points at regular intervals.
- P7. Facilitate a mix and diversity of medium density housing opportunities through residential zoning including residential flat buildings up to 12.2m, low rise medium density housing, low density housing, affordable and aged housing, student accommodation (ancillary with an education or health campus).
- P8. Achieve affordable housing outcomes through co-ordinated negotiations between Council, the developer and affordable housing providers to meet agreed affordable housing benchmarks established through an Affordable Housing Strategy to be undertaken by the developer. Land use and density targets have been identified in Figure 2.6 and Section 2.8 of this DCP.
- P9. Allocation of a large open space area (approximately 9.5ha) to cater for the active and passive recreational needs of future residents. Open space area adjoining the north-south drainage corridor, over part of the rehabilitated WWTP site could act as an overland flow storage area and buffer to an area of high ecological significance.
- P10. Embellish the north-south drainage corridor as a defining 'green corridor' for combined drainage, vegetation link, open space and shared pathway uses.
- P11. Create a 10m wide vegetative buffer to Tweed Coast Road frontage providing a 'green edge' to the site with opportunity plant out with large street trees and under storey vegetation.
- P12. Facilitate the protection and management of land identified as environmentally and/or ecologically important through appropriate land use zoning and provisions for ongoing habitat management.
- P13. Identify lands for offset planting and provisions for ongoing habitat restoration and management.

| Business & Knowledge Precinct – Land Use & Residential Density Targets |               |                 |                     |                                 |                                   |
|--|---------------|-----------------|---------------------|---------------------------------|-----------------------------------|
| Land Use   | Area (ha)     | Building Height | Density/Employment* | Dwelling /GFA (A/2xh)           | Projected Pop./Jobs               |
| SP1 Knowledge Campus   | 6ha           | 13.6m           | 1/50sqm (E)         | 50 000sqm                       | 1000 jobs                         |
| B7 Business Park#  | 5ha           | 13.6m           | 1/20sqm (E)         | 75 000sqm                       | 3750 jobs                         |
| B4 Mixed Use (Dwellings)   | 2ha           | 13.6m           | 30 (D)              | 60                              | 108 people                        |
| B4 Mixed Use (Business)  | 2ha           | 13.6            | 1/44sqm (E)         | 10 000sqm                       | 227 jobs                          |
| B2 Local Centre (Retail)   | 2.6ha         | 10m             | 1/44sqm (E)         | 13 000sqm                       | 295 jobs                          |
| B5 Business Development#   | 16.5ha        | 10m             | 1/90sqm (E)         | 82 500sqm                       | 916 jobs                          |
| IN1 Industrial (Existing)  | 8.0ha         | 10m             | 1/50 (E)            | 40000sqm                        | 800 jobs                          |
| Service Centre   | 4.0ha         | 10m             | 1/44sqm (E)         | 2000sqm                         | 45 jobs                           |
| R3 Medium Density (greenfield)   | 4.4ha         | 12.2m           | 80 (D)              | 352(D)                          | 633 people                        |
| R3 Medium Density (greenfield)   | 4.4ha         | 9m              | 30 (D)              | 132 (D)                         | 238 people                        |
| R2 Low Density (greenfield)  | 3ha           | 9m              | 16 (D)              | 48 (D)                          | 115 people                        |
| Open Space (Passive – District Park)                                   | 1.4ha         | –               | –                   | –                               | –                                 |
| Open Space (Active)  | 8.1ha         | –               | –                   | –                               | –                                 |
| Road Reserve/Buffers/<br>Drainage/Infrastructure                       | 13.7ha (15%)  | –               | –                   | –                               | –                                 |
| <b>Total</b>   | <b>81.1ha</b> | <b>9m</b>       | <b>16-80 dph</b>    | <b>592 dwellings/270,500sqm</b> | <b>1094 people/<br/>7000 jobs</b> |
| <b>Open Space Required</b>   |               |                 |                     |                                 |                                   |
| <b>Open Space Type</b>   | <b>Rate</b>   | <b>Required</b> | <b>Existing</b>     | <b>Master Plan</b>              | <b>Difference</b>                 |
| Active   | 1.7ha/1000    | 1.85ha          | –                   | 9.0ha                           | +7.65ha                           |
| Passive  | 1.13ha/1000   | 1.23ha          | –                   | 1.23ha                          | 0.0ha                             |

\* Employment density derived from City of Sydney Floor Space and Employment Survey 2012, City of Perth 2009.

# Business Park and Business Development serving a broader regional catchment.

Table 6 Business & Knowledge Precinct Land Use and Residential Density Targets



**Business and Knowledge Precinct:** This precinct has the opportunity to play a strong transformative role to expand sub-regional economic and employment generating land uses. Future employment and economic growth has the opportunity to diversify opportunity within existing industry pillars including tourism, agriculture, education, health and construction.

## 2.17 Key Greenfield Site – Cudgen Village Precinct

### 2.17.1 Character Statement

The Cudgen greenfield development site will extend the existing village urban footprint north over filled land bounded by the Tweed Coast Road and Crescent St. The development site will facilitate additional low density residential housing types complementing the existing Cudgen residential character as well as providing an opportunity to develop low rise medium density development which may include seniors housing. This future residential area will be visually screened from the Tweed Coast Road by a vegetative buffer along its length. The parcel of land with frontages to both Tweed Coast Road and Crescent Street presents opportunity for a small retail centre and/or service station servicing the convenience needs of the immediate resident population and Tweed Coast Road travellers. Following the conclusion of sand extraction to the west of the settlement there is also long term opportunity to develop regional active open space facilities as well as a holiday park with associated facilities fronting the artificial lake (private recreation).

The key planning and design opportunities for this greenfield development site include:

- Retain the low scale landscape residential character of Cudgen Village.
- Pursuing a subdivision pattern based around principles of sustainable design which will allow a range of housing typologies and mix of density including opportunity for some low rise medium density housing and seniors housing.
- Facilitate opportunity for some business and tourist land uses which may be associated with adjoining agricultural land uses including a fresh produce market, food and beverage uses, rural tourist uses and accommodation and or small scale retail uses servicing the existing Cudgen Village.
- Review active and passive recreation uses and holiday or tourist accommodation and associated ancillary retail and food and beverage uses surrounding the future artificial lake at the cessation of sand extraction operations.
- Embellishment of the existing Cudgen Recreation Ground.

### 2.17.2 Planning and Design Principles:

- P1. Undertake a master planning process to underpin future planning proposal, concept development application or subdivision development application(s) as required to facilitate:
  - Future residential development across the developable portion Lot 1 DP 828298 currently zoned R1 General Residential to achieve a mix of housing types, stormwater works, environmental protection and a 10m landscaped buffer along the Tweed Coast Road frontage.
  - Detailed design of the intersection of Altona Road with Tweed Coast Road and the Turnock Street extension (dependent on final alignment).
  - Demonstration of how passive solar design principles have been embedded to include a generally a grid street network resulting in regular shaped allotments with good solar and passive design opportunities.
  - Integration of water sensitive urban design as part of the overall subdivision design.
  - Create a 10m wide vegetative buffer to Tweed Coast Road frontage providing a 'green edge' to the site with opportunity plant out with trees and understorey vegetation.
- P2. Investigate future opportunity to establish a holiday park or tourist accommodation and associated ancillary retail and food and beverage uses over part of Lot 21 DP 1082482 at the cessation of sand extraction operations in consideration of key constraints including flooding.
- P3. Maintenance of a 500m buffer surrounding the waste water treatment plant restricting any residential or tourist development within that buffer.
- P4. A developer initiated and funded stormwater drainage strategy for the precinct and management plan for the future artificial lake to identify methods to manage, maintain, fund and implement the works necessary to achieve development of the precinct while protecting the integrity of receiving environmental land.





Figure 2.25 Cudgen Village Indicative Structure Plan

- P5. Review the existing R2 Low Density Residential zoning over Lot 101 DP 1056576 for a more appropriate land use or additional permitted uses given the incompatibility between this use and the adjoining state significant farmland and buffers required. This includes commercial premises uses which may be associated with adjoining agricultural land uses including a fresh produce market, food and beverage uses, rural tourist uses and accommodation and or small scale retail uses servicing the existing Cudgen Village.
- P6. Facilitate the protection and management of land identified as environmentally and/or ecologically important through appropriate land use zoning and provisions for ongoing habitat management.
- P7. Identify lands for offset planting and provisions for ongoing habitat restoration and management.
- P8. Undertake heritage assessment on the stand of Norfolk Island Pine Trees along Collier Street for consideration to be included on a significant tree register or potentially as a landscape heritage item under Schedule 5 of the Tweed LEP 2014.
- P9. Integrate a pedestrian and cycling network of path throughout the precinct and connect with surrounding existing and proposed residential areas and open space. The pathway network is to achieve user comfort and amenity in terms of co-location of street trees for shade, adequate lighting and regular way finding with water points and seating. This includes:
  - Strengthen pedestrian and cycling links towards Kingscliff township via the new Turnock St extension and along Cudgen Road passing the Tweed Valley Hospital site / Kingscliff TAFE.
  - Lighting of pathways.
  - Provision for on-road cycle along Tweed Coast Road.



Cudgen is a contained low scale residential settlement surrounded by agricultural land uses. Being elevated there are a number of long views experienced around the town. In terms of built form, dwellings within Cudgen includes a mix of brick and tile as well as lightweight low density residential dwellings with some dual occupancy developments as well as a two storey 25 dwelling townhouse development accessed off John Robb Way.

| Cudgen Precinct – Land Use & Residential Density Targets |              |                 |                      |                   |                              |
|--|--------------|-----------------|----------------------|-------------------|------------------------------|
| Land Use   | Area (ha)    | Building Height | Density Target (dph) | Dwelling/Unit No. | Projected Pop. (2.35 per dw) |
| R2 Low Density (existing)                                | 14 ha        | 9m              | 13–35                | 200               | 470                          |
| R2 Low density (undeveloped)                             | 10.69 ha     | 9m              | 16                   | 144               | 345                          |
| Open Space (Existing Passive)                            | 0.5 ha       | –               | –                    | –                 | –                            |
| Open Space (Existing Active)                             | 1.22 ha      | –               | –                    | –                 | –                            |
| School   | 0.8 ha       | 9m              | –                    | –                 | –                            |
| Road Reserve/Drainage Corridor/<br>Infrastructure        | 7.38ha (19%) | –               | –                    | –                 | –                            |
| <b>TOTAL</b>   | <b>38ha</b>  | <b>9m</b>       | <b>13-35 dph</b>     | <b>344</b>        | <b>815</b>                   |
| Open Space Required                                      |              |                 |                      |                   |                              |
| Open Space Type  | Rate         | Required        | Existing             | Master Plan       | Difference                   |
| Active   | 1.7ha/1000   | 1.38ha          | 1.72ha               | –                 | +0.34ha                      |
| Passive  | 1.13ha/1000  | 0.92ha          | 0.5ha                | 0.5ha             | +0.08ha                      |

Note: Potential future open space north of Altona Road alignment not included due to the planned long-term sand extraction over the site.

Table 7 Cudgen Land Use and Residential Density Targets



**Future Artificial Lake:** Opportunity for a wide range of active and passive recreation uses as well as tourism accommodation or holiday park fronting a future artificial lake as RE2 Private Recreation.



## PART 03 KINGSCLIFF TOWN CENTRE

- 3.1 Town Centre Precinct Character Statement
- 3.2 Town Centre Precinct Objectives
- 3.3 Site analysis
- 3.4 Townscape and urban design
- 3.5 Building height
- 3.6 Floor space ratio
- 3.7 Site coverage
- 3.8 Setback and building separation
- 3.9 Active frontage and building orientation
- 3.10 Building form, materials and passive design
- 3.11 Mixed use development
- 3.12 Large floor plate development
- 3.13 Corner sites
- 3.14 Awnings and weather protection
- 3.15 Outdoor dining areas
- 3.16 Signage
- 3.17 Access and car parking
- 3.18 Public Domain and Civic Improvements



### 3.1 Town Centre Precinct Character Statement:

*The town centre will continue to be the primary activity centre for the locality by providing a range of retail, commercial, community, health, education, tourism, recreation and accommodation uses which meets the service needs of locals, tourists as well as other surrounding settlements. The existing urban structure of the narrow fronted retail uses along Marine Parade facing the beach with larger floor plate uses to Pearl and Turnock Street will be retained. The low scale building character along Marine Parade will be reinforced through building form and building height controls.*

*Future town centre growth will be achieved by planned expansion into the greenfield development area along Turnock Street. These new development areas will be increasing the overall retail and commercial land uses and provide a range of floor space sizes to suit different business needs as well as providing additional areas of car parking. All new buildings will be required to be designed to take advantage of the sub tropical context to strengthen and enhance the Kingscliff coastal character.*

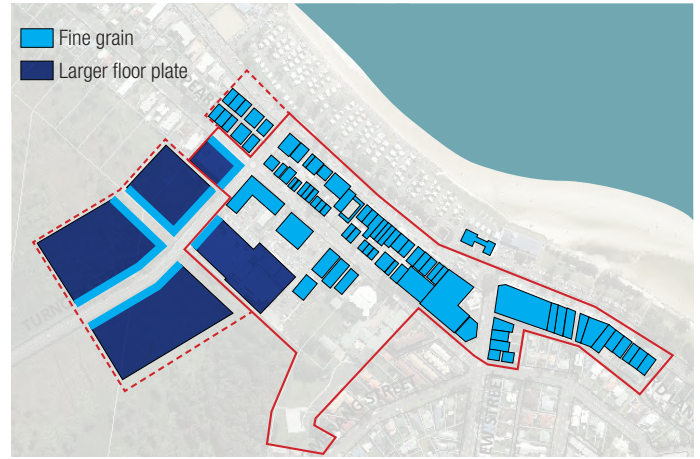
*Additional and improved public domain areas throughout the town centre will achieve greater pedestrian priority, connectivity, landscape and shade whilst effectively managing traffic and car parking. This will include an improved public transportation interchange.*

### 3.2 Town Centre Precinct Objectives:

1. Reinforce the urban structure of the town centre including maintaining the low rise building scale and 'fine grain' retail frontages and outdoor dining areas along Marine Parade whilst encouraging the development of larger retail uses such as supermarkets and mixed use development to the west of Pearl Street and along Turnock Street.
2. Expand the town centre west along Turnock St and north along Pearl Street and Marine Parade through a B4 mixed use zoning and encourage additional retail, community and mid-rise residential development over the Kingscliff Shopping Town site.
3. Integrate multi- purpose community facilities within or within proximity of the town centre which will total approximately 3000sqm of floor area.
4. Achieve a high standard of public domain and architectural design reflective of Kingscliff's subtropical coastal context.
5. Improve public domain and pedestrian connections between Pearl Street and Marine Parade and provide comfortable and equitable grades and universal design access throughout the town centre.
6. Create a new town square and area(s) of public domain on the Kingscliff Shopping Village site and over new town centre greenfield expansion sites.
7. To provide an opportunity for additional tourist and shop top housing to increase accommodation supply within the town centre location.
8. Provide extensive, integrated and usable green spaces and landscaping throughout the town centre with additional landscape bays and street trees.
9. Embrace and celebrate the coastal edge by improving pedestrian access, embellishing adjoining open space and opening view line opportunities, whilst recognising the need to implement coastal erosion measures.
10. Encourage walking, cycling and use of buses through improved pedestrian and cycle pathways and provision of a new bus interchange(s) along Turnock Street.



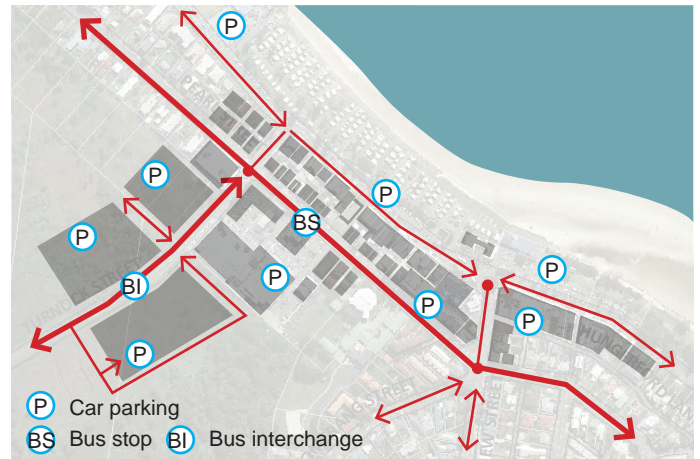
**Embrace the coastal edge.** Improve universal pedestrian access to the coastal edge, connect open space and where possible open up new view lines.



**Reinforce the town centre scale and structure.** Retain 'fine grain' shop fronts to Marine Pde whilst providing opportunity for larger retail and commercial uses to the expansion areas west of Pearl St.



**Expand the town centre.** Extend the town centre along Turnock St to include mixed use development and shop top housing and encourage redevelopment over Kingscliff Shopping Village site to activate Turnock and Pearl St.



**Manage and integrate traffic and car parking.** Encourage multi-deck and part basement car park to Kingscliff Shopping Village site and integrated parking within all new development along Turnock St. Reduce car primacy along Marine Pde. Include a new bus interchange.



**Strengthen town centre public domain.** Create a town square over part of the Kingscliff Shopping Village site to create a civic heart, increase through block connections and increase green spaces through landscape bays and street trees.



**Community uses within the town centre.** Locate multi-purpose community facilities within or within proximity to the town centre.

Figure 3.0 Key Kingscliff Town Centre Planning and Design Principles

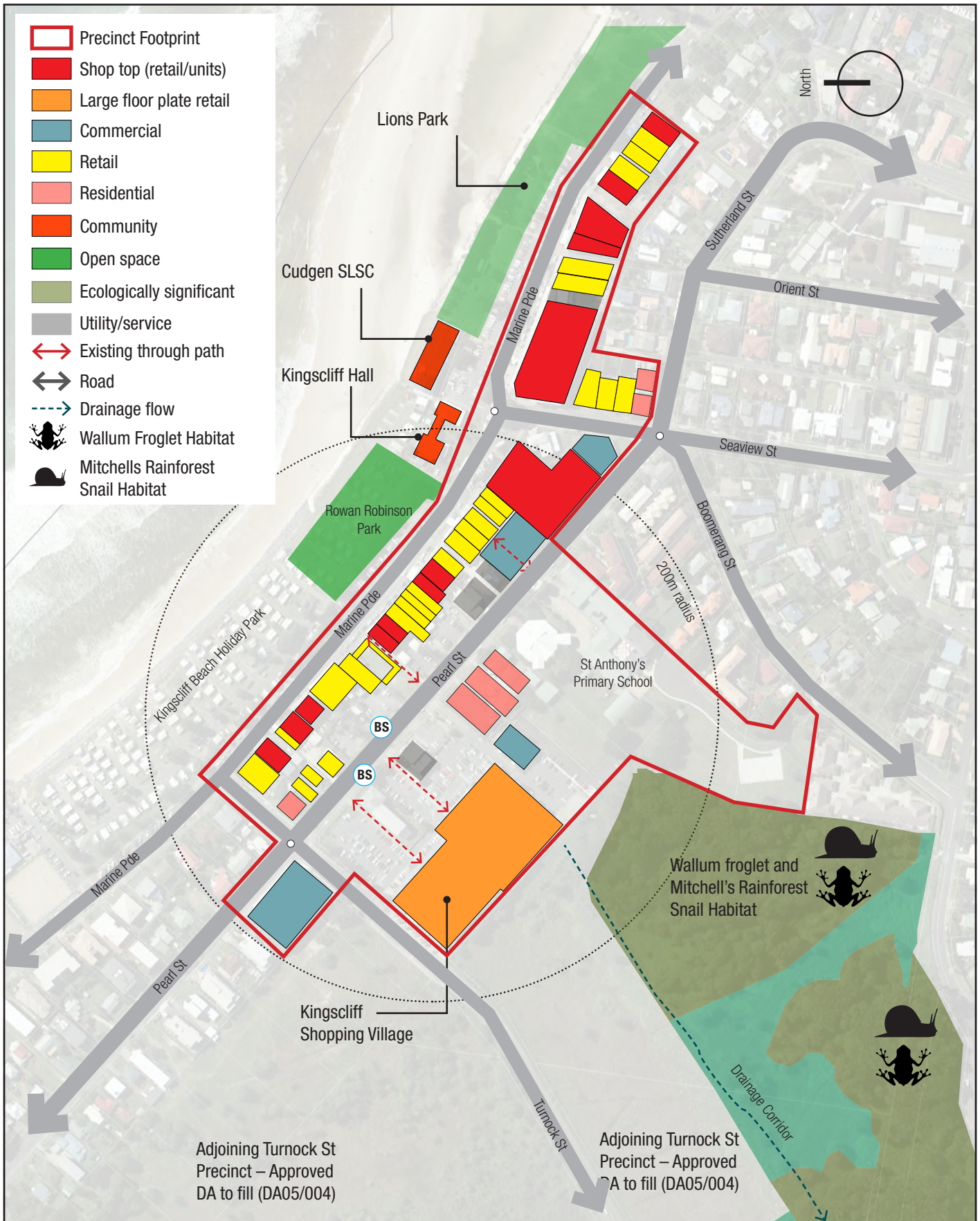


Figure 3.1 Existing Land use – Kingscliff Town Centre Precinct





Figure 3.2 Indicative Structure Plan – Kingscliff Town Centre Precinct



**Town Centre Precinct** – Retain smaller scale developments along Marine Parade and Pearl Street with a vibrant mix of retail and commercial uses activating a landscaped street edge with opportunity for expanded town centre uses including larger floor plate retail and commercial development west along Turnock Street.



**Legend**

- ① Rowan Robinson Park with new beach access and upgraded community hall
- ② Upgraded holiday park with reduced footprint
- ③ New Foreshore Seawall
- ④ Footpath widening and additional landscaping along Marine Pde
- ⑤ New through block connections
- ⑥ New infill development along Marine Parade to 11.0m building height
- ⑦ Expanded retail/commercial uses over Kingscliff Shopping Village Site including public domain areas
- ⑧ Multi-storey car park over Kingscliff Shopping Village Site
- ⑨ New multi-purpose community facility
- ⑩ Expanded town centre west along Turnock Street
- ⑪ New public domain areas fronting Turnock St including new bus interchange
- ⑫ Medium density residential precinct to 12.2m building height
- ⑬ New east-west road connection
- ⑭ New Turnock Street extension to Tweed Coast Road
- ⑮ New passive open space area

## Development Controls

### 3.3 Site Analysis

#### 3.3.1 Preamble

The first step to achieving well designed outcomes is to understand the context and attributes of a development site and an appreciation of the character of the surrounding area. A site analysis should document important site information including but not limited to allotment configuration, required setbacks and buffers, street interface and access and circulation, services, site levels and topography considerations, adjoining land uses including buildings and window locations as well as the location of any significant vegetation and environmental areas. In order to incorporate passive design principles, site analysis should also understand climatic information such as solar path and prevailing breeze directions throughout the year. This information should then be used as the basis to undertake a preliminary design. As such a site analysis comprises two parts:

1. Assessment of the existing condition, opportunities and constraints of a particular site, and
2. Design the development to respond to the characteristics, opportunities, constraints, unique features and potential hazards of the particular site.

#### 3.3.2 Objectives:

1. To ensure that the specific opportunities and constraints of the site and its surroundings are considered in the design development of town centre sites.
2. To ensure that key town and streetscape features, open space, proposed and adjoining development, existing and new pedestrian connections are considered during the early concept stage of development.
3. To ensure traffic movements, car parking, local and regional road networks (including the capacity of those networks) are considered during concept development.
4. To ensure that positioning of a building or structure on the site takes into account the intended use and seeks to minimise potential adverse impacts on the amenity of neighbours and the public.
5. To ensure that development occurs in an ecologically sustainable manner, and is energy and water efficient in terms of design and layout, resource consumption, materials, and the provision of additional features such as water sensitive design and renewable energy.
6. To ensure that the site is appropriately and effectively serviced by utility services to support the intended development.

#### 3.3.3 Controls:

- C1. All proposals for development within the town centre shall prepare and provide a site analysis in two parts including:
- 'existing site condition plan' which identifies the key characteristics, opportunities and constraints of the site and demonstrating a contextual understanding of the site, adjoining site features and the broader surrounding area.
  - 'design response plan' demonstrating how the proposed design specifically responds to these site analysis opportunities and constraints.

#### 3.3.4 Additional advice:

Proponents for new development within the town centre precinct are encouraged to undertake a pre-development application meeting. Site analysis plans along with concept plans should be developed and submitted for discussion and review as part of the pre-DA meeting.

Understand the site context and climatic influences, adjoining land uses, existing services, access and parking, street interface, setbacks, buffers and other constraints.

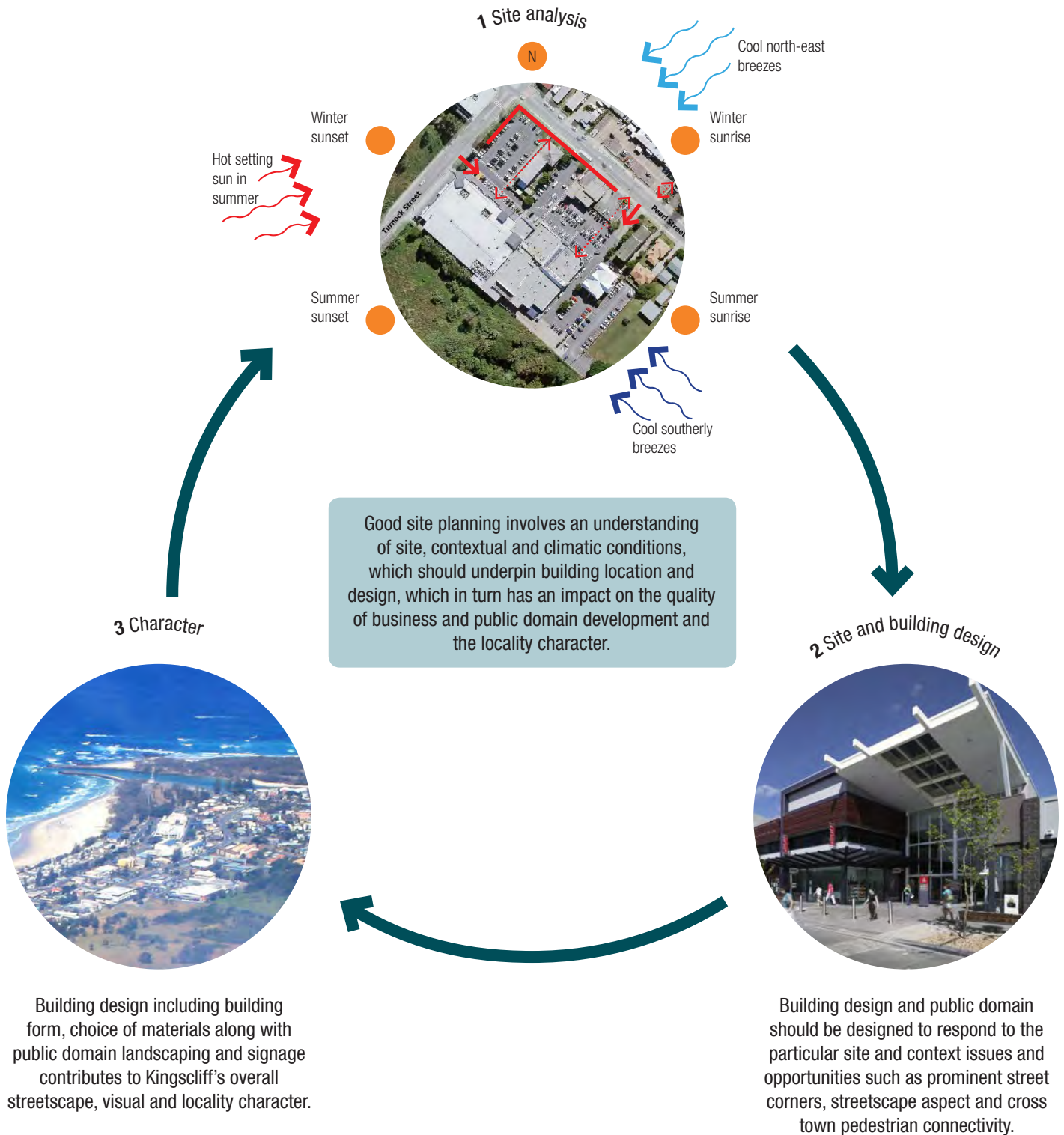


Figure 3.3 Town Centre Site Analysis – An integrated site analysis represents the relationship of a particular site within a given context. It looks at opportunities and constraints to ensure this information is used to inform a design outcome. Buildings are to be designed to specifically address contextual, topographic, climatic as well as economic, social and infrastructure considerations.

## 3.4 Town centre structure and urban design

### 3.4.1 Preamble

The town centre precinct consists of two character areas; the low rise 'fine grain' retail shop frontages and outdoor dining areas along Marine Parade and the larger floor plate retail development across the Kingscliff Shopping Village site. The broad townscape and urban design strategy is to retain the low scale Beach Front retail area and continue to encourage smaller scale retail and food and beverage uses including outdoor dining. Land to the west of the Kingscliff shopping village site and along Turnock Street provides an opportunity for additional smaller retail uses with larger floor plate retail uses behind. Improvement of pedestrian connections and public domain areas will further assist in movement around the town centre and general improvement of the overall visual amenity.

### 3.4.2 Objectives:

1. Strengthen and enhance the design quality of town centre precinct as the retail and commercial heart of the Kingscliff locality.
2. Retain the fine grain retail shop fronts and low rise built form character along Marine Parade and the eastern side of Pearl Street.
3. Facilitate larger floor plate development including supermarkets, mixed use development, basement and multi- storey car parking and a new multi-purpose community facility either within the town centre or adjoining Turnock St precinct.

### 3.4.3 Controls:

- C1. Retain the existing allotment configuration along Marine Parade and the eastern side of Pearl Street. Allotment consolidation along Marine Parade to create larger development lots will generally not be accepted as it disrupts the fine grain/narrow shopfront character at the street level street and generally results in larger and bulkier buildings.
- C2. All development applications for new buildings on identified key development sites (Figure 3.4) are to submit a Statement of Design Intent and relevant documentation and diagrams to supplement the development application. The Statement of Design Intent should address the urban design principles (Figure 3.5) as they relate to the site and broader town centre including:
  - Design Principle 1 – Architectural Excellence
  - Design Principle 2 – Building legibility and entry
  - Design Principle 3 – Activate street edges
  - Design Principle 4 – Pedestrian circulation
  - Design Principle 5 – Corners and prominent sites
  - Design Principle 6 – Facade and elevation treatment
  - Design Principle 7 – Materials and detail
  - Design Principle 8 – Building Form
  - Design Principle 9 – Expressed roof form
  - Design Principle 10 – Sustainable design

### 3.4.4 Additional advice:

Applicants are encouraged to engage the services of a qualified architect in the preparation of design concepts, documentation and administration of construction in order to achieve high quality design and detail in construction and best practice in sustainability. On identified key development sites, a qualified architect is expected to be engaged.

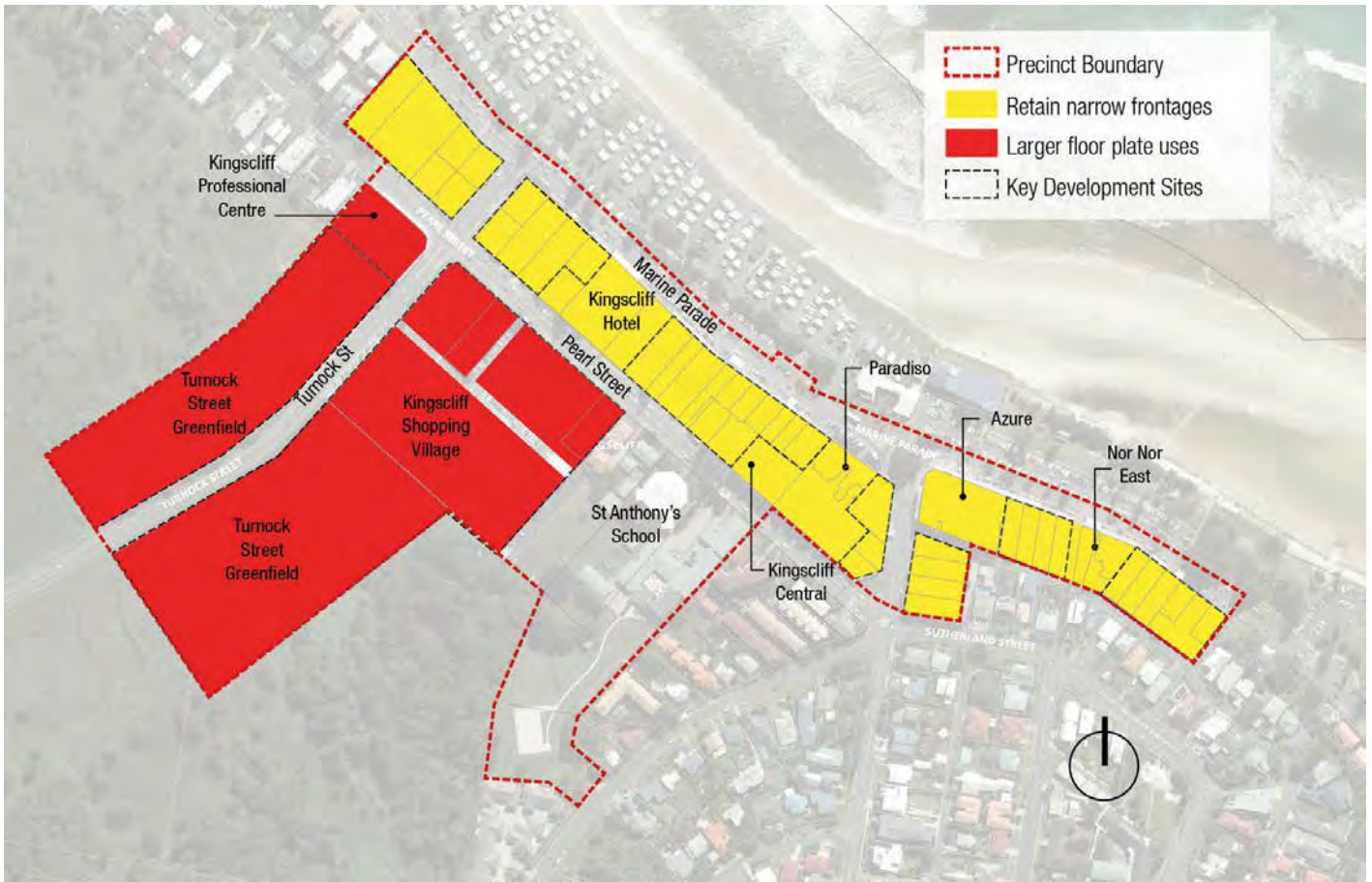


Figure 3.4 Key Development Sites – Map of Extents



Marine Parade is a popular cafe and restaurant strip with outdoor dining areas defining and activating street edges.



Narrow fronted (fine grain) shops and lower building forms combined with wider footpaths and outdoor dining areas underpin Marine Parades streetscape character.



**Design Principle 1 – Architectural Excellence**

Achieve a high quality architectural and urban design outcome which strengthens the Kingscliff coastal character by way of building form, roof design, materials, street interface, public domain, landscape and application of sustainable design principles.



**Design Principle 2 – Building legibility and entry**

Pedestrian entrances to all retail and commercial development, whether large or small should have clearly defined and legible entrances. The front elevation of this retail building is articulated with a double volume building entry filling the interior spaces with natural light. Despite the buildings overall length, a mix of materials reduce the visual scale and bulk. Well scaled shop windows create a visual connection between inside and outside.



**Design Principle 3 – Activate street edges**

Active street frontages are characterised by lively pedestrian friendly spaces with shade, outdoor dining areas and well scaled shop fronts which are visually engaging. In Kingscliff, and particularly along Marine Parade narrow shop fronts and smaller retail units are an important part of the town centre scale and character.



**Design Principle 4 – Pedestrian circulation**

Throughout the town centre and across larger retail developments it is important to design safe and legible circulation for people separated from cars and car parks. Walkways should be shaded, landscaped with opportunity for seating at regular intervals. Car parks should be landscaped and/or shaded to reduce heat island effects.



**Design Principle 5 – Corners and prominent sites**

Buildings on prominent sites such as corners and sites at the terminus of views and vistas can define the visual and built form character of precincts. These sites should be designed to reinforce the structure (frontage, height, setback) of the street block and enhance corners with architectural features and public domain. Corners can be addressed with architectural features, building entrances, awnings, splays or even recessed areas of enabling public domain.





### Design Principle 6 – Facade and Elevation Treatment

Design buildings with a distinctive ground middle and top. This building's facade expresses the well scaled shop frontages and upper level residential units through its primary and secondary structural systems and a mix of natural (timber, stone, glass) materials which breaks down the buildings overall scale and mass. The awning structure and upper level screens and planter beds to balcony edges contribute to its visual interest.



### Design Principle 7 – Materials and detail

Use a mix of materials, colours, textures together with architectural detail to improve the design quality of internal as well as external spaces. Avoid large featureless elevations or single material which result in visually bulky buildings. Large expanses of glass, typically used on commercial buildings, visually flattens a buildings elevation and increases reflectivity and glare which is not a preferred outcome.



### Design Principle 8 – Building Form

Buildings should be designed to reduce bulk and mass. This can be achieved by breaking the building form and elevations down into a series of volumes and forms and articulating those forms with different materials. Achieving a fine grain or pedestrian scale is particularly important for ground level town centre buildings. All upper level uses should include balconies, verandahs, semi indoor/outdoor spaces, light wells and the like to achieve adequate access to natural light and ventilation as well as adding architectural interest.



### Design Principle 9 – Expressed roof form

Upper levels and roofs of buildings are to contribute to the articulation of the overall building form and roof line. The roof form of this mixed use building creates defining architectural expression which responds to its Beach Front and subtropical context.



### Design Principle 10 – Sustainable design

Consideration of the climatic context and orientation of the site combined with an application of best practice passive and sustainable design principles and measures including the integration of built form and landscape to create comfortable pedestrian spaces.

Figure 3.5 Urban design principles – By designing active edges, attractive pedestrian spaces, a mix of materials, upper level decks, screens, shutters, expressive roof forms and landscape all combines to create more architecturally interesting buildings improving the visual amenity and design characteristics of the Kingscliff town centre.

## 3.5 Building Height

### 3.5.1 Preamble

'Building Height' is defined and mapped within the Tweed Local Environmental Plan 2014. Building heights in Kingscliff have been determined by a range of factors including:

- A strategy to preserve a low set building scale along Marine Parade.
- To encourage positive public benefit outcomes through increased development potential on key town centre sites where car parking, public domain and community facilities can be integrated.
- To enable an increased density of people, housing and tourist accommodation within and surrounding the town centre to capitalise on walkability.

### 3.5.2 Objectives:

1. Ensure that the height of buildings does not negatively impact on town centre character and the amenity of the public domain.
2. Step building heights back from the coastal reserve to reduce visual impacts and overshadowing of public spaces.
3. Ensure appropriate solar access to adjacent public and private space.
4. Provide opportunities for additional housing supply and tourist accommodation within the town centre.

### 3.5.3 Controls:

- C1. The maximum allowable height within the Kingscliff Town Centre is illustrated in Figure 3.6
- C2. Ground floors of all new buildings within the town centre precinct are to have a minimum floor to floor height of 3.6m in order to provide for flexibility of future use. Newer retail units within Marine Parade can provide lower heights where it can be demonstrated lower heights are required where integrated with existing adjoining retail units.
- C3. Above ground level, residential uses are to have a minimum floor to ceiling height of 2.7m.
- C4. Above ground level, commercial and retail uses are to have a minimum floor to floor height of 3.3m.

### 3.5.4 Additional advice:

For shop top housing and residential flat buildings refer to SEPP 65 and the Apartment Design Guide and Tweed Development Control Plan DCP A1 Part C.



Figure 3.6 Building Height- Map of Extents

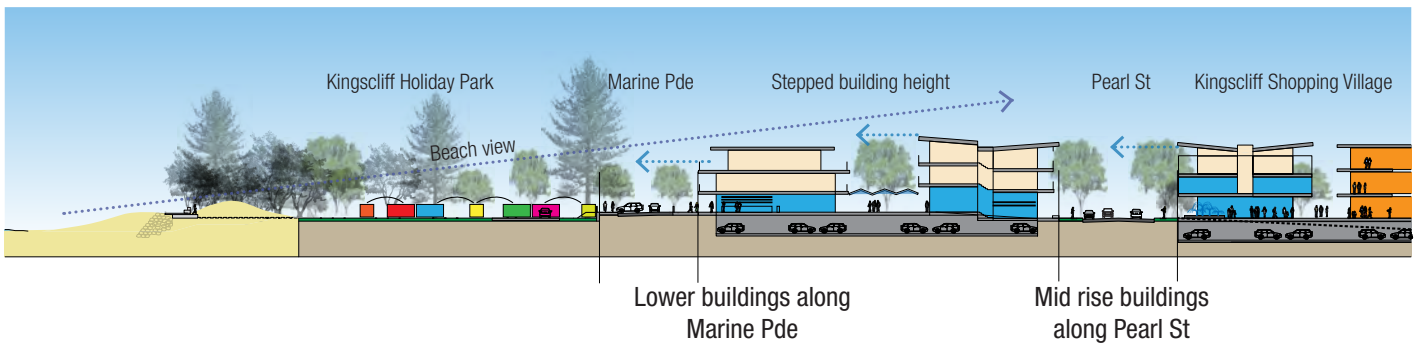
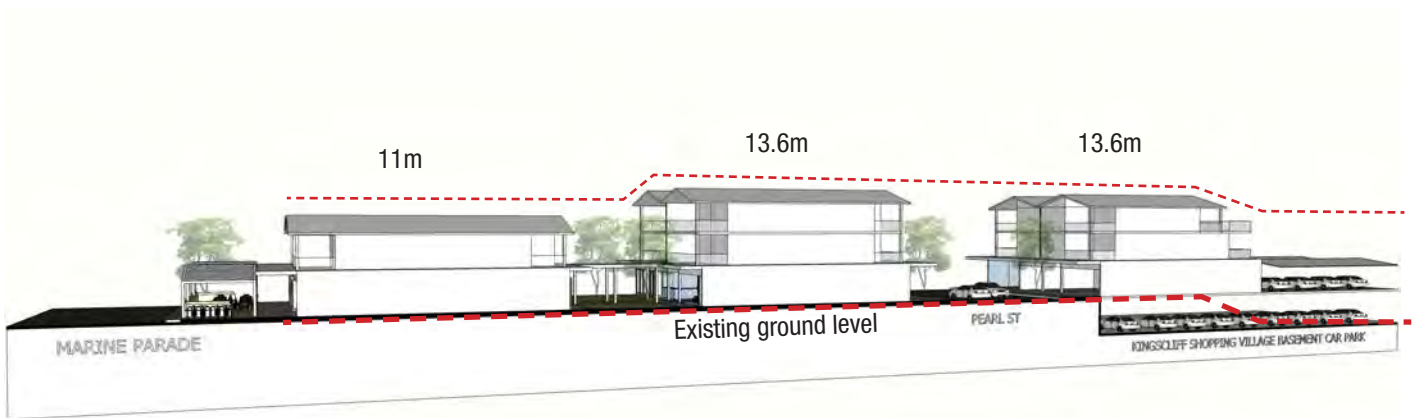


Figure 3.7 Stepping Town Centre Building Height Diagram – Buildings step up in heights back from the beach and coastal reserve to retain the low set character along Marine Parade but facilitate redevelopment of the Kingscliff Shopping Village site and greenfield development sites along Turnbuck St.

## 3.6 Floor Space Ratio

### 3.6.1 Preamble

'Floor Space Ratio' or FSR is defined and mapped within the Tweed Local Environmental Plan 2014. Floor space ratios can be used as a measure of the density of development on a site, and as a mechanism for defining the total development potential of a site, providing the market with a clear indication of potential viability and a means of assessing the potential external impacts of development. Together with height, setback, site coverage and building design considerations, FSR also forms part of the suite of building envelope controls which determine the bulk, scale and mass of a building within a given context.

### 3.6.2 Objectives:

1. Limit the potential for over development and underdevelopment of sites.
2. Provide incentives for positive redevelopment and revitalisation through flexibility and uplift of development potential for key sites.
3. Ensure the potential density of development for a site is coordinated with other built form controls and provision of parking.

### 3.6.3 Controls:

- C1. Development on any site within the Kingscliff Town Centre Precinct is not to exceed an FSR of 2:1 and is to have a minimum FSR of 1:1.
- C2. In order to encourage the positive redevelopment of the Kingscliff Shopping Town site and town centre expansion on greenfield development sites along Turnock Street, Council may permit a floor space bonus of up to 2.5:1 where the applicant prepares a Voluntary Planning Agreement with Council that would provide, at a minimum, the following community benefits:
  - i. High quality architectural design.
  - ii. Incorporation of a town square or public domain area with a minimum site area of 1000sqm.
  - iii. Upgrade of the streetscape immediately adjacent to the development site.
  - iv. Provision of publicly available car spaces within a basement and/or multi- storey car park configuration (10% of total car parking spaces).

### 3.6.4 Additional advice:

Approval of the FSR bonus referred to in 3.6.3 C2 will be at Council's discretion administered through a Clause 4.6 Variation to the TLEP 2014 based on satisfaction of the above criteria and endorsement of an agreed voluntary planning agreement. It is acknowledged that in order to achieve this FSR bonus across certain sites development standards may require variation which will also be determined at Council's discretion.

Where developments include through-block connections in the form of arcades or pedestrian pathways (covered or uncovered) this area is excluded from gross floor area calculations.

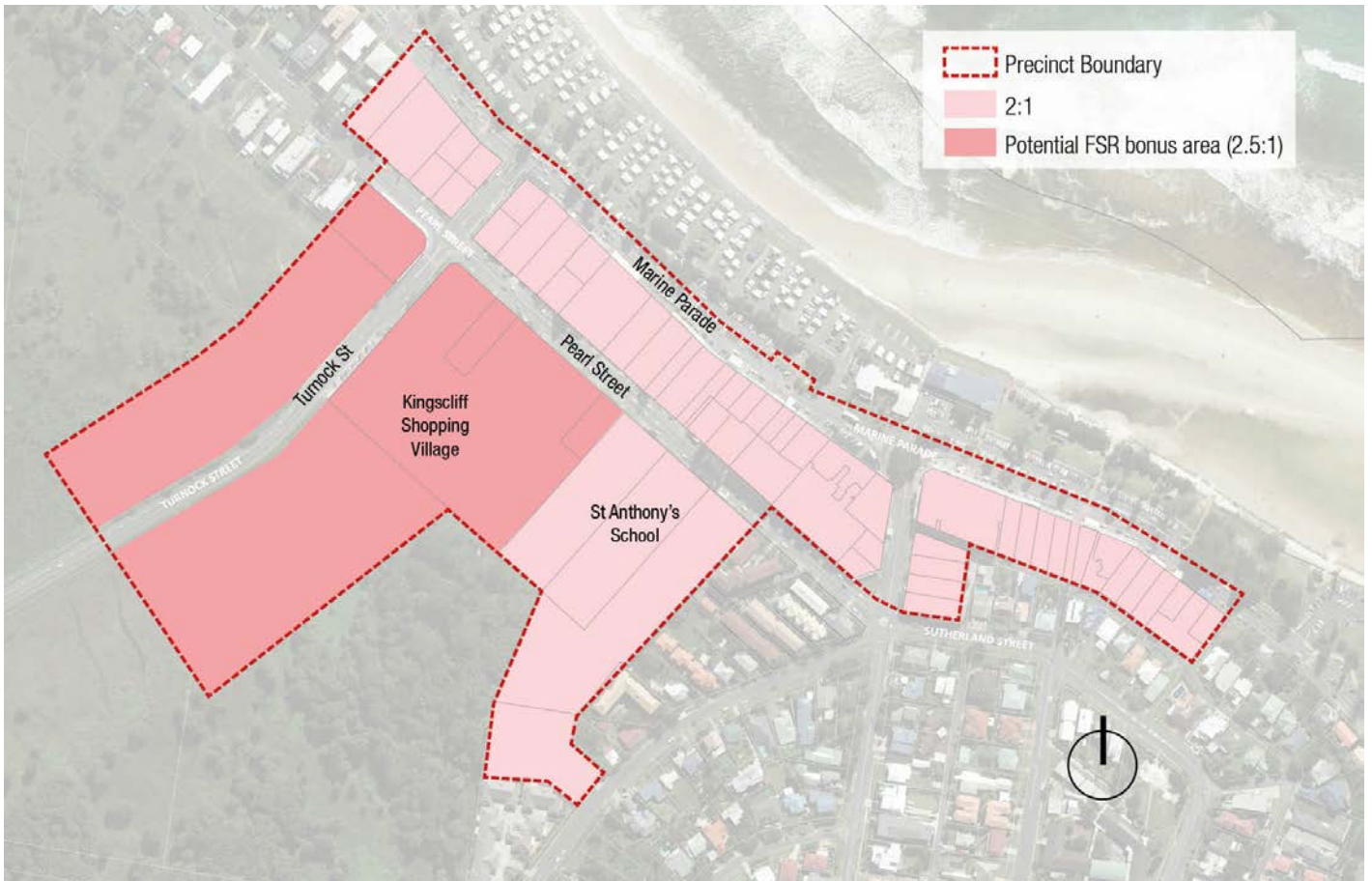


Figure 3.8 Floor Space Ratio – Map of Extents

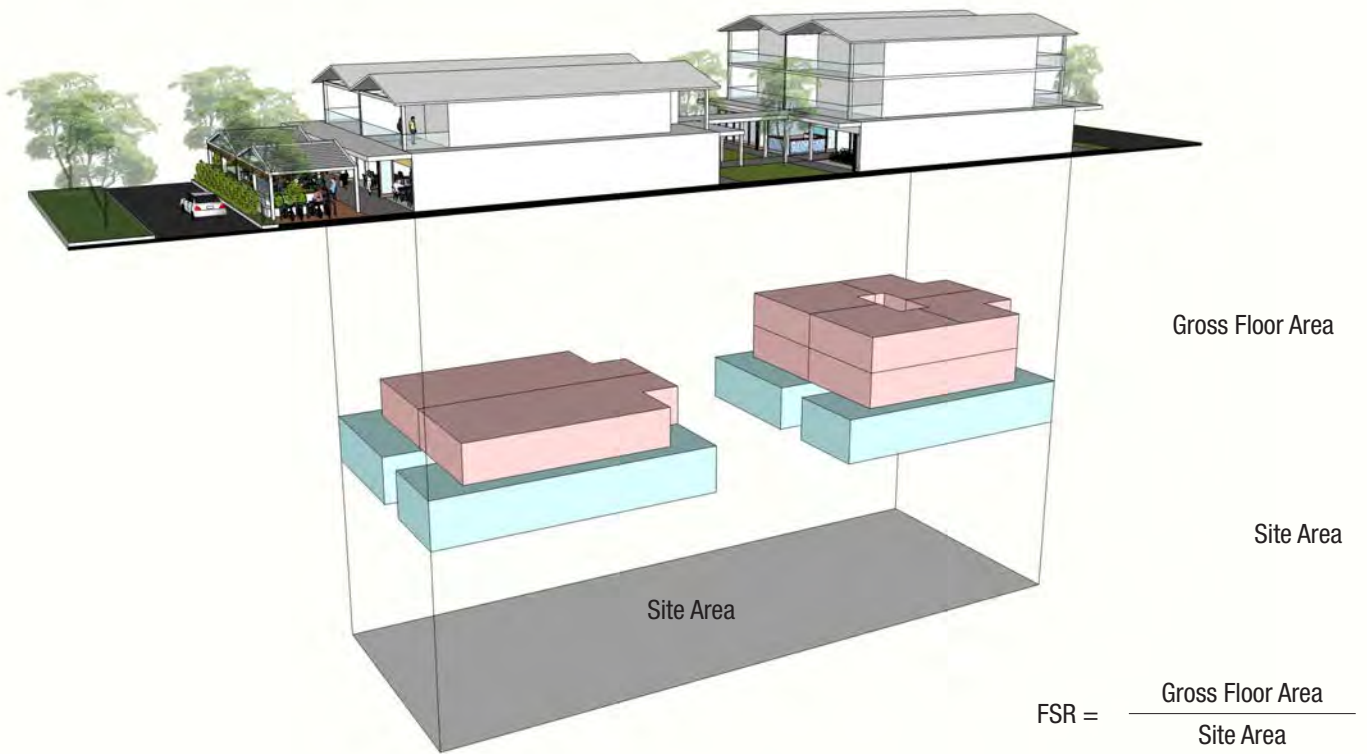


Figure 3.9 Floor Space Ratio Diagram – Promote an appropriate scale of development and avoid under-development within the Town Centre.

## 3.7 Site Coverage

### 3.7.1 Preamble

Site coverage is defined in the Tweed Local Environmental Plan as the proportion of a site that is covered by buildings and is impermeable. It includes all enclosed buildings and at-grade hardstand parking. Limiting site coverage can achieve improved natural ventilation between buildings, improved amenity, improved solar access and greater opportunities for landscape planting and stormwater infiltration.

The Kingscliff Town centre has an established character of less than 100% site coverage across most sites. Whilst many buildings fronting Marine Parade typically have a zero lot setback, some buildings have rear setbacks containing areas of landscape and some have side setbacks providing vehicular access. It is expected that infill development and redevelopment of allotments with dual frontage to Marine Parade and Pearl Street will incorporate an area of landscaped open space.

### 3.7.2 Objectives:

1. Provide an area over town centre sites that enables soft landscaping and deep soil planting, permitting the retention and/or planting of trees that will grow to a large or medium size.
2. Encouraging mid block green areas to enable appropriate building separation, act as light wells and enable natural ventilation and movement of air as well as opening up the potential for views and outlook from upper level units.
3. Limit building bulk on town centre sites and improve the amenity of developments, allowing for good daylight access, ventilation and provision of high quality private and semi-public landscaped spaces.

### 3.7.3 Controls:

- C1. The maximum site coverage of any development within the Kingscliff town centre is not to exceed 80%.

### 3.7.4 Additional advice:

For the purposes of this Plan, site coverage is defined in accordance with the Tweed Local Environmental Plan.

Where developments include through block connections in the form of arcades or pedestrian pathways (covered or uncovered) this area is excluded from site coverage calculations.

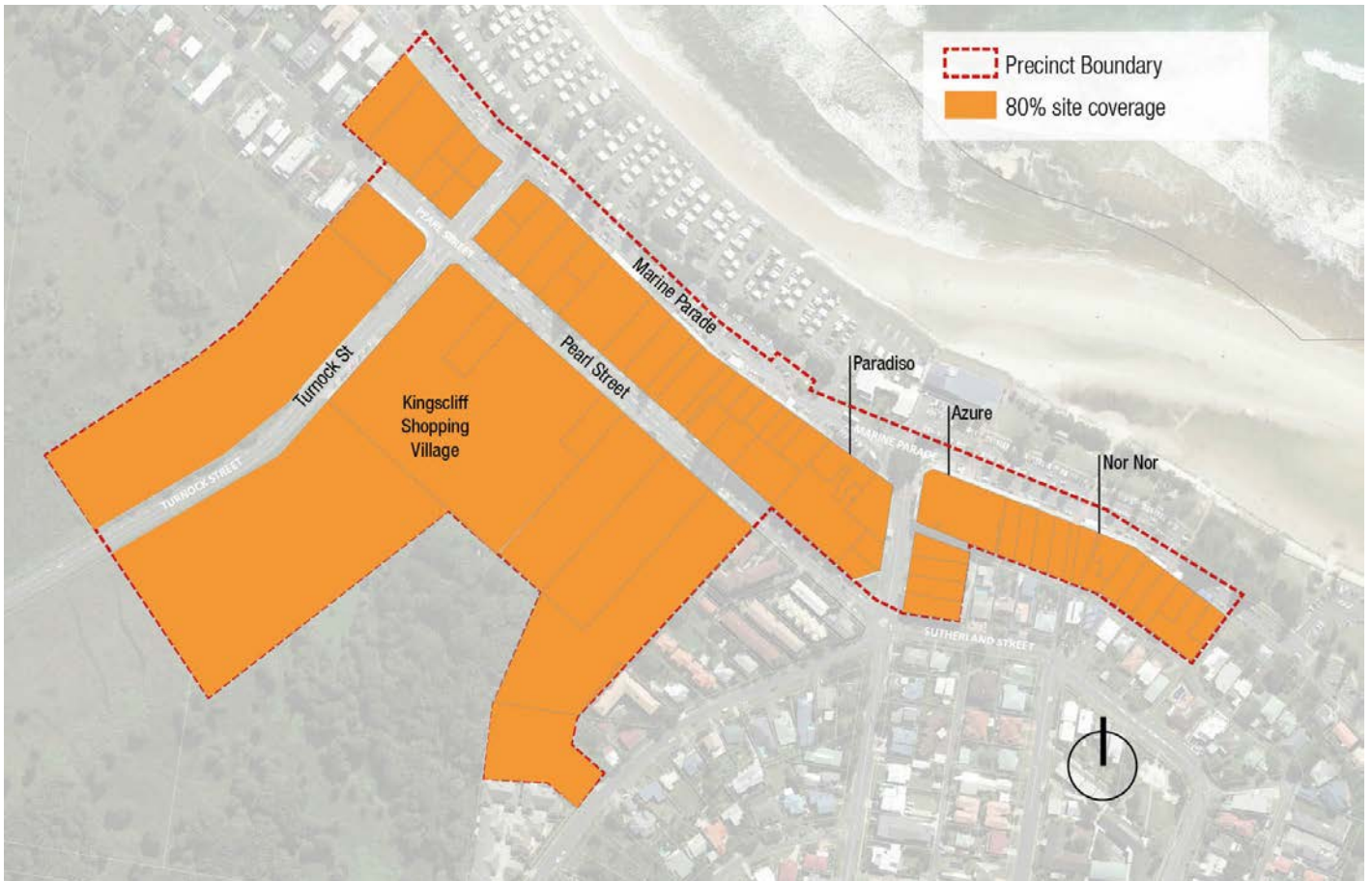


Figure 3.10 Site Coverage – Map of Extents

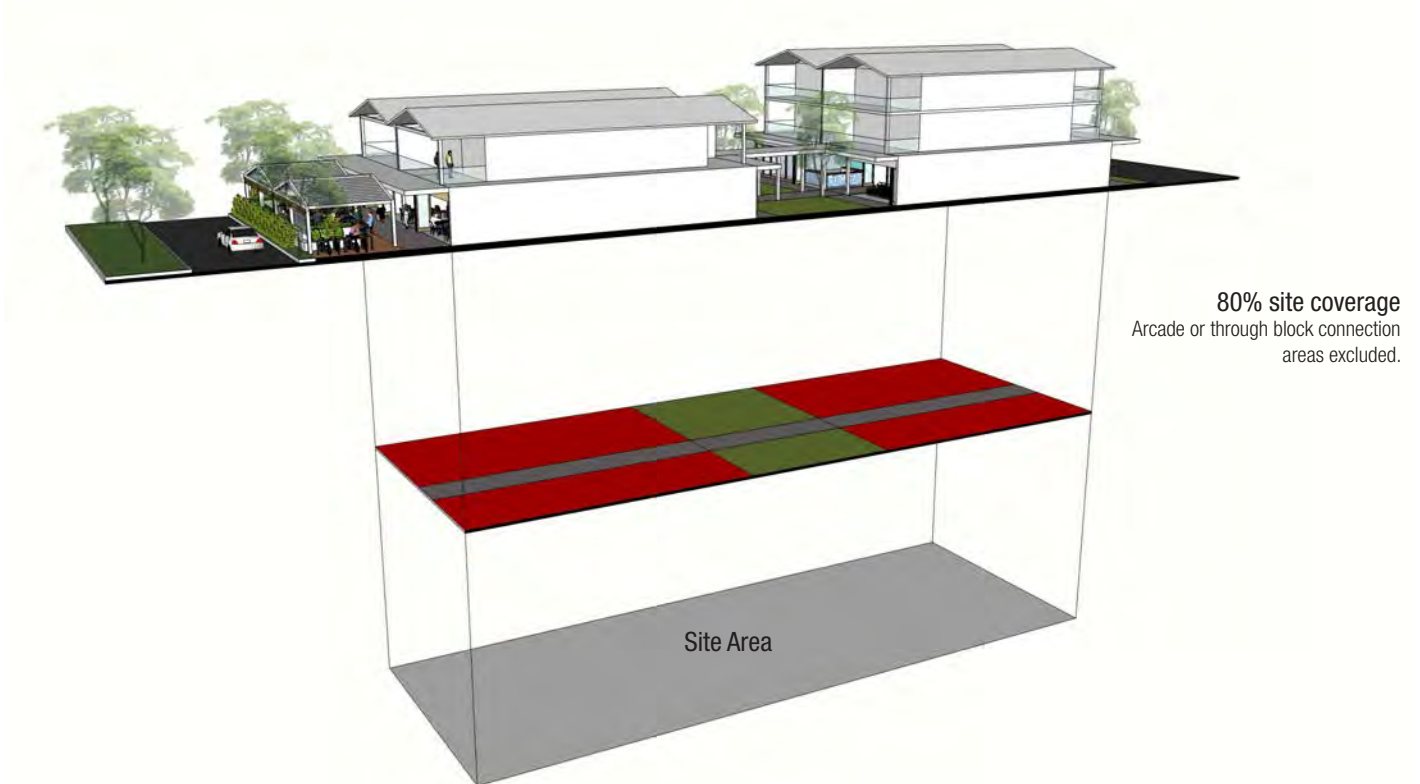


Figure 3.11 Site Coverage Diagram – Promote development which is of a scale, bulk and mass amenable to the Marine Parade character.

## 3.8 Setback and Building Separation

### 3.8.1 Preamble

The character of Marine Parade is defined by the narrow shop fronts, zero front setback, relatively low building height and outdoor dining areas which creates a pleasant and engaging pedestrian environment. New development and infill development within the town centre should contribute to and strengthen this defining character. Achieving setbacks to upper levels will ensure building articulation and reduce bulk. Upper level side setbacks will ensure building separation and enable side window to access light, air and view. Achieving building setback and integrating noise attenuating insulation is particularly important between residential and tourist uses and lower level food and beverage uses where there may be a greater incidence of noise and odour based amenity issues.

### 3.8.2 Objectives:

1. To ensure that new buildings and development maintain a compatible relationship with existing buildings in their presentation to the street and minimise their visual impact on the streetscape.
2. To avoid bulky and continuous upper level development along Marine Parade and Pearl Street, which would undermine the existing built form character, and to provide additional visual interest and diversity to the streetscape.
3. To assist in providing for occupant amenity by way of natural light access, natural ventilation, and views achieved through building separation.

### 3.8.3 Controls:

- C1. Ground floor front building setbacks are identified in Figure 3.12. New development along Marine Parade and Pearl Street are to have minimum 85% of the ground floor building frontage aligned with the street property boundary (zero setback).
- C2. Additional front setback space is allowable on the Kingscliff Village Shopping site and for developments on greenfield sites fronting Turnock Street, provided that the setback area is designed and used as public domain and comprises outdoor dining and/or usable landscaping and embellishment that contributes to the activity, vitality and pedestrian priority of the street edge.
- C3. Upper levels are to be setback 3.0m from the predominant front building line of the ground floor. This setback area may be roofed and utilised as balcony or external living. However, should retain an open outdoor appearance (see Figure 3.13).
- C4. Minimum separation distances between buildings within the town centre shall be in accordance with SEPP 65 Apartment Design Guide (12m between habitable rooms and balconies; 9m between habitable and non-habitable rooms; 6m between non-habitable rooms).
- C5. The building separation provisions of C4 above do not apply to side setbacks for properties fronting Marine Parade or the beach side of Pearl Street within the town centre precinct. Over these sites a zero ground floor side setback is acceptable, and upper floor levels are required to have a minimum 2m setback from side boundaries to create a 4.0m building separation between allotments. Windows along these side elevations project 600mm into the setback to gain access to natural light, ventilation and views if designed to achieve privacy between buildings.

### 3.8.4 Additional advice:

In recognition of the narrow (less than 15 metres wide) allotments fronting Marine Parade and the allotments that adjoin more recent developments with zero side setbacks to upper levels, there may be opportunity to vary the above side setback controls through alternative solutions. Any such proposal must still meet the objectives of this section and must be presented in concept stage where the merits of the alternative solution can be discussed prior to lodging any development application.





Figure 3.12 Setbacks – Map of Extents

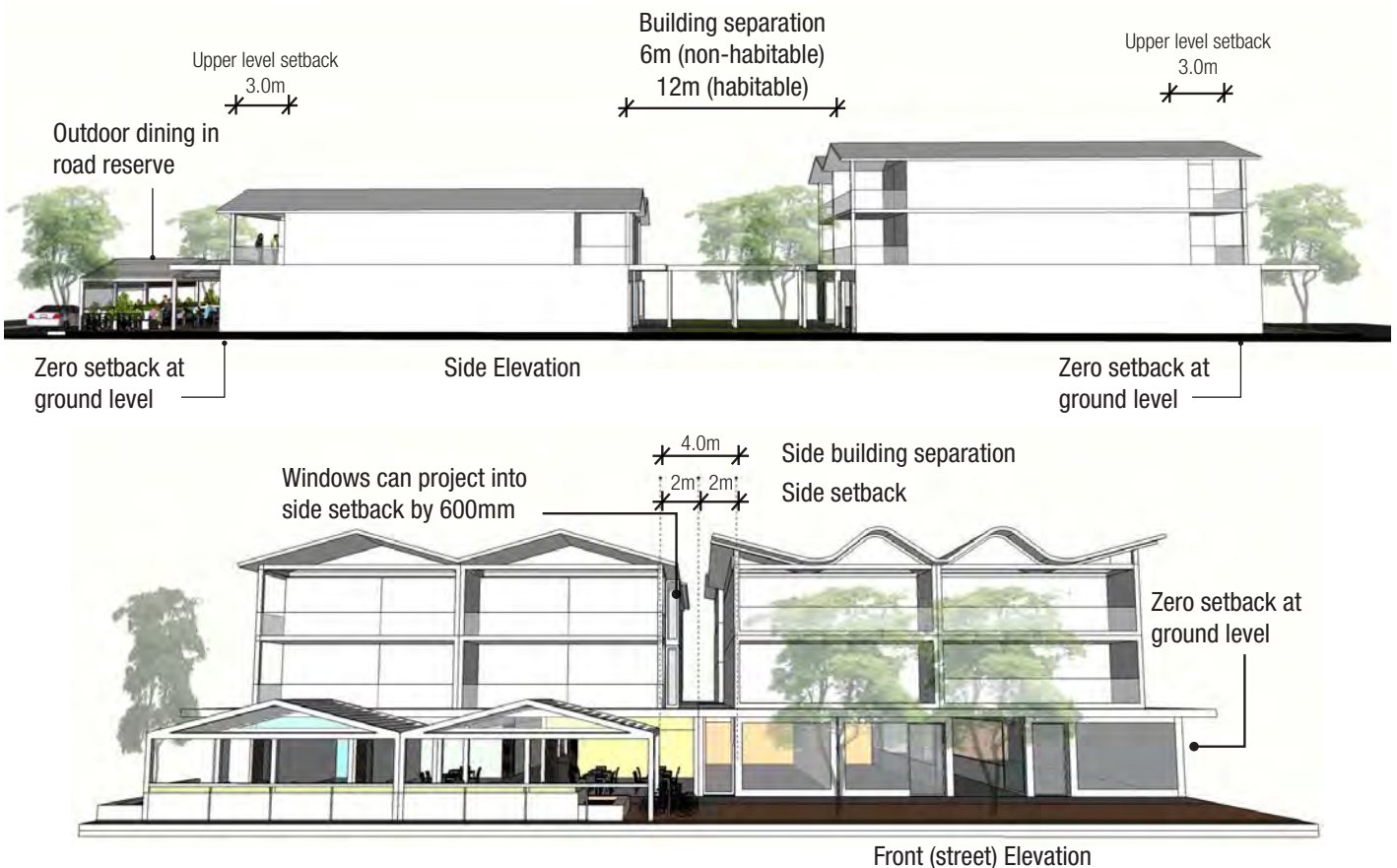


Figure 3.13 Setbacks and Building Separation Diagram

## 3.9 Active frontages and building legibility

### 3.9.1 Preamble

It is important that new buildings and uses within the Kingscliff Town Centre contribute towards creating an active and engaging street edge. The vitality of the town centre relates directly to how a retail unit engages with the pedestrian at the ground level. Narrow shop frontages with large windows and door openings offers visual and retail diversity which defines the existing character along Marine Parade. The average shop front width is 6 metres wide. There is opportunity to extend this 'fine grain' retail character to street edges along Pearl Street and along Turnock Street. Larger floor plate development such as markets should be shrouded in smaller retail units, particularly to the street and public domain edges.

### 3.9.2 Objectives:

1. Achieve a strong definition and create lively and interesting frontages to urban spaces and primary street frontages.
2. Achieve a mix of retail and commercial unit sizes and land use diversity to encourage place vitality and business prosperity.
3. Achieve public surveillance over public spaces and the street.
4. Reinforce a visual connection between the street, pedestrian realm, building entrance and upper level uses.
5. Encourage active uses including food and beverage to upper levels fronting Marine Parade to take advantage of view and aspect whilst achieving privacy and amenity for any upper level residential uses.

### 3.9.3 Controls:

- C1. The ground storey of premises within a defined primary frontage (Figure 3.14) are to incorporate an active front by way of shopfront, windows and doors across 85% of the primary frontage elevation.
- C2. Active uses including food and beverage at first floor level for properties fronting Marine Parade to take advantage of elevated views are encouraged subject to compliance with other relevant legislation including National Construction Code and relevant Australian Standards requirements.
- C3. Shop fronts along Marine Parade are to have a maximum width of 6 metres. Smaller shop frontages will generally be supported. Larger businesses which may extend across multiple retail units should be articulated as narrow shop fronts ( average 6.0 metres).
- C4. Shop fronts are to provide a high degree of visual transparency with no dominant graphics obscuring the view between the footpath and retail units and where required grille or translucent security screens rather than solid shutters, screens or roller-doors. Use of bifold doors and/or generously sized shopfront windows is encouraged to promote active and visually engaging retail unit elevations.
- C5. The external facades of buildings are to be aligned with the streets that they front and all building entries are to be clearly identifiable from the street.
- C6. Where a building is located on an allotment that has more than one primary frontage design entries and or shopfront windows to addressing both frontages.



Figure 3.14 Active Frontages – Map of Extents

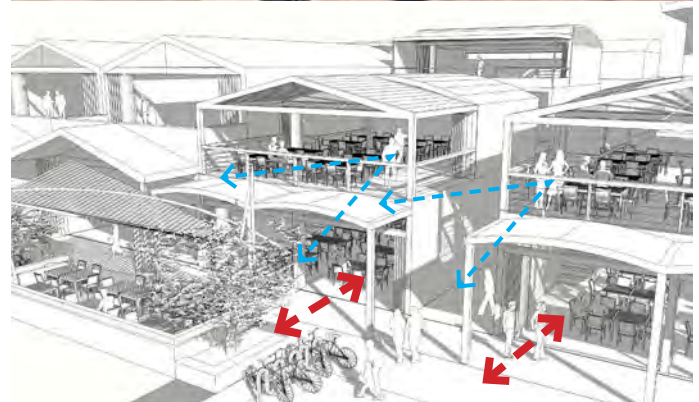
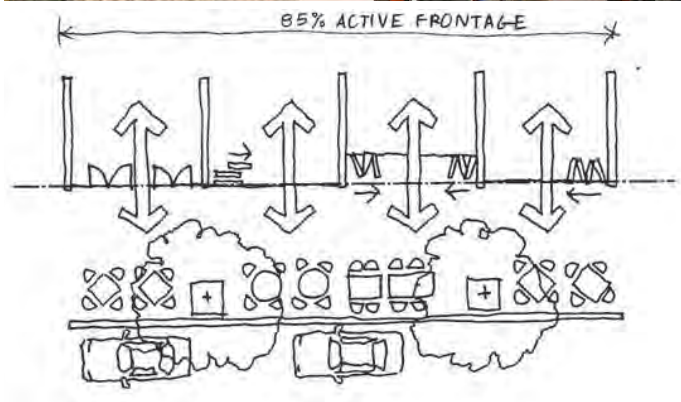


Figure 3.15 Active frontages and Building Orientation Diagrams – Buildings should respond to the street in terms of entrance, overlooking and facade treatment.

## 3.10 Building Form, Materials and Passive Design

### 3.10.1 Preamble

Building form combined with material and fenestration composition (windows, doors, openings and skylights) influences the bulk, scale, mass and visual appearance of a building. This in turn influences streetscape and the visual amenity of the broader town centre precinct. Particularly within the town centre it is important to have attractive and well designed buildings which will strengthen the coastal character and enhance the pedestrian experience. Consideration should also be given to the life cycle of materials, the longevity and maintenance requirements particularly given the marine environment.

### 3.10.2 Objectives:

1. Ensure building form, architectural features, materials and colours are utilised to achieve attractive streetscapes, address other important interface elevations and improve building design.
2. To encourage finishes and building materials appropriate to the local climatic conditions, solar orientation and site specific features.
3. To encourage a mix of materials which serve to break down the overall scale, bulk and mass of large buildings.
4. To encourage building design that presents a harmoniously coordinated streetscape throughout the town centre.
5. To encourage the use of non-toxic and low embodied energy materials that minimise the impact on the environment.
6. To encourage materials and finishes that are durable, high quality and low maintenance which assist in the thermal comfort and amenity of the building.

### 3.10.3 Controls:

- C7. In addition to town centre structure and urban design controls nominated at section 3.4, the design of new buildings and substantial alterations and additions in the town centre precinct are to address the following:
  - i. All new buildings should be designed to have a distinctive street level base, middle and top. Articulate the different three dimensional elements of the building by expressing structure, volume, form and material mix.
  - ii. Upper levels and roofs of buildings are to contribute to the articulation of the overall building form and roof line.
  - iii. Buildings are to be articulated with at least 3 different building materials to create material variation. Large expanses of single use material is not acceptable.
  - iv. All upper level uses should include balconies, verandahs, semi indoor/outdoor spaces, light wells and the like to achieve adequate access to natural light and ventilation and architectural interest.
  - v. Incorporate the use of screening, sun shading, window hoods and elevation recesses for solar control and privacy to create architectural interest.
  - vi. Provide screens to conceal loading, storage, rubbish disposal, plant, equipment and other similar uses in side and rear areas.
- C2. Coloured elevations and 3D visualisations of new development including adjoining buildings/streetscape context and a schedule of colours and materials and coloured elevations shall be submitted with development applications.

- C3. Development applications for new buildings and substantial alterations and additions are to demonstrate the integration of the following passive design principles:
- i. Design to control solar access into the building during different times of the day and year to reduce reliance on artificial lighting and to maximise sun entry during winter months and maximise sun shading (especially north, western and south western elevations) during summer months.
  - ii. Design to promote natural cross ventilation and stack ventilation.
  - iii. Materials are chosen for their climatic appropriateness, low toxicity and having low embodied energies in their production.
  - iv. A high level of energy efficiency through building design, passive solar design, insulation, minimised reliance on mechanical HVAC (Heating, Ventilation and Air Conditioning), and lighting systems and smart metering.
  - v. Collection and reuse of rainwater throughout the building and surrounding landscaping areas.
  - vi. A high standard for an energy efficient hot water supply system.
  - vii. Reduced non-renewable energy use through efficient heating/cooling systems, water supply systems and use of renewable energies.

#### **3.10.4 Additional advice:**

Applicants are encouraged to engage the services of a registered architect in the preparation of design concepts, documentation and administration of construction in order to achieve high quality design and detail in construction and best practice in sustainability.

For shop top housing and residential flat buildings refer to SEPP 65 and the Apartment Design Guide and Tweed Development Control Plan DCP A1 Part C.

## 3.11 Mixed Use Development

### 3.11.1 Preamble

Mixed use buildings can include combinations of retail, commercial, residential and tourist accommodation uses and would thereby potentially fall under different building classifications under the national construction code (NCC). It is important to note that differing building classes require specific design elements to be considered including access requirements and fire ratings. As such the engagement of a specialist design consultants early in the process is encouraged.

Within the town centre, mixed use development including residential or tourist accommodation including associated car parking is only appropriate where adequate vehicular access and car parking can be achieved. Due to physical site limitations there are parts of Marine Parade and Pearl Street where mixed use development would be difficult to achieve due to the inability to provide access and onsite car parking. Over these constrained sites the integration of upper level commercial floor space or food and beverage uses, where monetary car parking contributions can be made in lieu of on-site car parking, may be more achievable. New vehicle access off Marine Parade for new development will generally not be supported.

### 3.11.2 Objectives:

1. Encourage a mix of land uses contributing to the vibrancy of the town centre.
2. Provide opportunities for additional residential and tourist accommodation within the town centre.
3. Promote mixed use development and shop top housing over development sites within the town centre where there is adequate access and provision of on site parking preferable in a basement configuration.
4. Ensure appropriate residential amenity is fostered in the design of mixed use development in Kingscliff Town Centre.
5. Provide enhanced surveillance and activity within the town centre.
6. Provide opportunities for non-retail development in order to broaden the economic base and attractiveness of the town centre.

### 3.11.3 Controls:

- C1. Mixed use development including shop top and tourist accommodation (upper levels) is encouraged within the town centre where onsite car parking can be provided for residential and tourist accommodation land uses.
- C2. On constrained allotments where car access and onsite car parking can not be achieved, the integration of upper level commercial floor space or food and beverage uses, where monetary car parking contributions can be made in lieu of on site car parking, is encouraged.
- C3. The upper floors of mixed use buildings must have clearly defined and separate entrances from the street frontage.
- C4. The entrances for upper floor activities is not to occupy more than 10% of the street frontage.
- C5. Mixed use buildings must establish clear sight lines to the public domain, whilst allowing for suitable privacy for upper storey residential apartments;
- C6. All parking is to be located at the rear or in a basement car park, accessed via a rear lane wherever possible. Vehicle access off Marine Parade will not be supported unless it can be demonstrated this is a development sites only street access.

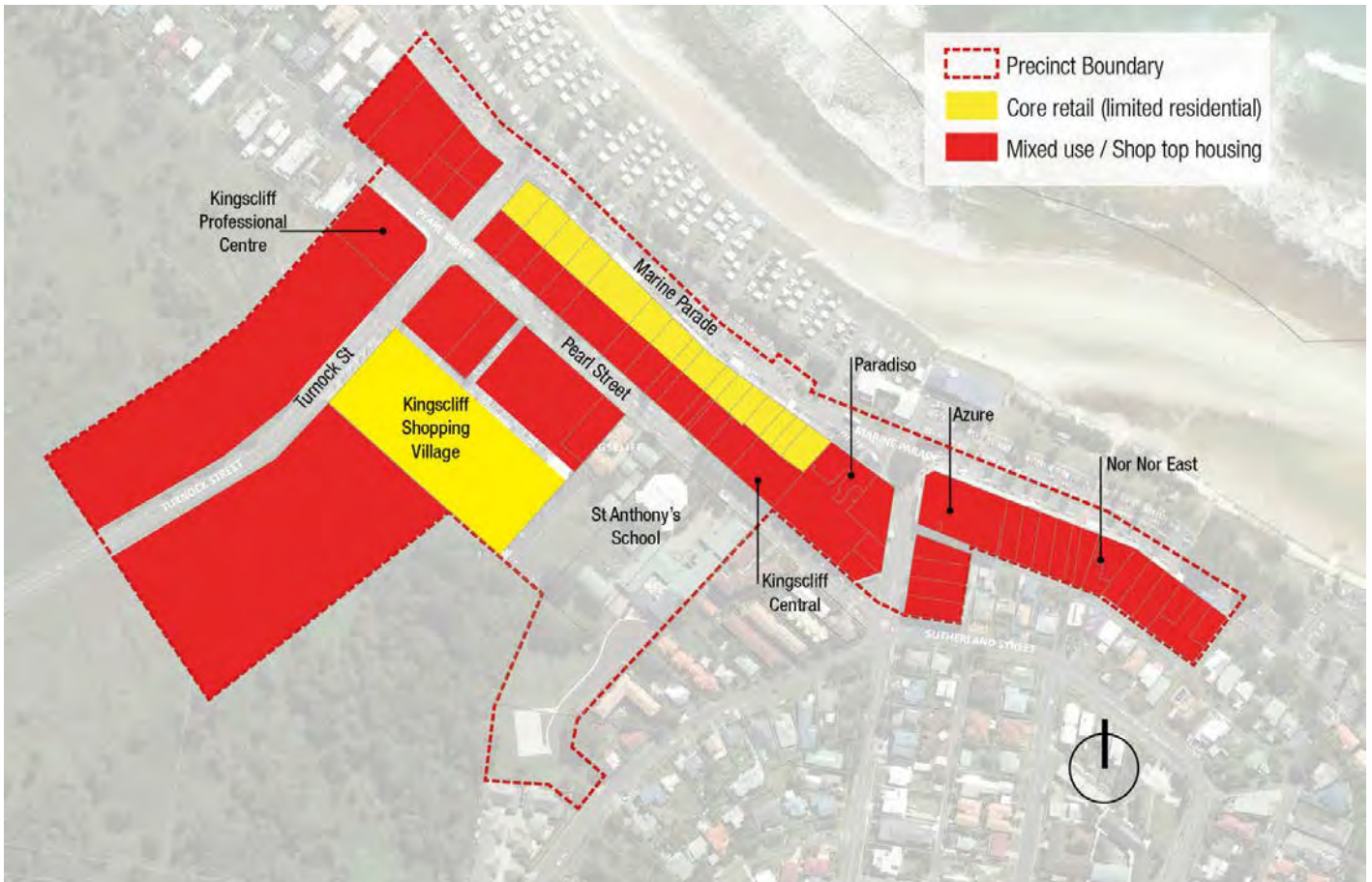


Figure 3.16 Mixed Use Development – Map of Extents

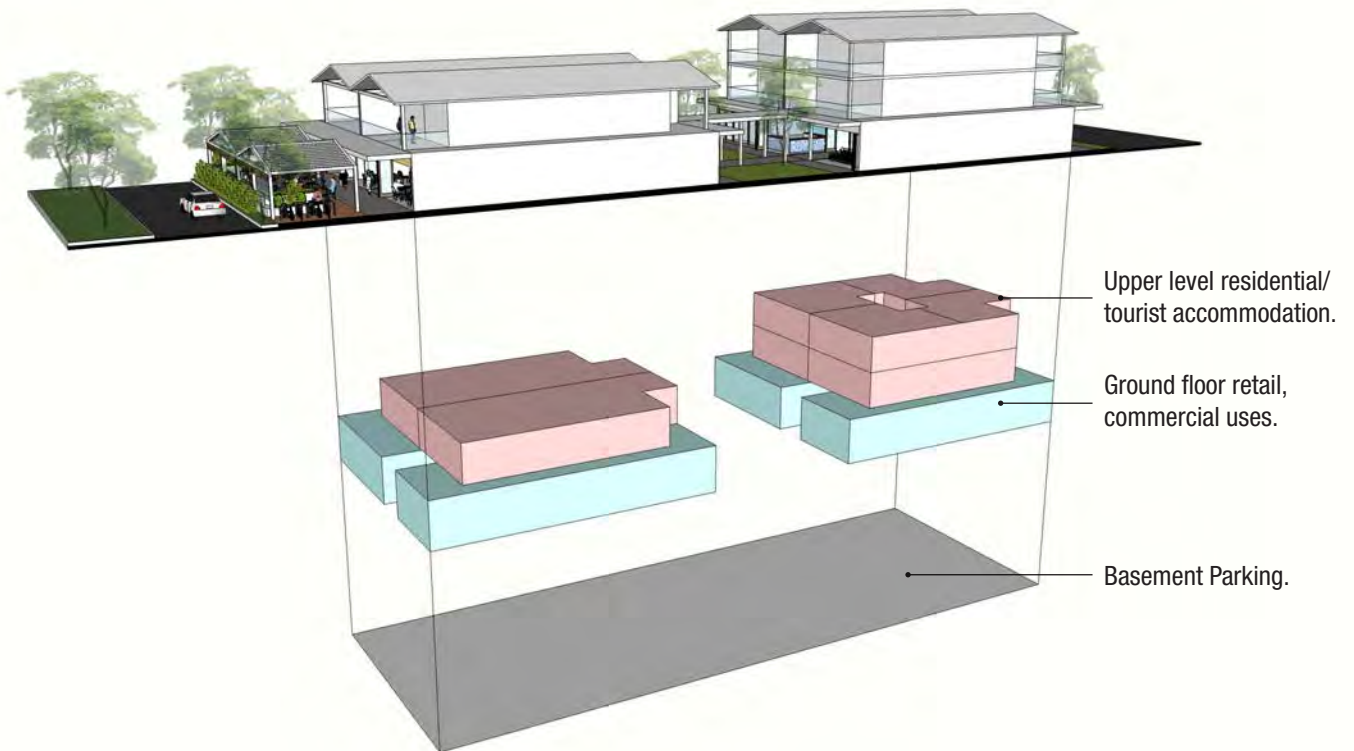


Figure 3.17 Mixed Use Development Diagram – Promote Infill Development with a mix of uses such as shop top housing where onsite car parking can be provided.

## 3.12 Large Floor Plate Development

### 3.12.1 Preamble

Large floor plate development comprises development over 1000sqm GFA. Large floor plate development typically includes uses such as supermarkets, small department stores and mini majors, fresh fruit and produce markets, health centres, larger commercial office uses and larger civic buildings such as community centres and libraries.

The extension of the town centre to the west along Turnock Street presents opportunity to develop a variety retail and commercial land uses including of small, medium as well as larger floor plate uses. As a greenfield development site it is imperative to concurrently master plan these town centre development allotments with an optimised traffic movement, car parking access and public transportation network.

Large floor plate retail and commercial development typically require significant amounts of car parking based on car movements that these landuses generate. It is important that in the context of the future expansion of the Kingscliff town centre that car parking areas are well designed and located towards the rear of the development sites so that they do not visually and physically dominate primary street frontages. Similarly large floor plate development typically results in larger and longer building elevations. It is important that each elevation is appropriately designed and articulated to avoid large featureless building expanses, particularly where those elevations have a public interface.

### 3.12.2 Objectives:

1. Facilitate the future development of larger floor plate development within the Town Centre precinct to the west of Pearl Street in order to broaden the retail and economic base of the town centre.
2. To avoid large featureless elevations which front prominent street edges, corners or public domain.
3. To ensure traffic management and provision of car parking is appropriately designed to accommodate associated car movements and that service access is rationalised.
4. To ensure an appropriate level of pedestrian amenity and connectivity is achieved in association with large floor plate development.

### 3.12.3 Controls:

- C5. All new large floor plate development applications are to be accompanied by a:
- Statement of design intent considering urban design principles (Figure 3.5).
  - Coloured streetscape elevations and 3d images with material palette.
  - Traffic and car parking management plan demonstrating the adequate provision of car parking, coordinated access, service, loading and unloading arrangements.
- C1. Public domain and landscape plan including pedestrian circulation arrangements in the broader town centre and site specific context, landscape treatment of car parking areas with adequate areas for water sensitive urban design treatment, infiltration and shade trees. Ensure active building edges and public domain define prominent street frontage rather than car parking areas. Car parking should be designed and located to the rear of the site wherever possible.
- C2. Activate at least 80% of the building frontage with a generous and identifiable building entrance, smaller scale retail units sleeving the larger floor plate development, display windows and human scale built form elements including pedestrian paths, shade structures, awnings, outdoor amenity spaces and landscaping. Large expanses of blank or unarticulated elevations to primary frontages are not permitted.
- C3. Large format signage, branding across whole elevations or large format images across display windows is not permitted.



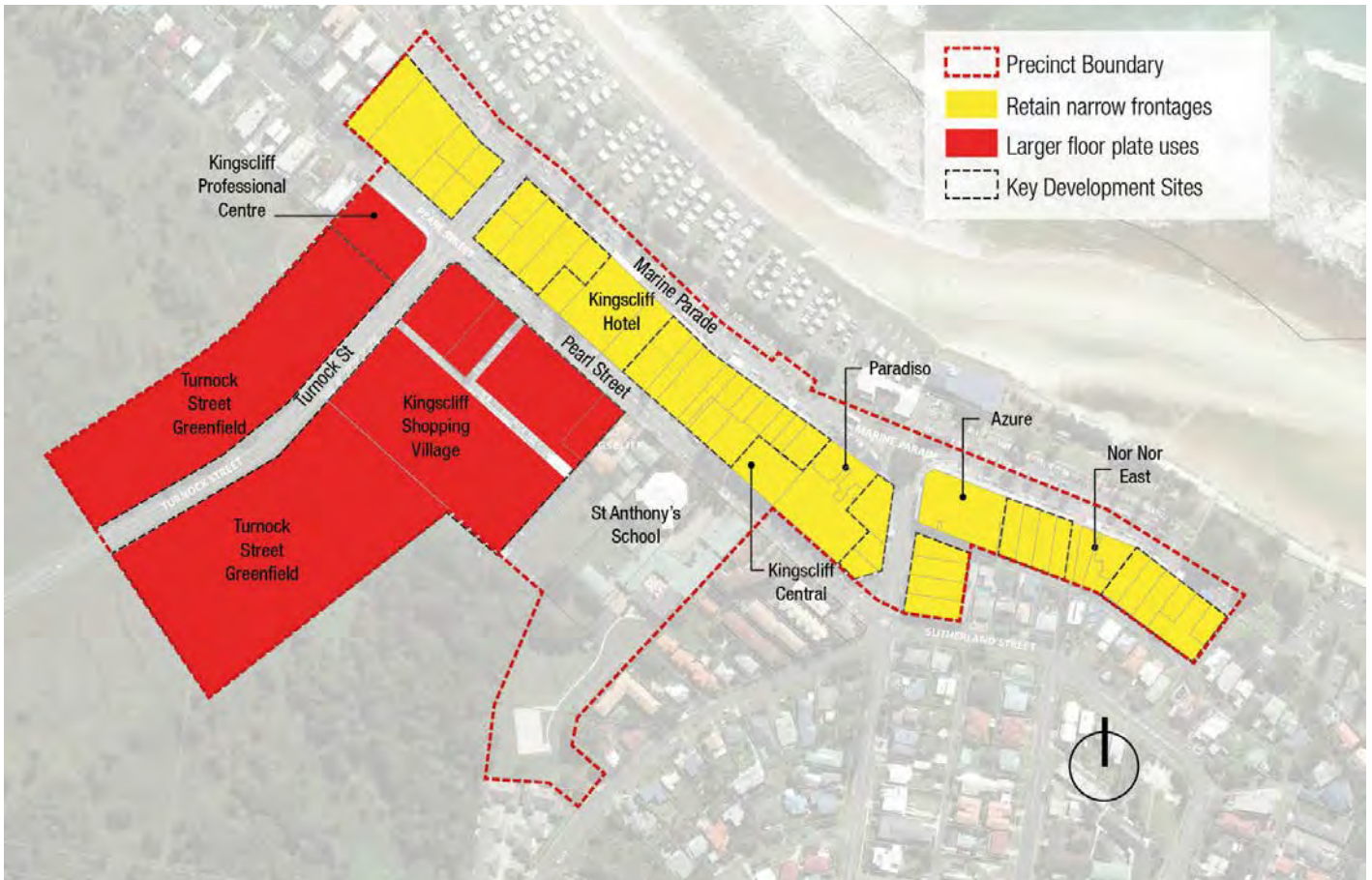


Figure 3.19 Large Floor Plate Development – Map of Extents



**Large floor plate Development** – Large retail and commercial uses should be well articulated and provide well designed legible pedestrian friendly spaces.

### 3.13 Corner Sites

#### 3.13.1 Preamble

Corner buildings are highly visible and provide the opportunity for character defining architectural outcomes. Well designed corner buildings which address and respond to the public interface can significantly influence the visual character and experiential qualities of the town centre precinct. In this respect, they are important elements in terms of framing and providing definition to both a street and town centre block. There are a number of design options that can be pursued on corner buildings including a zero setback which defines the street edge, zero setback but visually and physically open with large openings or semi enclosed spaces or be recessed to allow public domain.

#### 3.13.2 Objectives:

1. Enhance street edge definition, legibility and presentation of key corners within the Town Centre.
2. Provide a strong architectural design address for important junctions to enhance and frame the built form and visual character of the town centre.
3. Achieve an area of public domain addressing the Turnock Street and Pearl Street intersection over the Kingscliff Shopping Village site.

#### 3.13.3 Controls:

- C1. All development applications for new buildings or substantial alterations or additions on corner site within the town identified in Figure 3.21 are to:
- Provide an activated edge by way of building opening or shopfront to each street frontage.
  - Provide a strong design contribution to the definition and shaping of the street edge through buildings elevations, awnings and roof form.
  - Provide landscape and public domain treatment to reinforce the significance of the corners.
  - Provide illustration of proposed building form, architectural and material elements which articulate and address the corner context which should include coloured elevations and 3d images/montages of proposals set within the development sites surrounding context.
- C2. Incorporate an area of public domain on the corner of Pearl and Turnock Street over the Kingscliff Shopping Village site as part of any staged development application, concept application or master plan. This prominent corner may be expressed through a combination of building edge, public domain and landscape areas.



Figure 3.20 Montage of Pearl and Turnock Street Corner Treatment – There is opportunity for buildings on the corner of Pearl and Turnock St to frame the street with an active edge and frame a pedestrian entrance into a new town square over the Kingscliff Shopping Village site.



Figure 3.21 Key Corner Sites – Map of Extents



**Corner Treatment Images** – Corners can be delineated either with an active and/or open edge (left) which blurs the boundary between public and retail unit or articulated with expressive built form (right).

## 3.14 Awnings and Weather Protection

### 3.14.1 Preamble

Awnings provide weather protection and contribute to a pleasant and comfortable pedestrian environment. With relatively long hot summers and higher than state average rainfall, ideally pedestrians should be able to traverse different parts of the town centre under an awning. Whist Marine Parade currently has a relatively good awning coverage, other parts of the town centre have limited or no weather protection. New development fronting Marine Parade, Pearl St and the future town centre expansion area along Turnock Street are required to continue providing continuous awnings to all primary and active edge street frontages. Within these areas, consideration will be given to the design of double storey awnings to create outdoor rooms and benefit ground floor as well as first floor uses.

### 3.14.2 Objectives:

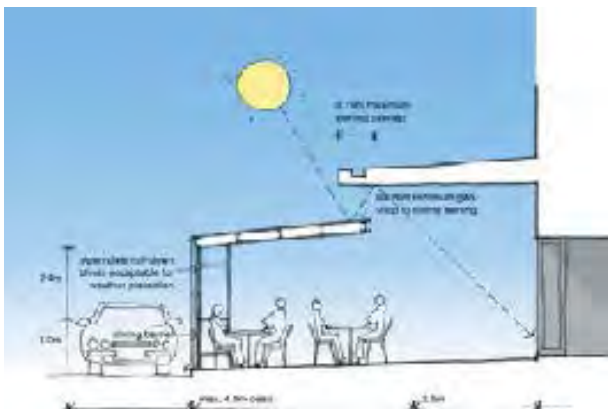
1. Provide weather protection in areas of high pedestrian traffic and in semi-public space.
2. Encourage the use of consistent and continuous awnings where retail shop fronts and active edges are provided.
3. Provide opportunity for double height awnings to create well shaded and landscaped outdoor rooms.

### 3.14.3 Controls:

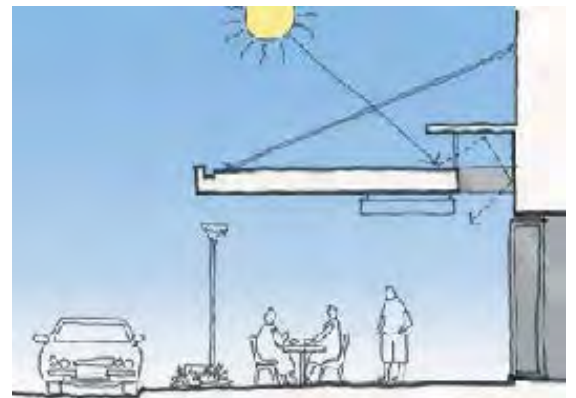
- C1. Provide continuous awnings for all retail and food and beverage shop fronts and active edges that are located within the town centre in accordance with Fig 3.22 cantilevered from the facade where possible to minimise obstructions to pedestrians.
- C2. Where buildings are set back from the street frontage boundary, alternate weather protection to the created semi-public space is to be provided. This could include pergolas, fabric screens, shade trees etc;
- C3. Awnings and shade structures should be designed to:
  - Exclude hot summer sun but allow for natural ventilation.
  - Articulate significant building entries.
  - Provide shelter to the street, but, where deep awnings occur, bring natural light to the shop front.
- C4. Awnings should have a minimum soffit height of 3.3 metres and be coordinated such that there are no more than minor variations in height between awning structures on adjacent buildings. Double volume awning design will be considered where compatible design, pedestrian amenity and streetscape character can be demonstrated and justified through 3d modelling and contextual street elevations.
- C5. Awnings should have a minimum depth of 3 metres and provide under awning lighting. Where a depth of 3 metres cannot be achieved due to the proximity of the carriageway to the property boundary, the outside edge of the awning is to be aligned to within 600mm of the kerb line.
- C6. Any under awning sign, treatment and lighting is to maintain a minimum clearance of 2.8 metres from the level of the pavement.



Figure 3.22 Awning and Weather Protection – Map of Extents



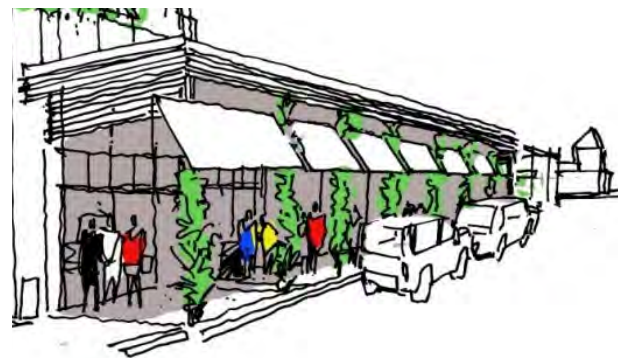
Integrated lower set awning over outdoor dining area.



Design opportunity for shade as well as diffuse light.



Higher awnings creates shaded volume to street edge.



Integrate shade features and landscape for pedestrian comfort and visual amenity.

Figure 3.23 Design Consideration for Awnings. Images: Architectus & Cairns tropical urbanism

## 3.15 Outdoor Dining Areas

### 3.15.1 Preamble

Outdoor dining areas are a key built form and streetscape character element of Marine Parade which facilitate a growing food and beverage niche contributing to the town centre vitality. There is potential for several additional outdoor dining areas with permanent shade structures to be provided within the town centre particularly along Marine Parade and Turnock Street. It is recommended that whilst each shade structure need not be identical, they should draw from the same architectural language of material and form; each contributing to a cohesive whole.

Conceptually an 'outdoor room' has an inherent lightness of structure and form and visual transparency which should be incorporated to maintain an 'openness' to the streetscape and provide practical shade and weather protection over the outdoor dining areas.

### 3.15.2 Objectives:

1. To facilitate outdoor dining areas along the street edge to encourage the food and beverage niche and contribute to the streetscape and activation and vitality of the town centre.
2. Ensure that outdoor dining areas are accessible, cool, and attractive spaces that positively contribute to the ambience of the street and the Kingscliff coastal character.
3. To ensure a degree of design cohesion between each individual outdoor dining area.
4. To ensure safe accessible pedestrian movement is maintained.

### 3.15.3 Controls:

- C1. All outdoor dining shall be in accordance with Council's Footpath Trading Policy and approval procedures (requires separate approval under s.138 of the Roads Act 1993) which may require endorsement of a voluntary planning agreement to set out management and maintenance responsibilities.
- C2. The dining areas built into the road reserve should be at footpath level. Level changes must be made with AS1428 compliant ramps.
- C3. Where outdoor dining areas would require development over onstreet car parking, replacement car parking contribution is to be made in accordance with relevant s.7.11 contributions plans (CP23).
- C4. Outdoor dining should be located on the street side of the footpath and maintain at least 2.0 metres of footpath between the outdoor dining edge and shop front to ensure a clear pedestrian passage. Outdoor dining areas should be offset by at least 600mm from the kerb edge to account for vehicle 'over swing'. Where separate outdoor dining areas adjoin, a 1.0 metre width between is to be maintained to allow pedestrian movement from the kerb to footpath.
- C5. Outdoor dining areas should be accompanied by a landscape plan and ideally have raised planters at each end (to delineate the space) and a 1.0m deep landscape area (can be planters) to the street edge as a means of physical separation and contribution to the 'greening' of the streetscape.
- C6. Shade structures and outdoor dining areas are to use materials, colours and architectural detailing that contribute to the coastal identity of Kingscliff and complements the desired streetscape character of the town centre precinct. Consideration should be given to the long term maintenance and corrosive influence of the marine environment.

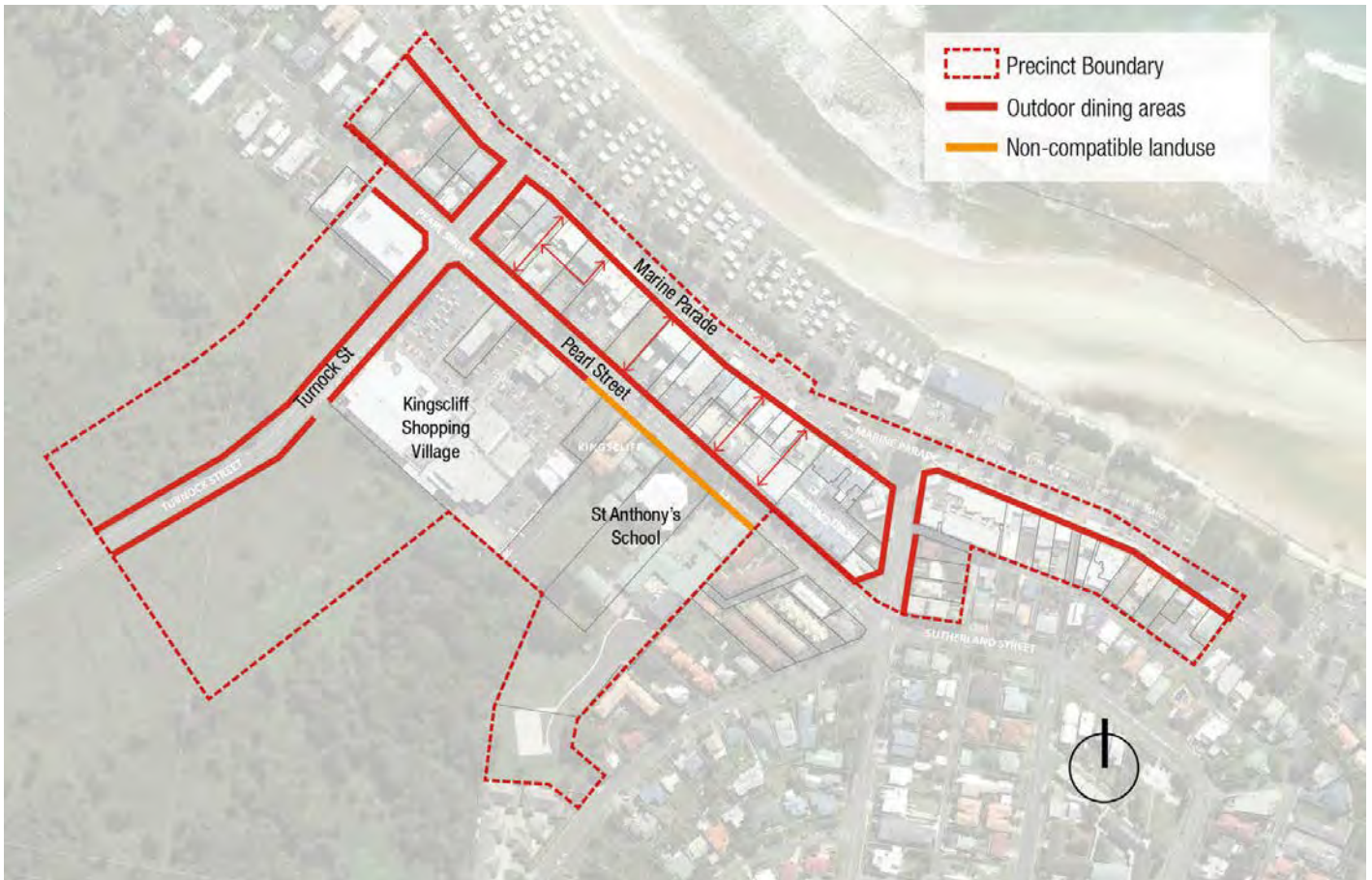


Figure 3.24 Outdoor dining areas – Map of Extents



Figure 3.25 Outdoor dining areas – Design outdoor dining areas to achieve universal access and a balance between rain, wind and UV protection, shade and filtered light. Use a mix of marine tolerant materials and integrate landscaping, street furniture and bike parking.

## 3.16 Signage

### 3.16.1 Preamble

Signage is essential to business premises and if designed and integrated well can contribute rather than detract from the streetscape and town centre character. Within the existing Marine Parade retail strip, business signage is generally subservient to the large shopfront windows and doorways. Businesses should seek to design new signage as an extension of the business use, business name and architecture of the building in terms of styling, materials and colour. Corporate branding or large format imaging deployed across shopfronts and building elevations are typically not supported.

### 3.16.2 Objectives:

1. To confine advertising and signage to appropriate, consistent and visually acceptable locations and ensure the scale proportion and form of advertising devices and entry features are appropriate to the streetscape or other settings in which they are located.
2. Promote a high standard of signage quality and prevent excessive advertising and visual clutter within the Kingscliff town centre by encouraging the rationalisation of existing and proposed signs.
3. Ensure that advertising and advertising structures are compatible and complement and do not detract from the coastal character of Kingscliff.
4. Promote adequate and effective advertising and recognise the legitimate need for signs to provide for directions, business identification and promotion.

### 3.16.3 Controls:

- C1. All signage is to comply with SEPP 64 and Tweed DCP A4 – Advertising Signs Code.
- C2. Signs are to be designed and treated as an extension of the architecture of the building reflecting the building in terms of styling, materials and colour and to minimise visual clutter. Within the primary signage control area under awning signage with a clearance of 2.8m to the footpath is acceptable. Large elevation and fascia signage (to parapets, awning fascias, drop down awnings etc) will not be supported.
- C3. Signage devices are to be constructed of robust materials so as to minimise ongoing maintenance.
- C4. All applications for new buildings or the substantial refurbishment of an existing building shall detail the design provisions made to accommodate future signage within submitted elevations and schedules.
- C5. Signs or large format images and lifestyle pictures are not to cover windows or detract from the architectural qualities of a building.
- C6. Signage devices shall not obstruct the passage of pedestrians or vehicles or impact on traffic safety and do not obstruct sight lines. Signage devices placed on footpaths and street edges (e.g. “a-frame” display boards) are not permitted.
- C7. Pole or Pylon signs are not permitted on key corners. Pole or pylon signs associated with the Kingscliff Shopping Village site are to be located generally in accordance with Figure 3.27.



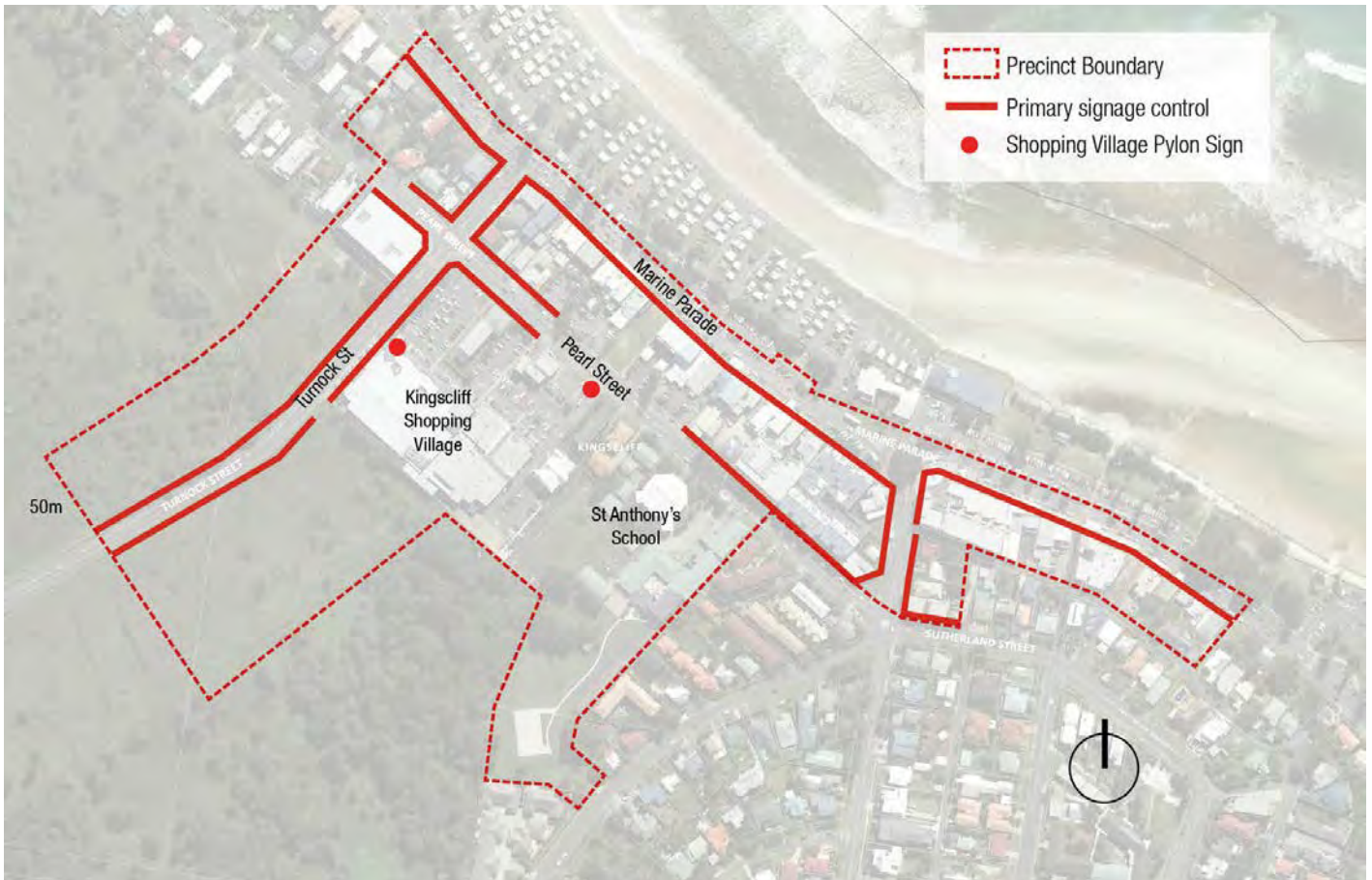


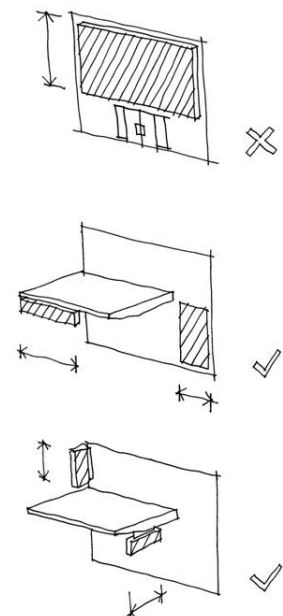
Figure 3.26 Signage – Map of Extents



Signs are to be designed and treated as an extension of the architecture of the building and land use in terms of styling, materials and colour so as to minimise visual clutter.

Photo: Bangalow.

Figure 3.27 Design Consideration for Signage



## 3.17 Access and car parking

### 3.17.1 Preamble

Providing a range of car parking opportunities within the town centre precinct will assist with dispersing traffic movements and car parking pressures throughout the precinct. Whilst there is limited available land area to increase car parking within Marine Parade and Pearl Street, there is opportunity to integrate required on-site car parking and additional public car parking within new town centre greenfield development areas along Turnock Street.

The existing urban structure and narrow lot configuration fronting Marine Parade and the eastern side of Pearl Street makes vehicular access and provision of on-site and basement car parking difficult without allotment amalgamation. In order to encourage future redevelopment within this area, there is opportunity through the existing s.7.11 Plan No.23 Off Site Parking Spaces to make monetary contributions towards a centralised multi-storey car park in lieu of off street parking where suitably justified for non-residential development.

New development sites along Turnock Street are less site constrained and are expected to develop sufficient off street car parking including basement car parking to meet land use requirements. It is important that in the context of the future expansion of the Kingscliff town centre that car parking areas are well designed and located towards the rear of the development sites so that they do not visually and physically dominate primary street frontages.

### 3.17.2 Objectives:

1. Facilitate flexibility in the provision and procurement of car parking within the town centre precinct to achieve land use objectives and greater pedestrian priority along Marine Parade.
2. Promote the development of centralised car parking area(s) through basement and or multi- storey car parks over key development sites to service the broader town centre precinct.

### 3.17.3 Controls:

- C1. The Tweed DCP A2 Site Access and Parking Code applies to the Kingscliff Town centre precinct.
- C2. Development applications proposing over 1000sqm of GFA within the town centre is to be accompanied by a traffic study.
- C3. A single service lane for co-ordinated access across adjoining sites is to be provided off Turnock Street servicing the existing Kingscliff Shopping Village site and future town centre expansion area west along Turnock Street.
- C4. Over the Kingscliff Shopping Village site any future redevelopment proposal which contains a multi-storey car park should include a 'green' or dynamic facade to contribute to the streetscape. If a proposed multi-storey car park is proposed to be adjoining Pearl Street, it should be shelved behind retail unit frontages to Pearl Street and have a pedestrian awning running the length of the multi-storey car park street frontage.
- C5. Car parking over town centre greenfield expansion areas are to be designed and located either in basements and/or the rear of development sites. At grade car parking areas are to integrate landscape design with adequate areas for water sensitive urban design treatment, infiltration and shade trees. Large car parking areas interfacing with primary street frontages is to be avoided.

### Additional Information:

Car parking within the town centre precinct can be provided off-site via a monetary contribution (in accordance with Section 7.11 Plan No. 23 – Off Site Parking Spaces) for the land use categories identified in Table 2(c) and 2(g) within Tweed DCP A2 Site Access and Parking Code. Off site monetary contribution provisions do not relate to residential or tourist accommodation land uses where on-site provisions as per DCP A2 apply.

Land owners are encouraged to engage with Council early in the design process to ascertain opportunities for joint venture development partnership to provide additional public accessible car parking.



Figure 3.28 Access and Car Parking – Map of Extents



**Multistorey car park design Images** – Within the town centre a multi-storey car park will be pursued as part of the Kingscliff's shopping Village redevelopment. Given it is likely the elevation of the car park will front Pearl St, there is design opportunity to introduce a dynamic or green facade that will soften the car park elevation and contribute to the streetscape.

## 3.18 Public Domain and Civic Improvements

### 3.18.1 Preamble

Public domain and civic improvements within the town centre will include through block pedestrian linkages between Pearl Street and Marine Parade, wider footpaths, increased landscaped areas and additional street trees. There is also opportunity to create a town square as part of any future staged redevelopment of the Kingscliff Shopping Village site and new public domain areas to town centre greenfield development sites fronting Turnock Street. The Turnock Street greenfield development site will significantly expand retail, commercial and community uses with opportunity to also integrate a new bus interchange. Over time this expansion along Turnock Street will shift the core retail and commercial focus from Marine Parade which will continue to expand its boutique retail, food and beverage offer.

With a growing resident population there is also a need to multi-purpose community facility within or within proximity of the town centre. As part of additional public domain and streetscape improvement works, there is also an opportunity to further connect the surrounding adjoining native vegetation into the town centre through a continuous canopy of street trees which will line road and pedestrian networks.

### 3.18.2 Objectives:

1. To improve pedestrian amenity, connectivity, safety and universal accessibility within the town centre precinct.
2. To create vibrant and safe areas of public open space which are of high quality design and include embellishment befitting of a town centre.
3. To provide a focus for social and recreational activity and public life.
4. To meet the public domain and open space needs of residents in an equitable manner.

### 3.18.3 Controls:

- C1. Public domain, civic improvements and community facilities are to be provided in accordance with Figure 3.29 and Table 8. Where identified community benefit is indicated over private land, landowners are encouraged to engage with Council early in the design development process in regards to public domain and civic improvement scope and brief, site design, procurement and timing as part of future (re)development.
- C2. All development applications for new buildings over greenfield development sites and substantial new building works over the Kingscliff Shopping Village site are to prepare an open space and landscape master plan in consultation with Council. This plan shall detail the treatment of the public domain and open space within the land subject to the 'schedule of identified public domain improvement works' (Figure 3.28 and Table 8). The following considerations and design detail should be included where relevant:
  - general layout
  - plant species and sizes (at time of planting and maturity)
  - vehicular, cyclist and pedestrian safety
  - public art
  - street furniture
  - drinking fountains
  - signage
  - feature fencing
  - earthworks
  - connections to cycleways and pedestrian paths
  - utilities and services
  - hard and soft landscaping treatments
  - shade structures/outdoor dining areas
  - play equipment
  - planter boxes
  - safety features & lighting
- C3. Integrate universal design principles and address 'Safer-by-Design' principles (including the NSW Police 'Safer by Design' crime prevention through environmental design (CPTED) principles) in all public and private domain development.

- C4. The universal design of public domain areas and facilities (including car parking requirements) for people with disabilities must comply with the relevant Australian Standard (AS 1428 Pt 1 and 2, AS 2890-1 Off Street Car parking or as amended) and the Disability Discrimination Act 1992 (as amended) including but not limited to:
- Provide continuous and universally accessible paths of travel from all public roads and spaces, as well as unimpeded internal access.
  - Provide durable materials commensurate with the standard of the adjoining public domain (street), with appropriate slip resistant materials, tactile surfaces and contrasting colours for all pedestrian access ways, public domain areas, entry paths and lobbies.
- C5. Provide lighting (LED smart lighting) to the relevant Australian standards to all pedestrian access ways, parking areas and building entries.
- C6. All landscape areas and planter beds including any public water features design are to integrate with WSUD systems where appropriate and use re-circulate, treat rainwater.
- C7. All pedestrian and cycleway routes and facilities are to be consistent with the Austroads guides for cycleways; Guide to Road Design Part 3 Geometric Design AGRD03-09 and Guide to Road Design Part 6A Pedestrian and Cyclist Paths AGRD06A-09.
- C8. Pedestrian and cycleways are to be constructed as part of the infrastructure works for each stage of development. The infrastructure staging needs to cover the primary routes as part of the essential street network.

### 3.18.4 Additional Information:

Update relevant Section 7.11 Plans to reflect identified public domain and civic improvements.



Opportunity for a small town square on the Kingscliff Shopping Village Site.



Opportunity for additional footpath widening and landscape along Marine Parade.



Well designed through block connections and arcades.

Image: DFJ Architects



Multi-purpose community facilities within the town centre or Turnock St Precinct.

Image: Greenway Architects

Public Domain Precedent Images

| No. | Public Domain Element  | Objective  | Location  | Area/Dimension/Quality                                | Indicative cost*  | Timing /Delivery  | Responsibility  |
|-----|--|--|---|---|---|---|---|
| 1   | Town Square  | Provide a defining civic space to the Kingscliff town centre   | Kingscliff Shopping Village Site Cnr Pearl and Turnock St   | Minimum 30mx30m >900sqm                               | 900sqm @ \$2000psm<br>Total – \$1.8m                                      | Redevelopment of KSV site.  | Owners of KSV.<br>Design in consultation with TSC.  |
| 2   | Basement and multi-storey car parking                            | Provide 100 public car spaces to serve the broader town centre.  | Kingscliff Shopping Village Site off Pearl St   | Min.100 public car spaces                             | 100 spaces @ \$25000 per space<br>Total – \$2.5m                          | Redevelopment of KSV site.  | Owners of KSV / TSC<br>Design in consultation with TSC  |
| 3   | Basement and multi-storey car parking                            | Provide 100 public car spaces to serve the broader town centre   | Turnock St development sites  | Min.100 public car spaces                             | 100 spaces @ \$25000 per space<br>Total – \$2.5m                          | Development of Turnock Street development sites.  | Owners of Turnock St development sites.<br>Design in consultation with TSC.                                   |
| 4   | Multi-purpose community facility                                 | Provide a new multi-purpose facility to meet the needs of the existing and future resident population.                   | Turnock St Greenfield Development site.   | 3000sqm   | 3000sqm @ \$3453psm<br>Total – \$10.3m                                    | Development of Turnock St Greenfield development site.  | TSC and Turnock St greenfield development site land owner.<br><br>Land dedication and s.7.11 plan.            |
| 5   | Pearl St Footpath Improvements (west)                            | Upgrade pedestrian footpath  | Western side of Pearl Street fronting the KSV site.   | Approx. 140m x 3m wide                                | 420sqm @ \$400psm<br>Total – \$168 000                                    | Redevelopment of KSV site.  | Owners of KSV / TSC<br>Design in consultation with TSC.   |
| 6   | Pearl St Footpath Improvements (east)                            | Upgrade pedestrian footpath  | Eastern side of Pearl Street.   | Approx. 240m x 3m wide                                | 720sqm @ \$400psm<br>Total – \$288 000                                    | Redevelopment of KSV site.  | TSC.  |
| 7   | Pearl St bus and taxi stop upgrade                               | Upgrade bus stop to accommodate future rapid bus transportation system. WIFI and IT integrated.                          | Western side of Pearl Street fronting the KSV site.   | 40m length x 3m wide                                  | 120sqm @ \$2000psm + IT and wifi upgrades<br>Total – \$0.30m              | Short term  | TSC funded through s.7.11 plan / grant.   |
| 8   | Turnock St Footpath Improvements (south)                         | Upgrade pedestrian footpath  | Southern side of Turnock Street fronting the KSV site.  | Approx. 130m x 3m wide                                | 390sqm @ \$400psm<br>Total – \$156 000                                    | Redevelopment of KSV site.  | Owners of KSV/TSC<br>Design in consultation with TSC.   |
| 9   | Turnock St Footpath Improvements (south)                         | New pedestrian footpath  | Southern side of Turnock Street fronting the Turnock St development site.                                       | Approx. 370m x 3m wide                                | 1110sqm @ \$400psm<br>Total – \$444 000                                   | Redevelopment of Turnock St development site.   | Owners of Turnock St development sites.<br>Design in consultation with TSC.                                   |
| 10  | Turnock St Footpath Improvements (north)                         | New pedestrian footpath  | Northern side of Turnock Street fronting the Turnock St development site.                                       | Approx. 370m x 3m wide                                | 1110sqm @ \$400psm<br>Total – \$444 000                                   | Redevelopment of Turnock St development site.   | Owners of Turnock St development sites.<br>Design in consultation with TSC.                                   |
| 11  | Turnock Street cycle path upgrade                                | Upgrade cycle path within Turnock St road reserve including safety bollards, surface painting, lighting and landscaping. | Northern side of Turnock Street fronting the Turnock St development site.                                       | Approx. 370m x 3m wide                                | 1110sqm @ \$400psm<br>Total – \$444 000                                   | Redevelopment of Turnock St development site.   | TSC funded through s.7.11 plan.   |
| 12  | Through block connections between Pearl Street and Marine Parade | Improve pedestrian connectivity throughout the town centre precinct.   | Lot 1 DP 740505<br>Lot 2/3 DP 11315<br>Lot 1 DP 573057<br>Lot 2 DP 400977<br>Lot 3 DP 418672<br>Lot 1 DP 781719 | Approximately 60m long (varies) minimum width of 3.0m | Varies.<br>Through block connections can be open air or enclosed arcades. | Redevelopment of subject allotments.  | Owners of subject allotments.<br>Concessions on site coverage and FSR requirements to offset connection area. |
| 13  | Marine Parade Footpath widening                                  | Improve the pedestrian amenity along Marine Parade and delete parallel parking spaces on western side of Marine Parade.  | Western side of Marine Parade.  | 720sqm  | 720sqm @ \$400psm<br>Total – \$288 000                                    | Short – medium term.<br>Dependent on providing replacement car spaces within a multi-storey car park. | TSC.  |

| No. | Public Domain Element                | Objective  | Location   | Area/Dimension/Quality                                    | Indicative cost*   | Timing /Delivery  | Responsibility                  |
|-----|--------------------------------------|--|--|---|--|---|---------------------------------|
| 14  | Marine Parade Landscape Improvements | Increase the amount of soft landscaping along Marine Parade and include additional street furniture concurrently with footpath widening. | Marine Parade (east and western sides)   | 6 landscape beds<br>Street Furniture<br>Upgraded lighting | \$0.8m   | Short – medium term.<br>Dependent on providing replacement car spaces within a multi-storey car park. | TSC.                            |
| 15  | Marine Parade Cycle Path             | Include a cycle path within the road reserve along Marine Parade.  | Marine Parade road reserve   | Paint road surface.<br>1.2m wide                          | \$0.08m  | Short   | TSC funded through s.7.11 plan. |
| 16  | Street tree planting                 | Increase street tree planting, areas of infiltration and improve visual amenity of the town centre.                                      | Marine Parade<br>Pearl St<br>Turnock St  | 200 trees   | 200 @ \$1000 per tree (including planting)<br>Total – \$0.2m | Short   | TSC.                            |
| 17  | Footpath and lighting                | Upgrade and form a continuous footpath with smart LED lighting.  | Western side of Marine Parade north of town centre. (Intersection with Turnock St to intersection with Wommin Bay Rd). | Footpath approx. 250m x 3m wide<br><br>Street lights      | \$0.5m   | Short - medium term.  | TSC / grant.                    |

\*All noted costs are indicative only and subject to cost estimations based off detailed design.

Table 8 Kingscliff Town Centre Public Domain and Civic Improvements



Figure 3.29 Public Domain and Civic Improvements – Map of Extents





## **PART 04 RESIDENTIAL AREAS**

- 4.1 Residential Development – Locality Wide**
- 4.2 Beachfront Precinct**
- 4.3 North Kingscliff Precinct**
- 4.4 West Kingscliff Precinct**
- 4.5 Kingscliff Hill Precinct**
- 4.6 Salt Precinct**
- 4.7 Seaside Precinct**
- 4.8 Cudgen Village**



## 4.1 Residential Development – Locality Wide

### 4.1.1 Preamble

#### Vision

*Kingscliff's residential precincts will deliver a diverse range of housing types with a strong focus on responding to the local character and subtropical coastal context. This includes undertaking site analysis to determine design opportunities, designing to integrate passive design principles and using appropriate building materials to improve household comfort whilst reducing energy usage. Greenfield development precincts will include a diversity of housing types with increased density in and around activity centres taking advantage of the walkable access to the town centre and coastal reserve areas. An increased housing supply with a more diverse range of housing types will facilitate housing which caters to a wider demographic and affordability profile.*

#### Housing diversity mix

Achieving housing diversity which caters to the wide demographic and affordability needs of the broader community is a high priority of the Kingscliff Locality Plan. Table 2.1 is provided to guide and encourage residential mix and density throughout Kingscliff's future new residential precincts.

Achieving appropriate housing diversity within existing residential estates which may include additional dual occupancies, manor houses, terrace houses, row houses, small lot houses and secondary dwellings is also encouraged where TLEP development standards (land use zoning, height, FSR, lot size) and TDCP controls can be achieved.

#### Site Planning and Sustainable design

Undertaking a site analysis is the first step in understanding a sites opportunities and constraint and provides valuable information to inform design decisions. All residential development sites including large greenfield development areas or individual allotments should integrate passive design principles including a consideration of solar orientation, prevailing winds direction, shading or landscape considerations.

Understanding these elements will assist in determining best site design and floor plan configurations to maximise natural light and air flow to indoor and outdoor living spaces. The consideration of passive design principles will assist in achieving thermal comfort levels throughout the year as well as reducing overall energy usage.

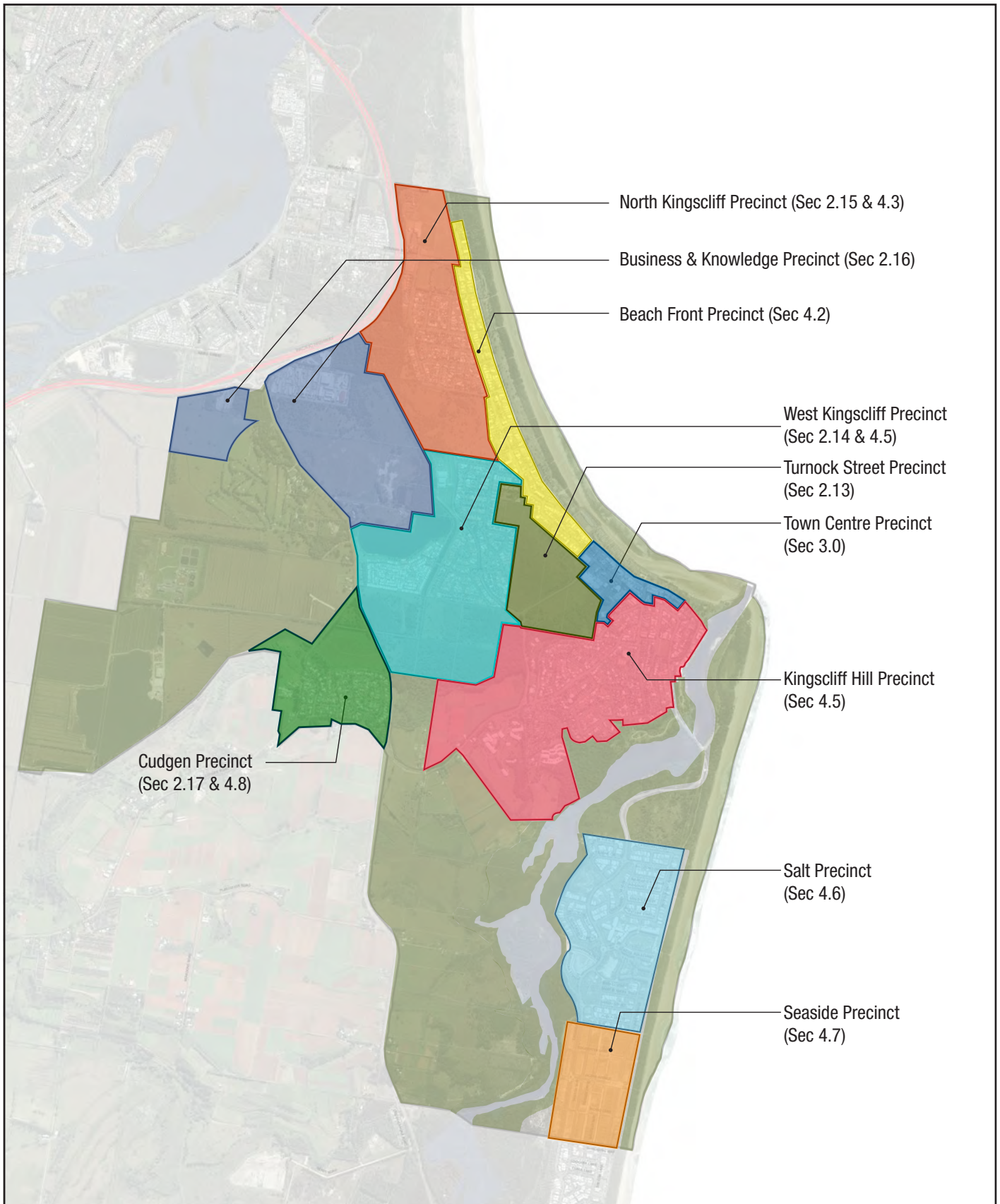
#### Building materials

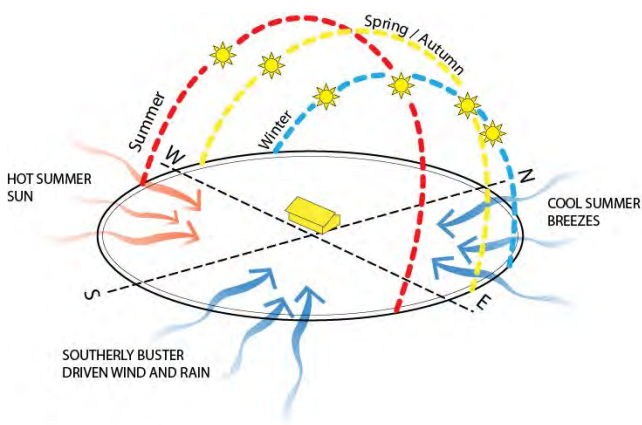
The choice of building materials not only has an influence on the visual appearance of the building itself but also influences streetscape character, thermal performance, building health, maintenance requirements and running costs.

Finishes and building materials chosen should be appropriate to the local climatic conditions, solar orientation and the site specific aspects of a site and building design. It is also important to consider how your building materials are produced particularly in relation to the use of VOC's (Volatile Organic Compounds) some of which can have long term health effects.

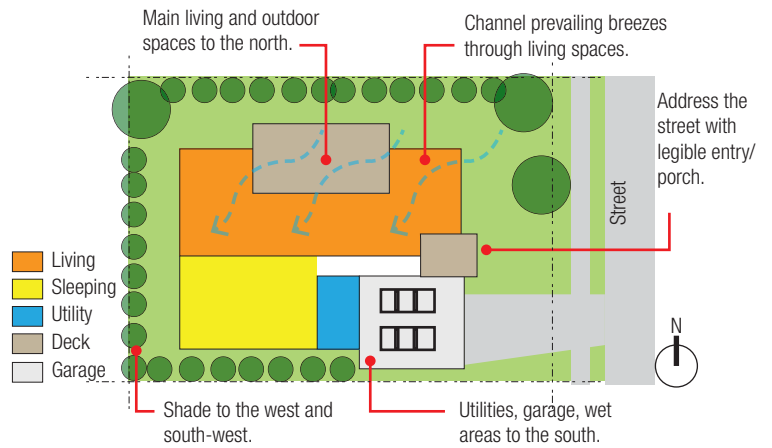
New residential buildings should aim to use a mix of materials which provide visual interest which breaks down bulk and scale as well as assisting with a buildings thermal performance.

Consideration should also be given to the life cycle of materials and the longevity and maintenance requirements of particular materials to a given location. For example, some metals will rust and corrode very quickly within a coastal context. Similarly many hardwoods (apart from the extremely dense varieties such as Ironbark) which are exposed to the elements (salt, sea, sun and wind) will require regular maintenance to avoid cracking and deterioration.





**Principle 1: Site Analysis** – Understand your sites solar orientation, prevailing breeze, views and aspects to form the basis of design.



**Principle 2: Site & Internal Planning** – Design with living spaces to the north, utility spaces to the south, generous outdoor living areas, good cross ventilation and good flow from inside to yard spaces.



**Principle 3: Mix of Materials** – Use a mix of materials appropriate to the sub-tropical coastal context.

Image: DegenhartSHEDD



**Principle 4: Design for outdoor living** – Integrate indoor and outdoor spaces. Image: Bourne Blue Architects



**Principle 5: Address the street** – Provide a legible front entry with habitable rooms or decks which overlook the street.

Image: Create Architects / Andy McPherson Photography.



**Principle 6: Integrate landscaping** – Design yard areas to provide shade, privacy and generous planting areas.

Image: Bourne Blue Architects

Figure 4.2 Residential design principles – Designing houses which respond to Kingscliff character and climatic condition starts with a site analysis to understand the solar path, prevailing breezes, view and aspect. All homes should have at least one living space which is orientated north. Materials should be selected for thermal performance as well as architectural character.

#### 4.1.2 Objectives:

1. Building design should respond to site, aspect and climatic considerations in terms of building planning, materiality choice and construction type within a subtropical context.
2. Pursue design and sustainability excellence through a holistic process and approach to site design including an understanding of solar path, prevailing breezes, as well as integrating landscape, view and privacy and overshadowing considerations as a preliminary stage in building design.
3. Be architecturally and sustainably responsive to the specific site, aspect and climatic conditions through floor plan configuration, window placement, materiality, structure and construction type.
4. Pursue residential development type and density that is appropriate to Kingscliff's urban structure and responds to site constraints.
5. Provide for a range and diversity of housing types to appeal to a broad demographic and housing affordability profile including shop top housing, residential flat buildings, medium density housing (row, terrace, integrated, multi-unit), suburban lots, small lots, secondary dwellings and 'tiny' and transportable homes.

#### 4.1.3 Controls:

- C1. All new residential buildings are to be designed in accordance with Tweed DCP A1 Residential and Tourist Development Code and where relevant SEPP 65 and the Apartment Design Guidelines.
- C2. All new residential flat buildings are to have a minimum 3.0m side setback. Where new development can not meet the building separation requirements of SEPP 65 Apartment Design Guide, all side facing windows and doors are to be designed to minimise overlooking and privacy issues by ways of size and setout height of windows, and the use of fixed privacy screens.
- C3. Where residential flat buildings are higher than 3 storey's, the upper level shall have as a minimum:
  - 5.0m side setback to the outer edge of building including decks, terraces and balustrades.
  - 9.0m front setback to the outer edge of building including decks, terraces and balustrades.
  - 9.0m rear setback to the outer edge of building including decks, terraces and balustrades.

#### 4.1.4 LEP Considerations:

All greenfield development sites will be the subject of future master planning/subdivision design led processes which will result in LEP land use and development standard amendments as an outcome of those processes.

#### 4.1.5 Additional Information:

Refer also to Tweed DCP A1, SEPP 65 and the apartment design guidelines and residential design principles in Figure 4.2.

## 4.2 Beach Front Precinct

### 4.2.1 Character Statement

The Beach Front Precinct has been an area in transition for the last 20 years with the gradual redevelopment of small coastal cottages into the predominantly three-storey residential flat building built form character which exists today. The newer and larger buildings capitalise on the ocean views and coastal reserve which provides level pedestrian and cycling access into the town centre to the south. This transition and increase in density has been facilitated by the precinct's existing R3 Medium density residential zoning.

The future character of the Beach Front Precinct will be defined by the continued development of well designed residential flat buildings and other medium density development which respond to the predominant allotment east-west aspect, ocean views and subtropical climatic context. There is also opportunity for mixed use development in the form of shop-top housing which may provide day-to-day convenience and limited food and beverage outlets servicing the local residential catchment and the north kingscliff holiday park.

A reduced building height and increased side setbacks will result in new development being consistent with the existing scale of development. The promotion of passive design principles will result in buildings which have a better thermal performance, units with a higher level of internal and external living amenity, innovative communal spaces and integrated landscaping. It is important that the overall building form design and use of building materials embody Kingscliff's coastal character.

### 4.2.2 Objectives:

1. To continue to facilitate the development of medium density and tourist accommodation which meets housing needs and is responsive to the subtropical climatic context.
2. Facilitate opportunity for shop top development fronting to provide day-to-day convenience and opportunity for and food and beverage (246-254 Marine Parade).
3. Provide additional street trees with a strong canopy the length of Kingscliff and Pearl Street visually asserting it as the main north south connector road.
4. Strengthen and enhance pedestrian connectivity throughout the precinct including new east-west connections from development sites to the west.

### 4.2.3 Controls:

- C1. All new residential flat buildings are to comply with the setback controls listed in section 4.1.3 of this plan and the SEPP 65 Apartment Design Guideline.
- C2. All new residential buildings are to be designed in accordance with Tweed DCP A1 Residential and Tourist Development Code.
- C3. Lot 6 DP 21242 is to be a future road reserve providing access from Pearl Street to the Turnock Street urban development site.

### 4.2.4 LEP Considerations:

Revise maximum building heights within this precinct from 13.6 metres to 12.2 metres for R3 Medium Density Residential areas to maintain consistency with the existing built form scale of three storey residential flat building developments.

### 4.2.5 Additional Information:

Refer also to Tweed DCP A1, SEPP 65 and the apartment design guidelines.

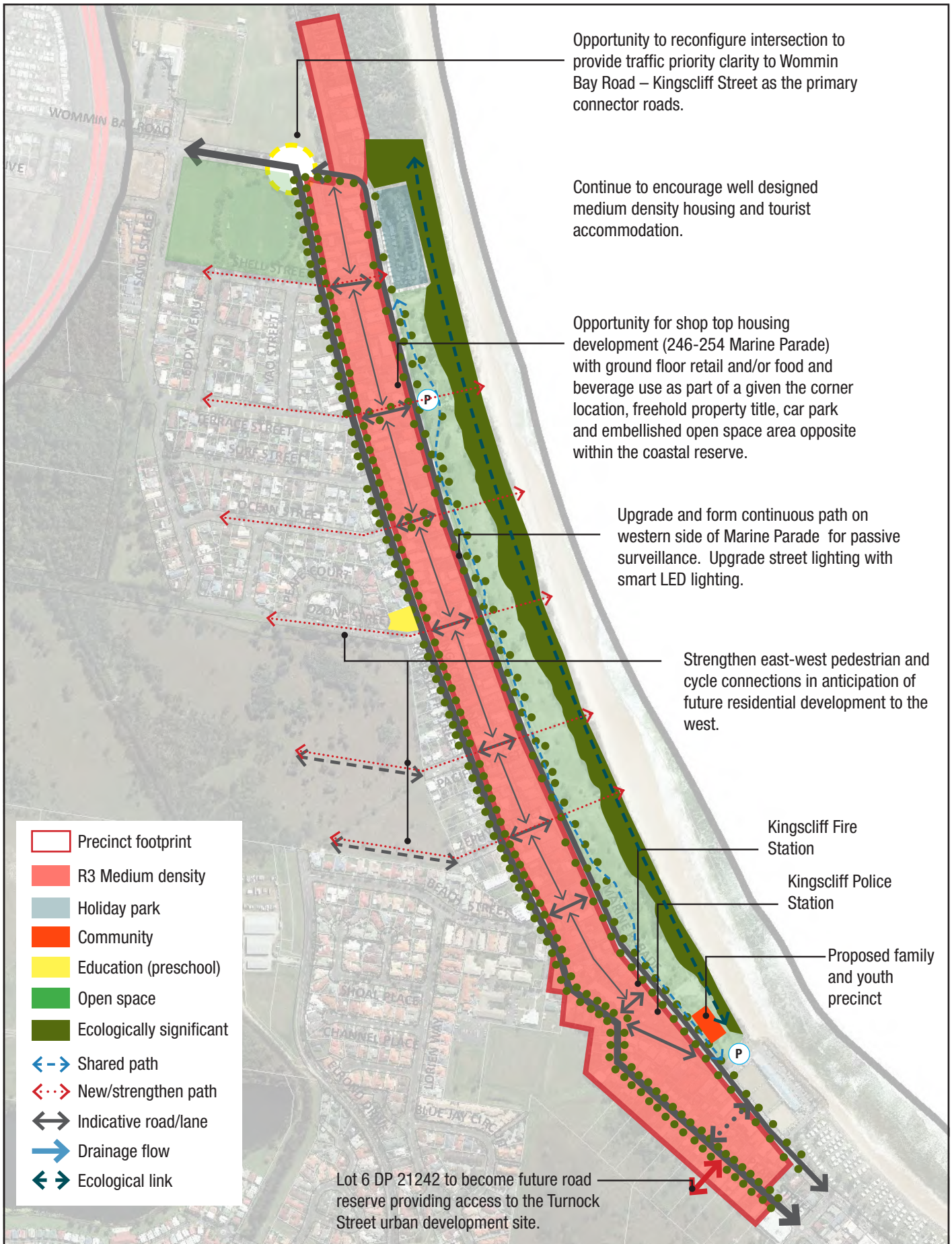


Figure 4.3 Beach Front Precinct

## 4.3 North Kingscliff Precinct

### 4.3.1 Character Statement

The North Kingscliff residential precinct consists of predominately low density, single and double storey dwellings on suburban allotments. Predominant dwelling materials include combination of both face brick and tile buildings as well as timber framed and lightweight clad construction. Due to the relatively wide road reserves, limited front fences and dispersed street trees, the streetscape character has an open and unshaded character. The strategy for this area is to retain its low density residential character, whilst allowing for additional infill development such as dual occupancies and secondary dwellings where appropriate.

The large undeveloped parcel of land located to the north of Beach Street and the south of Ozone Street provides a significant infill development opportunity. This key site already has the capacity within the existing planning framework, being zoned R1 General Residential, to be developed with a diversity and range of residential accommodation types. It is envisaged that appropriate development of this land will provide a transition in built form types from the existing surrounding low density residential to greater densities moving closer to the town centre to the south. The development of this site will provide additional north-south and east-west connections, linking the North Kingscliff and Elrond Drive Residential Precincts.

### 4.3.2 Objectives:

1. To continue to facilitate low density housing within the existing residential precinct.
2. Facilitate opportunities for a greater mix of low rise medium density housing types over the greenfield development site and western side of Kingscliff St.
3. Encourage additional street trees, providing a strong canopy for the length of Kingscliff and Pearl Street to visually assert it as the main north south connector road.
4. Strengthen and enhance pedestrian connectivity throughout the precinct including new east-west connections from development sites to the west.

### 4.3.3 Controls:

- C1. All new development within the precinct is to meet minimum design flood level requirements.
- C2. All new residential buildings are to be designed in accordance with Tweed DCP A1 Residential and Tourist Development Code and where relevant SEPP 65 and the Apartment Design Guidelines.
- C3. The greenfield development site is to be considered as part of a design led master planning process in consultation with Council.

### 4.3.4 LEP Considerations:

1. Facilitate the rezoning of the western side of Kingscliff Street from R2 Low Density Residential to R3 Medium Density Residential to allow a greater range of medium density housing types with a building height limit of 9m.
2. Facilitate the rezoning of the western side of Sand Street (Lot 37-39 & 41 DP 249808, SP22189) from R2 Low Density Residential to R3 Medium Density Residential to allow a greater range of medium density housing types with a building height limit of 9m.
3. Facilitate the rezoning of the allotments on the north side of Wommin Bay Road from R2 Low Density Residential to R3 Medium Density Residential to allow a greater range of medium density housing types with a building height limit of 9m.

### 4.2.5 Additional Information:

For greenfield development refer to Part 02 of this DCP and Tweed DCP A3 Development of Flood Prone Land and DCPA5 Subdivision Manual.



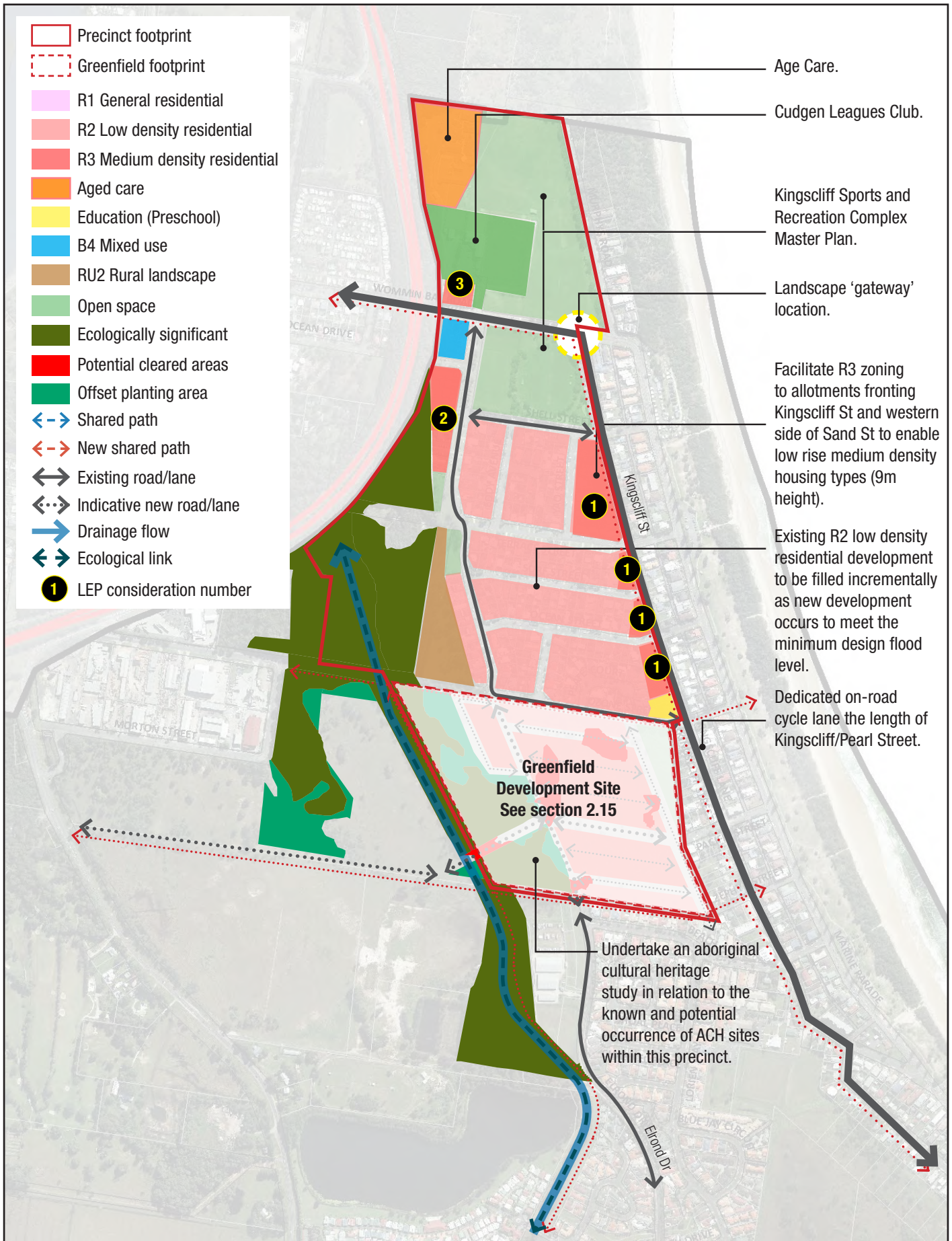


Figure 4.4 North Kingscliff Precinct

## 4.4 West Kingscliff Precinct

### 4.4.1 Character Statement

The existing West Kingscliff Precinct was developed in the 1990s and has a low density residential character consisting predominantly of single and two storey dwellings. Other housing types include dual occupancies, low rise multi-unit housing and seniors and aged care accommodation. Elrond Drive is the primary connector road running through the precinct providing access to the local streets, many of which are dead end cul-de-sacs. Whilst this road network limits road connectivity through the precinct, a series of pathways provide some pedestrian connectivity between streets and open space areas. The existing open space areas are relatively small however there is a significant opportunity to further embellish lands adjoining the drainage corridor with pedestrian and cycling pathways linking this precinct with the precinct further north.

The greenfield development site within the West Kingscliff Precinct (see section 2.14) will be accessed from the Turnock Street (west) extension which will form the primary vehicular access from the Tweed Coast Road into the Kingscliff township. The greenfield development site presents an opportunity to develop a range of medium density, low rise medium and low density residential housing types with the southern development boundary defined by the Turnock St extension alignment and areas of environmental protection south of the road alignment.

### 4.4.2 Objectives:

1. To continue to facilitate the development of low density housing within the existing residential precinct.
2. Facilitate opportunity for a greater mix of low rise medium density housing types over the greenfield development site across the less constrained parts of the precinct which will front the Turnock Street extension.
3. Design the Turnock Street extension as a tree lined boulevard with shared path delineating between environmental protection land (to the south) and settlement development land (to the north) to provide a improved vehicular, pedestrian and cycling connectivity and a high level of visual amenity and ecological protection;

### 4.4.3 Controls:

- C1. Refer to Tweed LEP 2014, Tweed DCP, SEPP 65 and the Apartment Design Guidelines.
- C2. The greenfield development site is to be considered as part of a design led site master planning process in consultation with Council. Refer to KLP Volume 01 and 02 as well as section 02 of this development code in addition to DCP A5 Subdivision Manual.

### 4.4.4 LEP Considerations:

1. In consultation with the land owner of Lot 130 DP 1039348 investigate rezoning from RU2 Rural Landscape to R3 Medium Density Residential to allow a greater range of medium density housing types with a building height limit of 9.0m in consideration of site constraints including but not limited to ecological buffers, flood and bushfire.
2. In consultation with the landowner of Lot 4 DP 727425 investigate rezoning of part of the allotment outside of identified high ecologically significant land from RU2 Rural Landscape to Medium Density Residential allow a greater range of medium density housing types with a building height limit of 9.0m as part of a design led master plan and planning proposal process which considers the broader West Kingscliff Precinct.

### 4.4.5 Additional Information:

For greenfield development refer to 02 of this development code and Tweed DCP A5 Subdivision Manual.

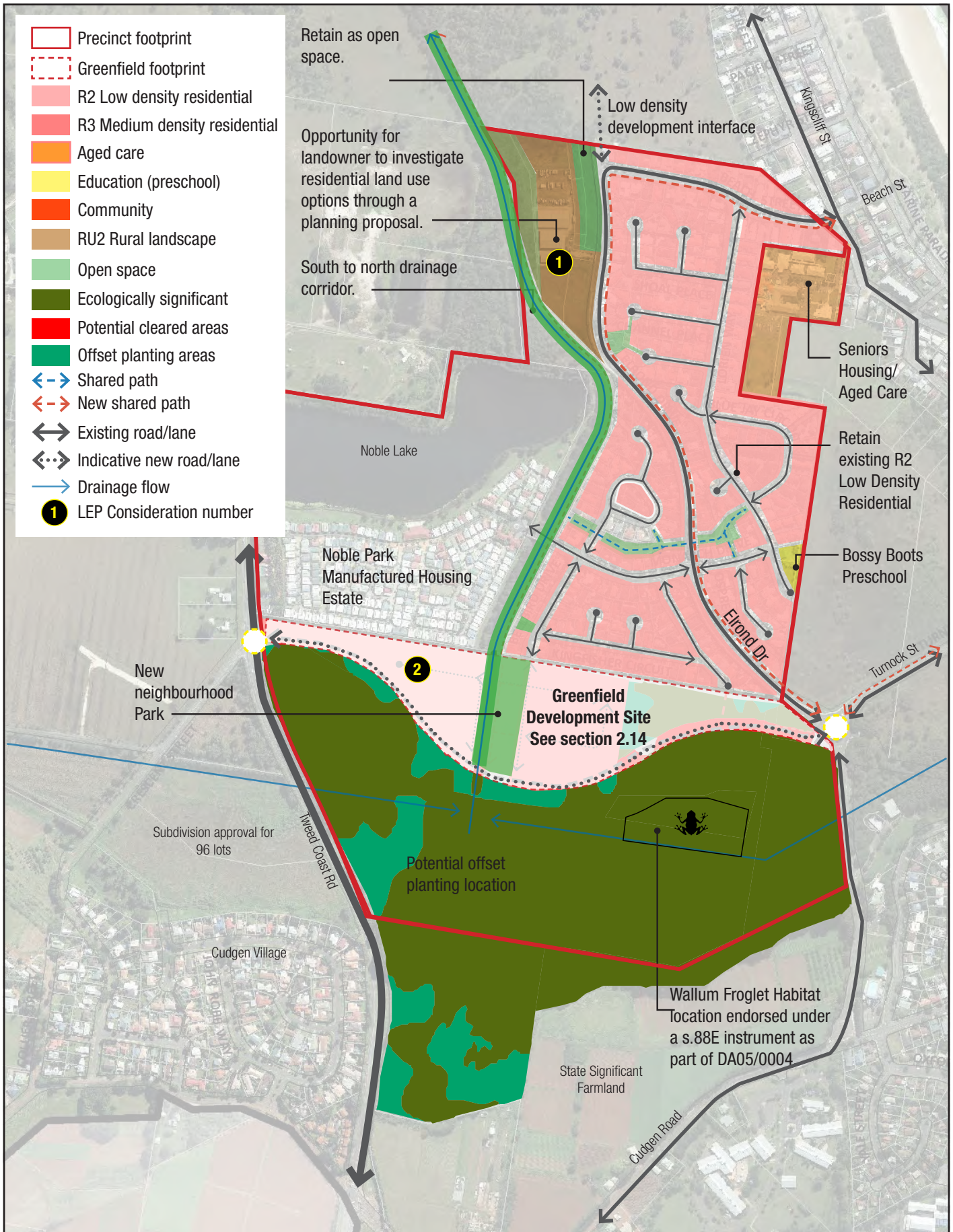


Figure 4.5 West Kingscliff Precinct

## 4.5 Kingscliff Hill Precinct

### 4.5.1 Character Statement

The Kingscliff Hill precinct was the first part of Kingscliff to be subdivided and developed by European settlers. Its character is defined by its elevated and sloping topography and expansive views over the creek, ocean, town centre and hinterland to the west. The precinct is also characterised by its diverse land use mix which includes areas of residential, education, health, community, recreation land uses as well as land of ecological significance particularly along the Cudgen Creek foreshore. In terms of residential land uses, the precinct contains a mix of R3 medium density low rise residential flat buildings along Moss Street and parts of Sutherland Street but more predominantly R2 Low Density Residential consisting of predominantly detached low density single and double storey residential dwelling. Given the trend of smaller housing being redeveloped as substantially larger houses, the consideration view sharing is a principle consideration for all new development applications. Other significant land uses within the precinct include the Kingscliff TAFE and Kingscliff High School, Kingscliff Public School, Kingscliff Pool, Library and community uses as well as recreation uses (Kingscliff Pool, Tennis club, parks). The Kingscliff Hill Precinct will also include the Tweed Valley Hospital which when developed together with the TAFE will establish a health and education precinct. As a regional referral hospital, there will be substantial ancillary land use opportunities for the locality including expanded public transport routes, expanded education programs as well as ancillary commercial, retail and accommodation land uses. There is also opportunity for the Cudgen farmlands to provide fresh food to the hospital precinct and locality in general. Additional traffic movements will require road and intersection upgrades.

### 4.5.2 Objectives:

1. To continue to facilitate the development of low density housing and low rise medium density housing within the existing residential precinct which meets housing needs and is responsive to the sloping site conditions, view sharing, aspect and subtropical climatic context.
2. The establishment of a health and education precinct with the development of the Tweed Valley Hospital opposite Kingscliff TAFE.

### 4.5.3 Controls:

- C1. Refer to Tweed LEP 2014, Tweed DCP, SEPP 65 and the Apartment Design Guidelines.
- C2. All development applications within the Kingscliff Hill Precinct which will effectively increase the building envelope (height, site coverage, floor space ratio) must submit a view analysis as part of the Statement of Environmental Effects supplemented by photographs and building envelope montages over affect views fields. The view analysis is to consider the four key planning principle (Tenacity Consulting v Warringah Council (2004) NSWLEC 140) assessment steps including:
  1. An assessment of the value of views to be affected by reference to their nature, extent and completeness.
  2. Consideration of how views are obtained and what part of the property the views are obtained.
  3. A qualitative assessment of the extent of the impact in terms of severity particularly as to whether that impact is negligible minor, moderate, severe or devastating.
  4. An assessment of the reasonableness of the proposal causing the impact particularly in terms of compliance with applicable planning controls and whether a different or complying design must produce a better result.

### 4.5.4 LEP Considerations:

1. Investigate future long term land use and master planning opportunities over lands fronting Turnock Street and Cudgen Road (Kingscliff Pool site including vacant lands to the immediate north, lands currently leased to Tweed Valley Respite Care, North Coast Area health and the Kingscliff Library) in consideration of Council's Community Infrastructure Framework Plan.
2. Investigate land use opportunities to establish a health and education precinct combining the Tweed Valley Hospital site, Kingscliff TAFE, Kingscliff High school, Lot 1 DP 803772 and Lot 1 DP 810063.

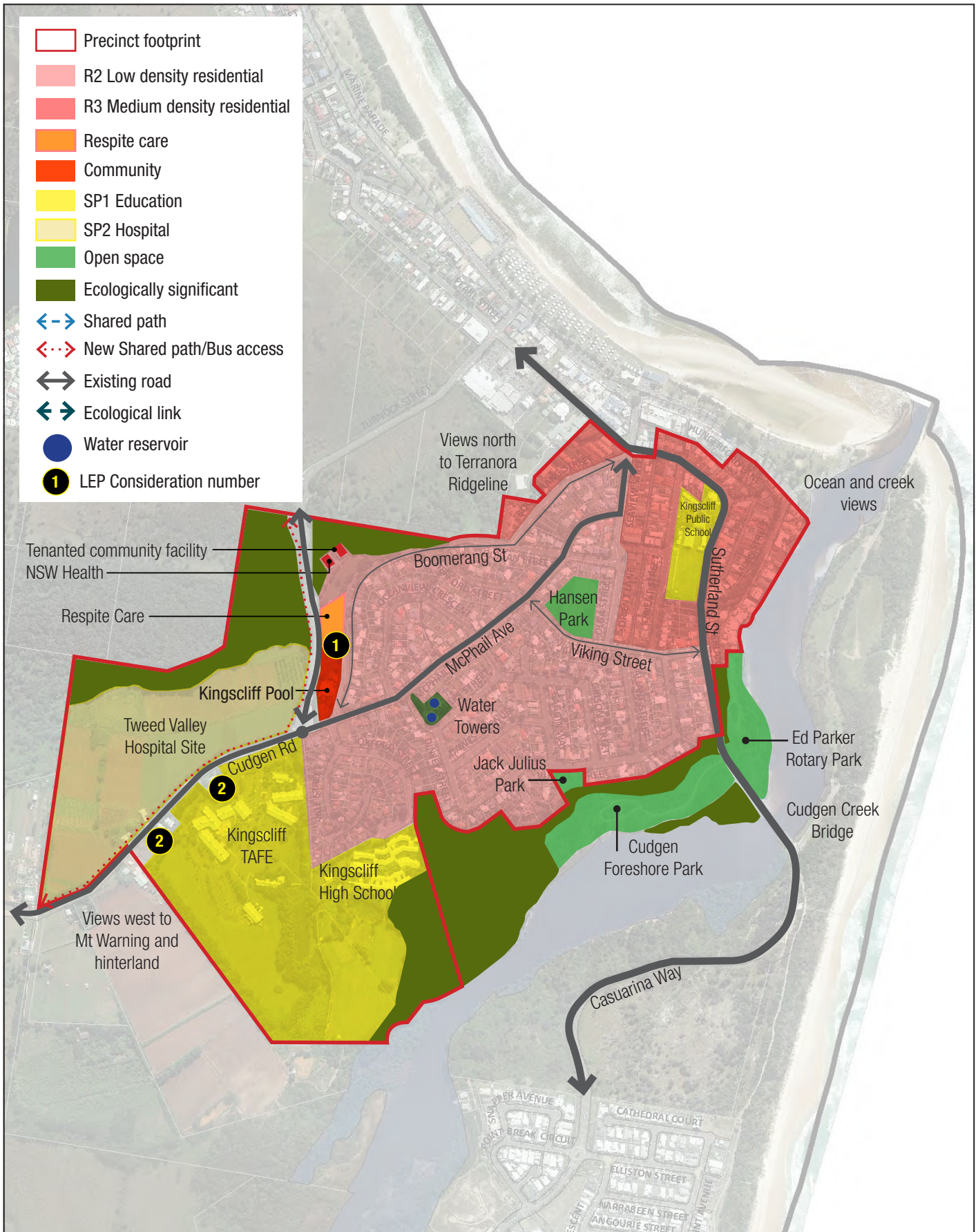


Figure 4.6 Kingscliff Hill Precinct

## 4.6 Salt Precinct

### 4.6.1 Character Statement

The Salt precinct is located to the south of the Cudgen Creek Bridge and represents one of the more recent master planned developments within the Kingscliff locality with construction commencing in the early 2000's. The precinct now combines retail, open space and tourism land uses at its core and is surrounded by low density residential housing. The formal tourism component currently accommodates two hotels (Peppers and Mantra) with retail units fronting Bells Boulevard, which in turn defines a large central town green/open space area. This open space area links through to the coastal reserve providing access to the ocean beach. The tourism-dedicated built form maximises its location advantages and is predominately 3 storeys throughout.

Outside the tourist footprint, dwellings are predominately large detached dwellings. The built form character was originally defined by a developer-led materials and finishes covenant and resulted in the consistent use of white and grey hues on rendered finishes, complemented by lightweight material throughout the buildings detailing. These materials and large form exude a distinct character that is unique to the Salt precinct. Future opportunities include further development of tourist and business uses over a remaining vacant allotment between the existing local centre and Casuarina Way.

### 4.6.2 Objectives:

1. To continue to facilitate tourist accommodation and tourist related development as well as the growth of the local centre and associated uses to service the needs of both tourists and local residents.
2. To retain the predominantly low density residential character surrounding the local centre in keeping with the estate's existing built form character and design covenants.
3. Implement more densely planted vegetation corridors along Casuarina Way, east-west pedestrian corridors and public reserves and Lot 465 DP1092331 to improve ecological/canopy connections through the precinct linking the coastal reserve with Cudgen Creek as well as improving the visual amenity with a defined vegetated edge to the precinct.

### 4.6.3 Controls:

- C1. Refer to Tweed LEP 2014, Tweed DCP, SEPP 65 and the Apartment Design Guidelines where relevant.

### 4.6.4 LEP Considerations:

1. In consultation with property owners facilitate the rezoning of Lot 169 DP 1075495 and Lot 930 DP 10791198 from SP3 and R1 to B4 Mixed Use to allow a range of residential, tourist, retail, food and beverage and other development uses with a building height limit of 13.6m.
2. Rezone areas of R1 General Residential to R2 Low Density Residential to reflect the existing character of low density development.
3. In consultation with the Tweed Byron Aboriginal Land Council regarding the future planning and management of Lot 1 & 2 DP 1117599 including the rezoning of these lands from SP3 to an appropriate E-Zone.

### 4.6.5 Additional Information:

Contribution Plan – No. 25 – SALT Open Space and Associated Car Parking.

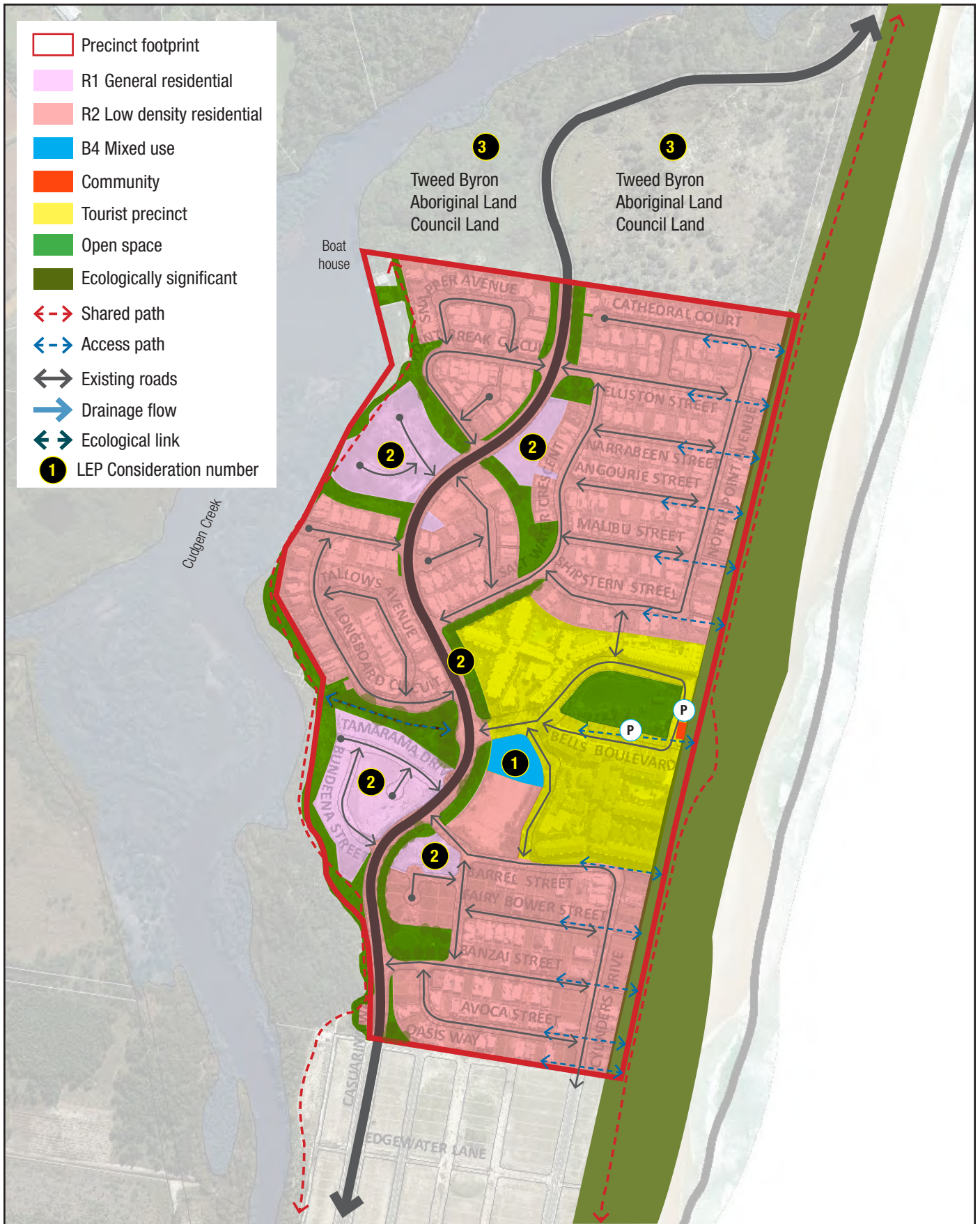


Figure 4.7 Salt Precinct

## 4.7 Seaside Precinct

### 4.7.1 Character Statement

Whilst the original Seaside paper subdivision design dates back to the 1920s the site remained undeveloped until 2012. Seaside represents the last of the Tweed Coast master planned precincts to be developed following Salt to the north and Casuarina to the south. Offering a similar character, housing type and lifestyle to both Salt and Casuarina estates, the grided road layout is its point of difference and defines Seaside's urban structure.

Although the Seaside is still being gradually developed, the precinct will achieve a mix of housing types facilitated through varying allotment sizes and the combination of R2 low density R3 medium density zoning. An area of B4 Mixed use zone where Ocean Avenue intersects with Cylinders Drive will enabling the development of multi-storey mixed use development with ground floor retail units on either side of the street and adjoining the coastal reserve.

There is an opportunity, particularly within the yet to be constructed medium density releases of the estate, to continue to achieve the established vision for the Seaside Precinct, being:

- Continue to encourage a mix of medium density housing opportunities across the developable greenfield portion of the precinct to take advantage of the flat site topography and good walking proximity to the existing town centre.
- Facilitate shop-top housing within the mixed use footprint and a variety of product within the medium density area will ensure housing options to meet the housing needs of a varied demographic profile.
- Facilitate the development of a small town square and surrounding public domain space along Ocean Avenue could include markets and other forms of temporal or seasonal trading, further complementing permanent stores and the Kingscliff events calendar.
- Similar to the Salt precinct and the Kingscliff locality as a whole, tourism is recognised as an economic driver within Seaside City.

### 4.7.2 Objectives:

1. To continue to facilitate the development of low density housing and low rise medium density housing within the existing residential precinct which meets housing needs and is responsive to site conditions, aspect and sub tropical climatic context.
2. To continue to facilitate the growth of the local centre including shop top housing as well as tourist accommodation and associated uses to service the needs of tourists as well as local residents.
3. To continue to embellish existing open spaces and improve connectivity throughout precinct.

### 4.7.3 Controls:

- C1. Refer to Tweed LEP 2014, Tweed DCP, SEPP 65 and the Apartment Design Guidelines where relevant. Specific controls relating to the Seaside precinct are contained within Tweed DCP A1 Part D.

### 4.7.4 LEP Considerations:

Nil

### 4.7.5 Additional Information:

Refer to Tweed LEP 2014, Tweed DCP, SEPP 65 and the Apartment Design Guidelines.



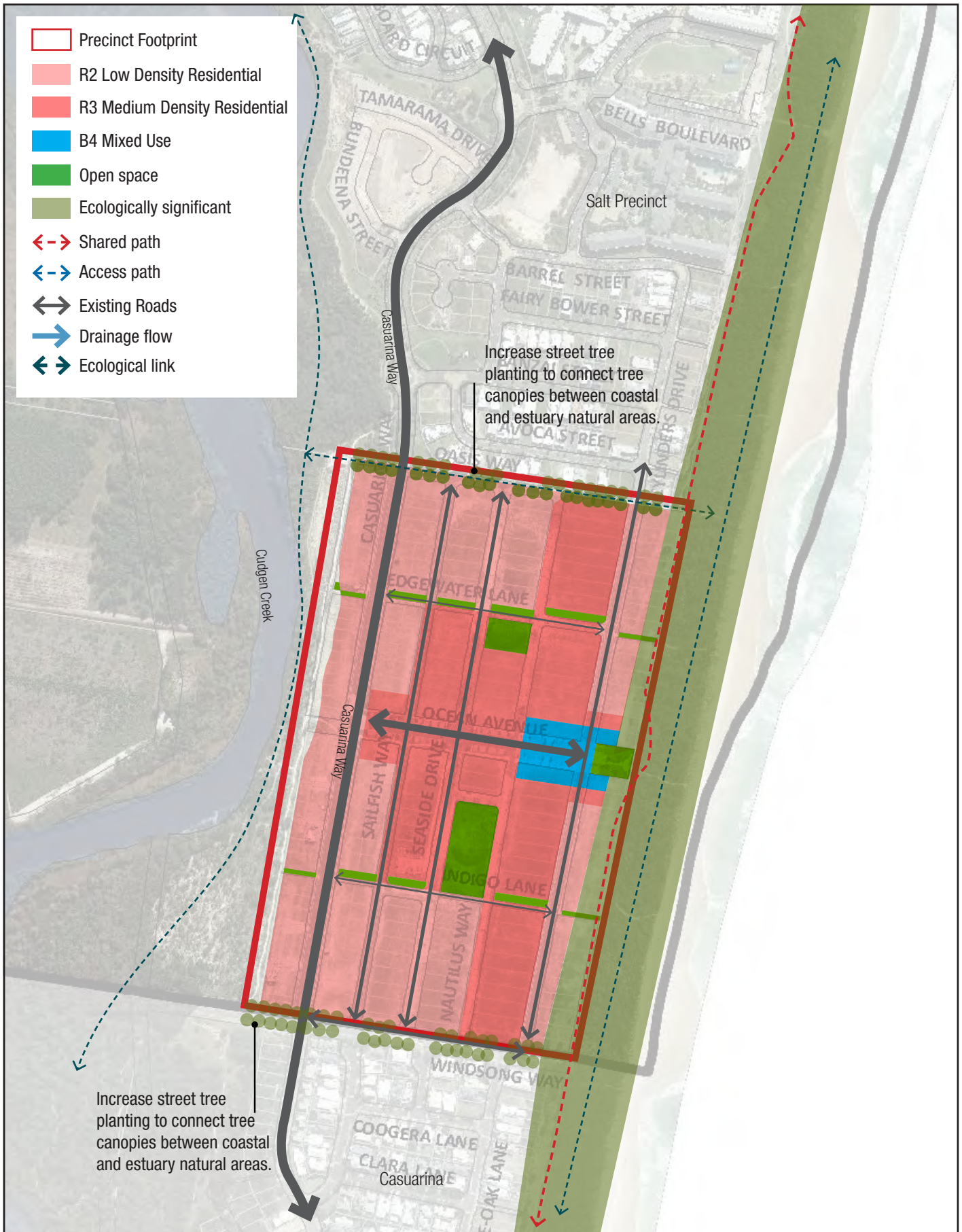


Figure 4.8 Seaside Precinct

## 4.8 Cudgen Village

### 4.8.1 Character Statement

In the late nineteenth century Cudgen was a busy agricultural and trading hub and at one point had a post office, general store and hotel. The dry stack stone walls built by the South Sea Islander indentured labourers found throughout the local area and sugar mill site to the east of the Tweed Coast Road are the only remaining remnants of this period.

Today the built character of Cudgen is defined by its contained low scale residential area surrounded by red soil agricultural areas of the Cudgen plateau. Housing types includes a combination of single and double storey brick and tile dwellings as well as older housing stock constructed of lightweight materials such as weatherboard with corrugated steel roofs. There are also a number of dual occupancy developments throughout the settlement and a two storey townhouse development adjoining Cudgen Road and Tweed Coast Rd intersection containing 25 dwellings.

The streetscape character within Cudgen is defined by its open landscape streets with grassed and treed verge with many established front gardens and low set fences, if any fence at all. The large Norfolk Pine street trees along Collier Street add significantly to the landscape visual character for the village. Being elevated there are a number of long views to the north towards Terranora Ridge and agricultural farmland views to the west and south. To a large extent, these longer view fields and sense of openness define the visual setting and character of the village.

The future character of Cudgen Village will be to maintain the low density residential character within both the existing settlement areas and greenfield development area. The combination of the greenfield development area to the immediate north, and the 40 lot subdivision currently being developed off Collier Street will substantially increase Cudgen's settlement envelope. In addition there is an existing approval for a sand extraction operation west of Crescent Street which at its cessation will result in a large artificial lake. This in turn may open up future opportunity for water based recreation and the potential for a holiday park to be developed adjoining.

This additional settlement size may generate the need for a small scale retail /convenience shop. Given the proximity to the existing agricultural land uses there is also an opportunity for fresh food production to serve the regional hospital, paddock to plate and fresh produce retail opportunities which could also fulfil day-to-day convenience needs service the local population, day-tripper and agri-tourists alike.

### 4.8.2 Objectives:

1. To continue to facilitate the development of low density housing within the existing residential areas and newer greenfield development areas.
2. Facilitate opportunity for a business uses to meet the day-to-day needs for local residents and opportunities to support ancillary rural land uses including fresh food retail outlets, paddock to plate and other agri-tourism initiatives.
3. To retain the remaining historic fabric of Cudgen including the South Sea Islander constructed dry stack stone walls, the Norfolk Island Pines in Collier Street and undertake a heritage review of the Cudgen Sugar Mill site.

### 4.8.3 Controls:

- C1. Refer to Tweed LEP 2014, Tweed DCP where relevant.

### 4.8.4 LEP Considerations:

1. Refer to section 2.17 Key Greenfield Site – Cudgen Village Precinct.
2. Review the existing R2 Low Density Residential zoning over Lot 101 DP 1056576 in consultation with the landowner to investigate other non-residential land uses due to potential buffer conflict with adjoining state significant farmland.

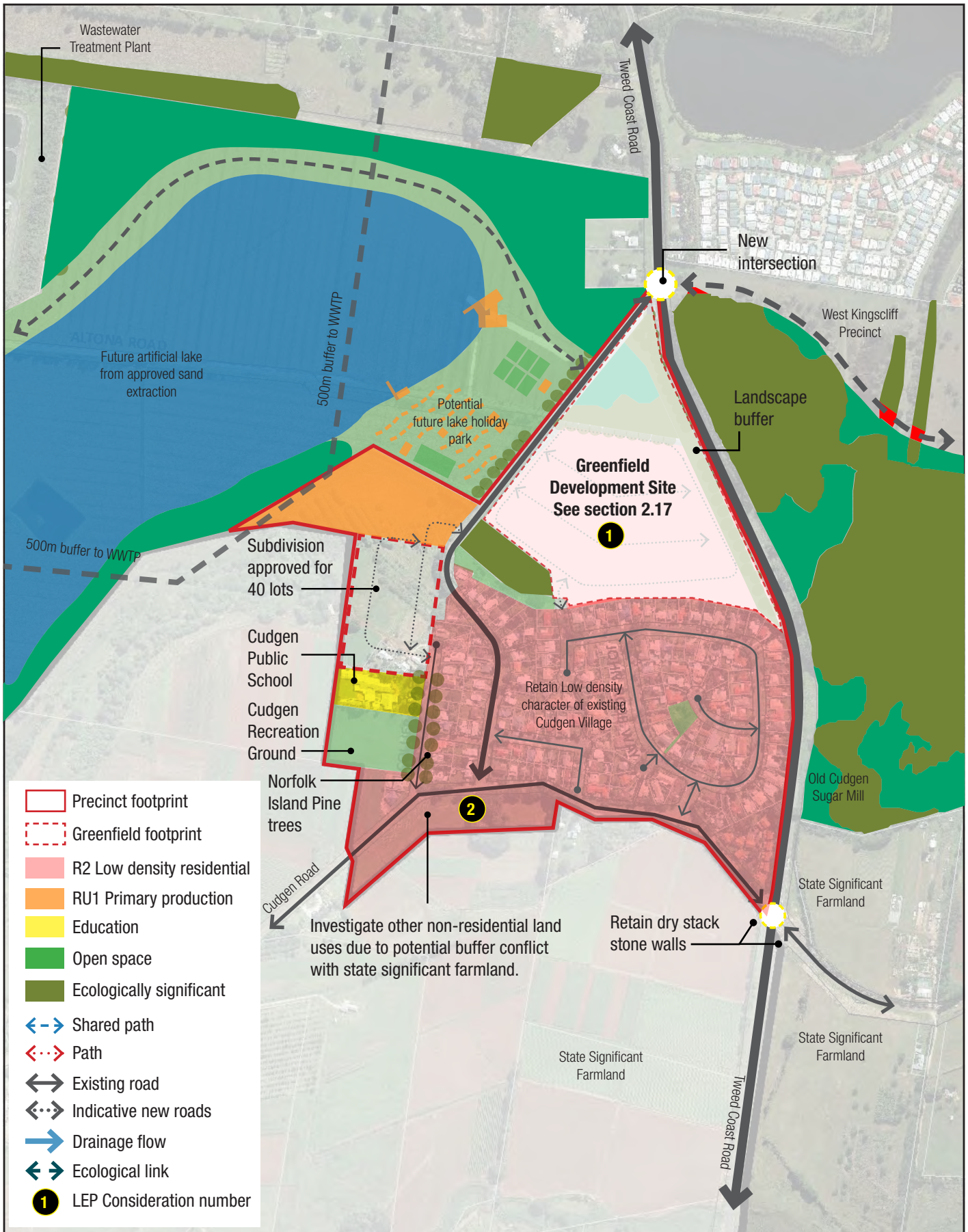


Figure 4.9 Cudgen Village Indicative Structure Plan

