

GENERAL

- THIS NOTE AND THE FOLLOWING NOTES FORM AN INTEGRAL PART OF THIS DRAWING SET.
- THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH THE RELEVANT TWEED SHIRE COUNCIL (TSC) DEVELOPMENT DESIGN SPECIFICATIONS AND STANDARD DRAWINGS.
- ALL DIMENSIONS ARE IN METRES UNLESS SHOWN OTHERWISE.
- DIMENSIONS SHALL NOT BE SCALED FROM THE DRAWINGS.
- MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, TOGETHER WITH THE REQUIREMENTS OF ALL APPLICABLE CODES OF PRACTICE, AUSTRALIAN STANDARDS AND STATUTORY AUTHORITIES.
- SURVEY DATA HAS BEEN COMPILED FROM FIELD PICK-UPS AND OFFICE RECORDS. THE PROJECT MANAGER SHOULD CONFIRM THAT SUFFICIENT DATA IS SHOWN TO ENABLE CONSTRUCTION WITHOUT DISTURBANCE TO FEATURES THAT ARE NOT SHOWN ON THE DRAWINGS.
- PRIOR TO ANY DEMOLITION, EXCAVATION OR CONSTRUCTION ON SITE, THE RELEVANT AUTHORITIES SHOULD BE CONTACTED FOR POSSIBLE LOCATION OF FURTHER UNDERGROUND SERVICES AND DETAILED LOCATION OF ALL SERVICES (DIAL BEFORE YOU DIG 1100).
- THE TITLE BOUNDARIES SHOWN HEREON WERE NOT FIELD INVESTIGATED OR MARKED AT THE TIME OF SURVEY AND HAVE BEEN DETERMINED BY PLAN DIMENSIONS ONLY.
- THE ORIGIN OF CO-ORDINATES IS MGA.
- THE DATUM FOR LEVELS IS AHD.

SITE WORKS

- ALL SOILS CONTAINING ORGANIC MATTER (E.G. ROOTS, GRASS ETC.) MUST BE STRIPPED FROM THE CONSTRUCTION SITE PRIOR TO EARTHWORKS AND MUST NOT BE USED AS FILL MATERIAL.
- CUT SLOPES MUST BE LIMITED TO 1½:1 (HORIZONTAL:VERTICAL). THE SLOPE SHOULD THEN BE GRASSED OR PAVED TO PREVENT SCOUR AND EROSION DAMAGE.

RESTORATION OF SURFACES

- THE CONSTRUCTOR SHALL CLEAN PAVEMENTS, LAWNS AND OTHER IMPROVED AREAS AND LEAVE THEM IN THE SAME ORDER AS THEY WERE AT THE COMMENCEMENT OF THE WORKS. THE CONSTRUCTOR SHALL RESTORE ANY FENCING REMOVED DURING CONSTRUCTION AND SHALL RESTORE LAWNS WITH TURF CUT AND SET ASIDE FROM THE ORIGINAL SURFACE AND WITH IMPORTED TURF FROM A SOURCE APPROVED BY THE CONSTRUCTION ENGINEER. (WSA 02 2002 PART 3, SECTION 25).
- IMMEDIATELY AFTER BACKFILLING OF A TRENCH EXCAVATED THROUGH A PAVEMENT HAS BEEN COMPLETED, THE CONSTRUCTOR SHALL TEMPORARILY RESTORE THE PAVEMENT. WHERE THE TRENCH CROSSES BITUMEN OR CONCRETE PAVEMENT, THE SURFACE IS TO BE PROTECTED FROM DETERIORATION. A PRE-MIXED ASPHALTIC MATERIAL MAY BE USED FOR SUCH TEMPORARY RESTORATION. THE CONSTRUCTOR SHALL MAINTAIN THE TEMPORARY RESTORATION UNTIL FINAL RESTORATION IS CARRIED OUT. FINAL RESTORATION OF THE PAVEMENT SHALL BE CARRIED OUT TO RESTORE THE PAVEMENT AND ITS SUB-BASE TO NO LESS THAN THE ORIGINAL CONDITION. FINAL RESTORATION MAY INCLUDE, IF REQUIRED BY THE CONSTRUCTION ENGINEER, THE REMOVAL OF TEMPORARY RESTORATION.
- IN OTHER THAN ROADWAYS, THE CONSTRUCTOR SHALL PLACE THE BACKFILL SUFFICIENTLY HIGH TO COMPENSATE FOR EXPECTED SETTLEMENT AND FURTHER BACKFILLING SHALL BE CARRIED OUT OR THE ORIGINAL BACKFILL TRIMMED AT THE END OF THE DEFECTS LIABILITY PERIOD IN ORDER THAT THE SURFACE OF THE COMPLETED TRENCH MAY THEN CONFORM WITH THE ADJACENT SURFACE. SURPLUS MATERIAL SHALL BE REMOVED AND DISPOSED OF TO AREAS ARRANGED BY THE CONSTRUCTOR. WHERE DRY WEATHER CONDITIONS HAVE PERSISTED AFTER THE ORIGINAL BACKFILLING, INCLUDING DURING THE DEFECTS LIABILITY PERIOD, THE CONSTRUCTOR SHALL TAKE ALL NECESSARY STEPS TO CONSOLIDATE THE TRENCH BEFORE REMOVING SURPLUS MATERIALS FROM THE SITE.
- IN LOCATIONS WHERE, IN THE OPINION OF THE CONSTRUCTION ENGINEER, SURPLUS MATERIAL LEFT IN THE VICINITY OF THE TRENCH WOULD NOT BE OBJECTIONABLE, THE SURPLUS MATERIAL MAY BE DISPOSED BY SPREADING NEATLY IN THE VICINITY OF THE TRENCH TO THE SATISFACTION OF THE CONSTRUCTION ENGINEER IN SUCH A WAY AS TO AVOID FUTURE EROSION OF THE BACKFILL AND ADJACENT GROUND SURFACES. THE CONSTRUCTOR SHALL MAINTAIN THE BACKFILL AND ADJACENT GROUND UNTIL THE EXPIRY OF THE DEFECTS LIABILITY PERIOD.
- WHERE, WITHIN PUBLIC OR PRIVATE PROPERTY, THE REASONABLE CONVENIENCE OF PERSONS WILL REQUIRE SUCH, THE CONSTRUCTION ENGINEER MAY ORDER THE CONSTRUCTOR TO LEVEL TRENCHES AT THE TIME OF BACKFILLING. THE CONSTRUCTOR SHALL MAKE GOOD ANY SUBSEQUENT SETTLEMENT, AS REQUIRED BY PLACING ADDITIONAL FILL.
- THE CONSTRUCTOR SHALL IMMEDIATELY RESTORE ANY DAMAGED OR DISTURBED PRIVATE PROPERTY AND SERVICES.
- SHOULD THE CONSTRUCTOR ELECT TO TUNNEL UNDER PAVING, KERB AND GUTTER OR OTHER IMPROVED SURFACES IN LIEU OF TRENCHING, BACKFILLING SHALL BE SO CARRIED OUT AS TO RESTORE FULL SUPPORT TO THOSE SURFACES. THE CONSTRUCTOR SHALL REMAIN RESPONSIBLE FOR THE REPAIR OF THE IMPROVED SURFACES, IF SUBSEQUENTLY DAMAGED DUE TO SUBSIDIENCE OF THE BACKFILL, UNTIL THE END OF THE DEFECTS LIABILITY PERIOD.
- THE CONSTRUCTOR SHALL PROVIDE NOTICE TO AFFECTED PROPERTY OWNERS OF ANY PENDING WORKS.

STORMWATER DRAINAGE NOTES

- DESIGN OF STORMWATER DRAINAGE HAS BEEN CARRIED OUT AS PER TWEED SHIRE COUNCIL DEVELOPMENT DESIGN SPECIFICATION D5 AND SUBDIVISION MANUAL SECTION A5.
- STORMWATER DRAINAGE CONSTRUCTION SHALL BE CARRIED OUT IN ACCORDANCE THE TWEED SHIRE COUNCIL DEVELOPMENT CONSTRUCTION SPECIFICATIONS, IN PARTICULAR, C220 TO C233.
- STORMWATER DRAINAGE PIPES SHALL BE STEEL REINFORCED CONCRETE PIPES (CLASS SPECIFIED ON DRAWINGS) UNLESS OTHERWISE APPROVED.

EXISTING SERVICES

- EXISTING SERVICES SHOWN ON PLANS ARE AN INDICATION OF SERVICES FOUND AT THE TIME OF DESIGN INVESTIGATION.
- QUALITY OF INFORMATION ON EXISTING UNDERGROUND SERVICES SHOWN ON PLANS ARE CLASSIFIED USING ASS488-2013 WITH QUALITY LEVELS A, B, C AND D;
 - A** POSITIVE IDENTIFICATION OF A POINT POSITION IN THREE DIMENSIONS WITH HORIZONTAL AND VERTICAL TOLERANCE OF 50mm
 - B** LOCATION RELATIVE TO SUBSURFACE FEATURE LOCATION IN THREE DIMENSIONS. EG INTERPRETATION BETWEEN TWO QL-A POINTS
 - C** INTERPRETATION OF APPROXIMATE HORIZONTAL LOCATION USING EXISTING RECORDS AND SITE SURVEY OF VISIBLE SURFACE FEATURES. PROVIDES RELATIVE SPATIAL POSITIONING.
 - D** COMPILED FROM ANY OR A COMBINATION OF:
 - a. EXISTING RECORDS
 - b. CURSORY SITE INSPECTION
 - c. ANECDOTAL EVIDENCE
- BY DEFAULT, IF THE DRAWINGS DO NOT INDICATE A QUALITY LEVEL THEN THE INFORMATION SHALL BE ASSUMED TO BE AT QUALITY LEVEL D.
- THE CONSTRUCTOR SHALL BE RESPONSIBLE FOR THE LOCATION OF EXISTING SERVICES PRIOR TO COMMENCING WITH THE WORKS.
- THE CONSTRUCTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY EXISTING SERVICES DAMAGED DURING CONSTRUCTION WITH NEW SERVICES OF EQUIVALENT TYPE AND SPECIFICATIONS.
- WHEN CONSTRUCTING OR WORKING NEAR EXISTING PRESSURE MAINS IT SHOULD BE EXPECTED THAT THERE ARE CONCRETE THRUST BLOCKS LOCATED AT BENDS OR OTHER FITTINGS ON THE EXISTING MAIN. IT IS VERY IMPORTANT NOT TO DISTURB THE BEARING SOIL BEHIND THE THRUST BLOCK TO AVOID FAILURE OF THE EXISTING PRESSURE MAIN. IF EXCAVATION AROUND EXISTING THRUST BLOCKS CAN NOT BE AVOIDED THEN THE EXISTING PRESSURE MAIN SHALL BE TAKEN OFF LINE DURING THE EXCAVATION WORKS.

CONCRETE NOTES

- ALL WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH A.S.3600, CURRENT EDITION WITH AMENDMENTS
- CONCRETE QUALITY (UNLESS OTHERWISE SHOWN) SHALL BE AS FOLLOWS
 - COURSE AGGREGATE - MAXIMUM SIZE 20mm
 - CEMENT - TYPE "A" PORTLAND CEMENT.
 - CONCRETE SHALL HAVE THE FOLLOWING SLUMP DURING PLACEMENT
 - BEAMS, SLABS AND FOOTINGS 80mm
 - COLUMNS AND WALLS 80mm
- SLAB JOINTS SHALL BE PLACED AS FOLLOWS
 - FOOTPATHS - AS PER TWEED SHIRE COUNCIL STANDARD DRAWING NUMBER S.D.014
 - SLABS AND WALLS - REFER TO SLAB JOINTING PLAN WITHIN THIS DRAWING SET
 - SLAB SAWN JOINTS SHALL BE CUT WITHIN 24 HOURS OF SLAB POURING IN A NEAT AND STRAIGHT CUT.
- ALL SPLATTER TO SURROUNDING SURFACES SHALL BE CLEANED UP IMMEDIATELY
- COVER TO REINFORCEMENT SHALL BE OBTAINED BY THE USE OF PLASTIC BAR CHAIRS WITH MAXIMUM SPACING OF 800mm IN ANY DIRECTION
- ALL CONCRETE SHALL BE COMPACTED USING HIGH FREQUENCY VIBRATORS.
- CURING OF CONCRETE SURFACES SHALL COMMENCE IMMEDIATELY AFTER SURFACES ARE FINISHED AND SHALL CONTINUE TO CURE FOR A MINIMUM OF 7 DAYS
- SLABS WITH SPECIFIC ROUGH FINISHES SHALL BE KEPT FREE OF BLEED WATER AND FLOATED TO PREVENT THE FORMATION OF PLASTIC SHRINKAGE CRACKS.
- SLAB THICKNESSES AS SHOWN ON THE DRAWINGS DO NOT INCLUDE ANY FINISHES INCLUDING STAMPING DEPTHS.

'WORK AS EXECUTED' REPORTING REQUIREMENTS

WORK AS EXECUTED INFORMATION MUST BE PROVIDED TO THE DESIGN UNIT BY THE CONSTRUCTION ENGINEER OR SITE FOREMAN AS A PART OF THIS WORK.

- WHERE WORK IS CONSTRUCTED BY TSC STAFF REFER TO THE WATER UNIT'S 'WORK AS EXECUTED' PROCEDURE FOR WATER UNIT CAPITAL WORKS'.
- WHERE WORK IS CONSTRUCTED BY CONTRACTORS REFER TO THE CONTRACT SPECIFICATIONS FOR 'WORK AS EXECUTED' REQUIREMENTS.

ROAD PAVEMENT CONSTRUCTION INSPECTION AND TEST PLAN

- THE ROAD PAVEMENT BOX SHALL BE EXCAVATED OR FILLED TO THE DEPTH AS SPECIFIED IN THE PAVEMENT DESIGN (REFER TO ROAD CROSS SECTION DRAWINGS) AND AS PER TWEED SHIRE COUNCIL DEVELOPMENT CONSTRUCTION SPECIFICATION C213
- SUB SURFACE DRAINAGE SHALL BE CONSTRUCTED UNDER ALL KERBING AS SHOWN ON THE DRAWINGS AND SHALL BE IN ACCORDANCE THE TWEED SHIRE COUNCIL DEVELOPMENT CONSTRUCTION SPACIFICATION C230
- WHERE JOINING INTO EXISTING ROADWAY CONSTRUCTION, REFER TO TIE IN DETAILS ON THE DRAWINGS FOR TREATMENT OF THE JOINING INTERFACE
- THE SUBGRADE SHALL BE PROOF ROLLED IN AN APPROVED MANOR AND ANY SOFT SPOTS IDENTIFIED SHALL BE EXCAVATED OUT AND REPLACED AS PER TWEED SHIRE COUNCIL DEVELOPMENT CONSTRUCTION SPECIFICATION C213
- SUB BASE GRAVEL SHALL BE PLACED, SPREAD, WATERED AND COMPACTED TO TWEED SHIRE COUNCIL DEVELOPMENT CONSTRUCTION SPECIFICATION C242
- KERBING (IF REQUIRED) SHALL BE CAST IN PLACE AFTER TESTING OF SUB BASE GRAVEL
- BASE COURSE GRAVEL SHALL BE PLACED, SPREAD, WATERED AND COMPACTED TO TWEED SHIRE COUNCIL DEVELOPMENT CONSTRUCTION SPECIFICATION C242
- SEALING OF ROADWAY SHALL BE CARRIED OUT AFTER TESTING OF BASE COURSE GRAVEL

SET OUT INFORMATION

- SET OUT INFORMATION FOR CONSTRUCTION IS PROVIDED TO THE CONSTRUCTION SURVEYOR IN DIGITAL FORM.
- SET OUT FOR ALL PIPEWORK IS CENTRE OF PIT (U.N.O.)
- SET OUT FOR ALL KERB AND GUTTER (INCLUDING DISH DRAIN) IS LIP OF KERB (REFER FIG 1 BELOW)
- SET OUT FOR ALL KERB ONLY (INCLUDING FLUSH EDGE STRIP) IS BITUMEN LEVEL AT FRONT FACE OF KERB (REFER FIG 1 BELOW)
- SET OUT FOR BITUMEN IS FINISHED SURFACE
- FINISHED SURFACE LEVEL SET OUT FOR STORMWATER DRAINAGE GULLY PITS IS AT THE LIP OF KERBING (REFER FIG 2 BELOW)
- FINISHED SURFACE LEVEL SET OUT FOR MANHOLES (SEWER AND STORMWATER), JUNCTION PITS, FIELD INLETS ETC. IS CENTRE OF LID / GRATE. (REFER FIG 2 BELOW)

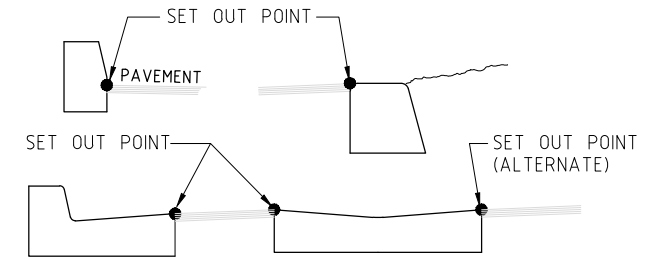


FIGURE 1

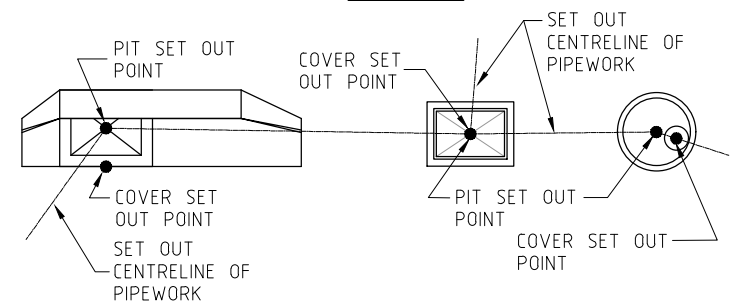


FIGURE 2

CONSTRUCTION FOR CONSTRUCTION				DESIGN UNIT				PROJECT: KENNEDY DRIVE BOAT RAMP CAR PARK UPGRADE				PROJECT NUMBER: MNP23																					
				COUNCIL OFFICES TUMBULGUM ROAD, MURWILLUMBAH NSW 2484.				DESIGN ENGINEER <i>W.A. Boyd</i> DATE 11.10.17.				PLAN TITLE: NOTES		DRAWING NUMBER																			
				PHONE 02 66702400 FAX 02 66727513 WEBSITE www.tweed.nsw.gov.au				<table border="1"> <tr> <th>DESIGNED</th> <th>N.O.</th> <th>19.07.17</th> <th>COORDS ADOPTED</th> <th>PM41250</th> </tr> <tr> <td>DRAWN</td> <td>N.O.</td> <td>19.07.17</td> <td>EASTING</td> <td>552145.608</td> </tr> <tr> <td>CHECKED</td> <td>J.M.M.</td> <td>19.07.17</td> <td>NORTHING</td> <td>6882057.637</td> </tr> <tr> <td>HORIZONTAL DATUM</td> <td>MGA</td> <td></td> <td>R.L.</td> <td>1.864</td> </tr> </table>				DESIGNED	N.O.	19.07.17	COORDS ADOPTED	PM41250	DRAWN	N.O.	19.07.17	EASTING	552145.608	CHECKED	J.M.M.	19.07.17	NORTHING	6882057.637	HORIZONTAL DATUM	MGA		R.L.	1.864	ISSUE	
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1 ISSUE FOR CONSTRUCTION				N.O. J.M.M. 18.10.17								MNP23-02																					
ISSUE AMENDMENT DETAILS				DRAWN CHECK DATE # USE FIGURED DIMENSIONS ONLY. DO NOT SCALE.								1																					



LEGEND - DEMOLITION

- EXISTING WATER MAIN
- EXISTING DRAINAGE PIPE
- EXISTING TELSTRA CABLE
- EXISTING TELSTRA OPTIC FIBRE CABLE
- EXISTING FENCE
- EXISTING POWER POLE
- EXISTING DRAINAGE PIT
- EXISTING ROAD TO BE EXCAVATED AND REMOVED
- EXISTING BOLLARDS TO BE REMOVED

**CONSTRUCTION
FOR CONSTRUCTION**

SCALE: A1 SHEET 1:125, A3 SHEET 1:250

DESIGN UNIT

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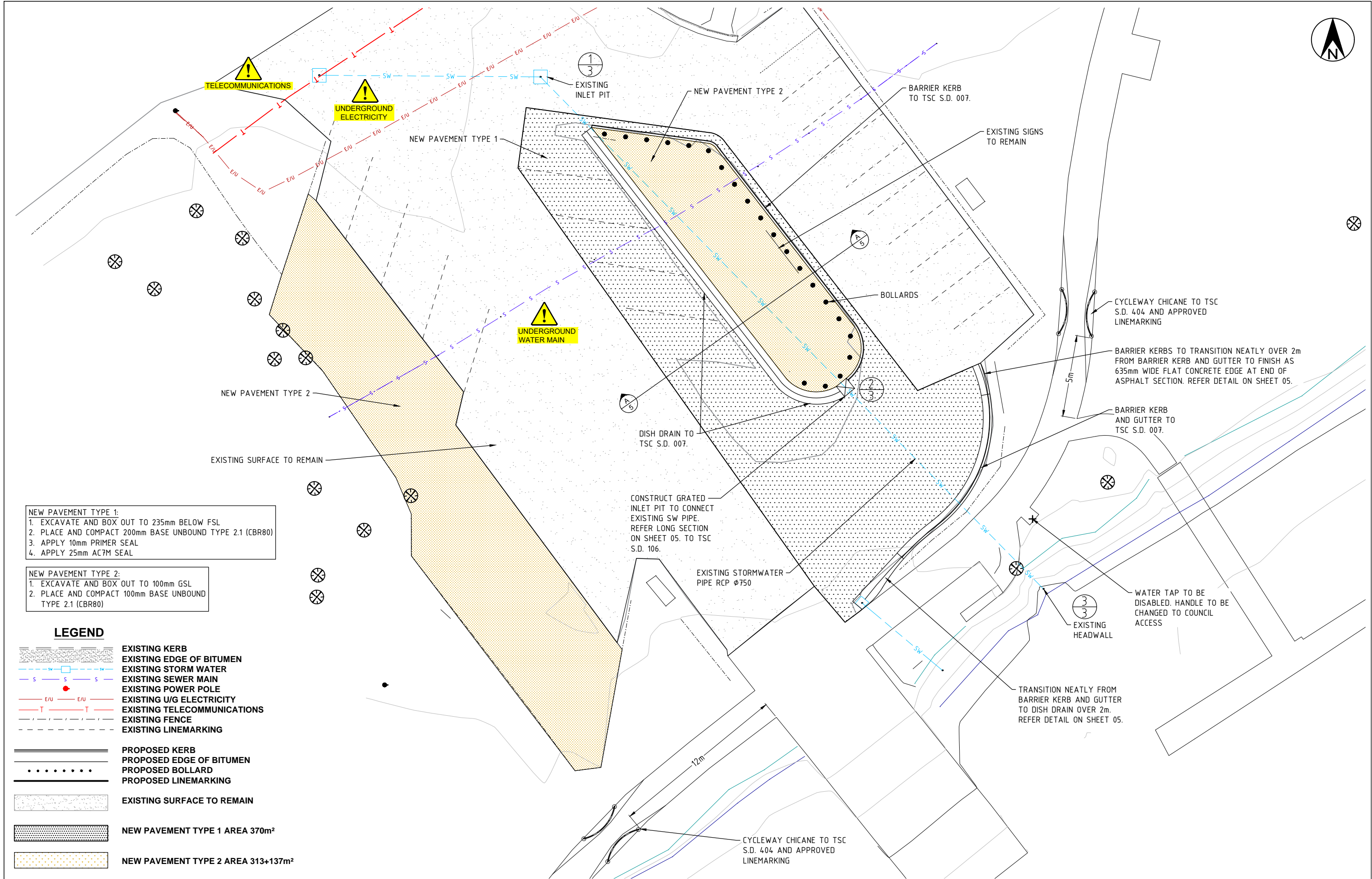
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AHD			R.L.		
			1.864		

PROJECT: **KENNEDY DRIVE BOAT RAMP
 CAR PARK UPGRADE**

PROJECT NUMBER:
MNP23

PLAN TITLE:
**EXISTING SERVICES AND DEMOLITION
 PLAN**

DRAWING NUMBER	ISSUE
MNP23-03	1



NEW PAVEMENT TYPE 1:
 1. EXCAVATE AND BOX OUT TO 235mm BELOW FSL
 2. PLACE AND COMPACT 200mm BASE UNBOUND TYPE 2.1 (CBR80)
 3. APPLY 10mm PRIMER SEAL
 4. APPLY 25mm AC7M SEAL

NEW PAVEMENT TYPE 2:
 1. EXCAVATE AND BOX OUT TO 100mm GSL
 2. PLACE AND COMPACT 100mm BASE UNBOUND TYPE 2.1 (CBR80)

LEGEND

- EXISTING KERB
- EXISTING EDGE OF BITUMEN
- EXISTING STORM WATER
- EXISTING SEWER MAIN
- EXISTING POWER POLE
- EXISTING U/G ELECTRICITY
- EXISTING TELECOMMUNICATIONS
- EXISTING FENCE
- EXISTING LINEMARKING
- PROPOSED KERB
- PROPOSED EDGE OF BITUMEN
- PROPOSED BOLLARD
- PROPOSED LINEMARKING
- EXISTING SURFACE TO REMAIN
- NEW PAVEMENT TYPE 1 AREA 370m²
- NEW PAVEMENT TYPE 2 AREA 313+137m²

CONSTRUCTION FOR CONSTRUCTION

1	ISSUE FOR CONSTRUCTION	N.O.	J.M.M.	18.10.17
ISSUE	AMENDMENT DETAILS	DRAWN	CHECK	DATE

SCALE: A1 SHEET 1:125, A3 SHEET 1:250

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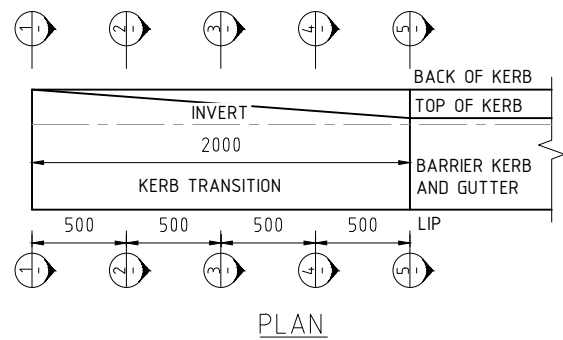
PROJECT: **KENNEDY DRIVE BOAT RAMP CAR PARK UPGRADE**

PLAN TITLE: **EXTENT OF WORKS PLAN**

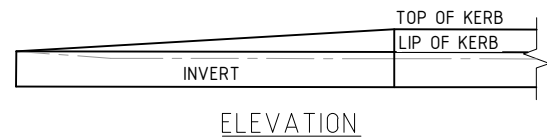
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DRAWING NUMBER: **MNP23-04**
 ISSUE: **1**

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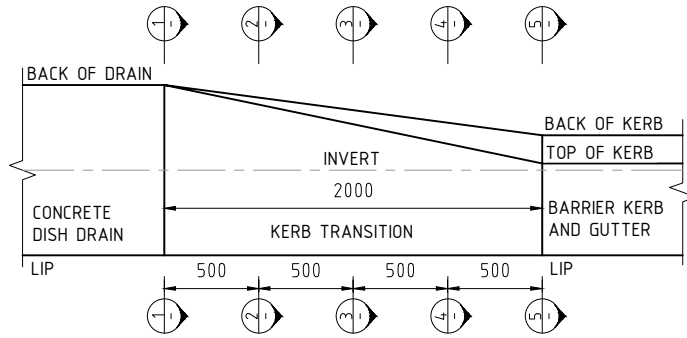


PLAN

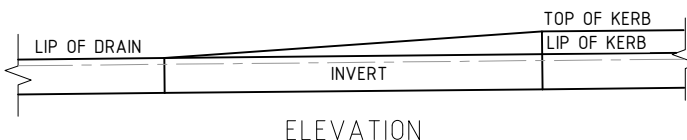


ELEVATION

KERB TO FLAT EDGE
TRANSITION DETAIL
A3 SCALE 1:40

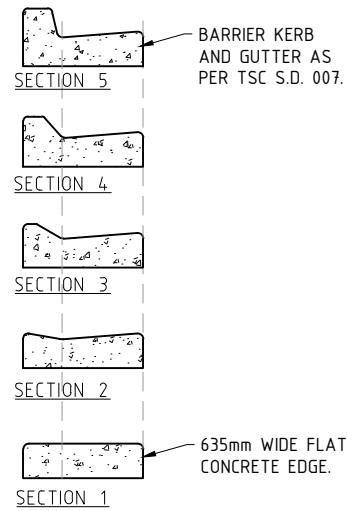


PLAN



ELEVATION

KERB TO DISH DRAIN
TRANSITION DETAIL
A3 SCALE 1:40



BARRIER KERB AND GUTTER AS PER TSC S.D. 007.

635mm WIDE FLAT CONCRETE EDGE.

BARRIER KERB AND GUTTER AS PER TSC S.D. 007.

900mm WIDE CONCRETE DISH DRAIN AS PER TSC S.D. 007.

1/3

EXISTING INLET PIT

DATUM RL. -2.0

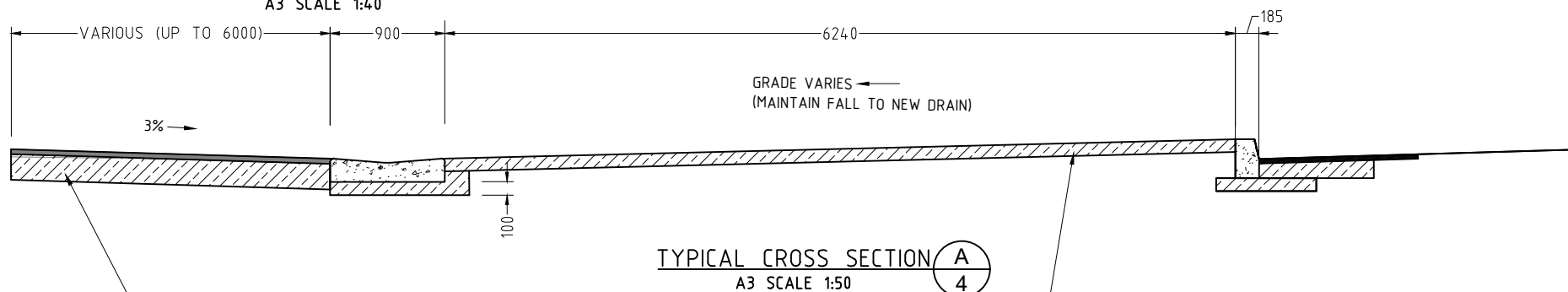
DEPTHS (m)	+1.740	+2.057	+0.362
INVERT LEVELS (m)	0.130	-0.252	-0.500
EXISTING LEVEL (m)	1.870	1.805	-0.138
CHAINAGE	0.000	31.663	52.188

2/3

PROPOSED GRATED INLET PIT
PIT SIZE: 1050X1050
ROCLA PIT V-GRATE 900X555
CONCRETE SURROUND
OR APPROVED PRODUCT

LONGITUDINAL SECTION
STORMWATER Ø750

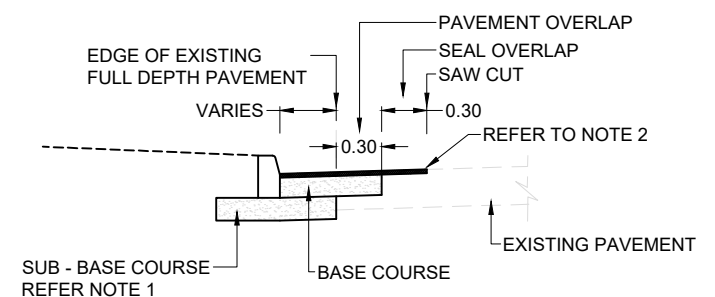
A3 SCALE: 1:25v 1:125h



TYPICAL CROSS SECTION A
A3 SCALE 1:50

- EXCAVATE AND BOX OUT TO 235mm BELOW FSL
- PLACE AND COMPACT 200mm BASE UNBOUND TYPE 2.1 (CBR80)
- APPLY 10mm PRIMER SEAL
- APPLY 25mm AC7M SEAL

- EXCAVATE AND BOX OUT TO 100mm GSL
- PLACE AND COMPACT 100mm BASE UNBOUND TYPE 2.1 (CBR80)



TYPICAL PAVEMENT
JOINT DETAIL
A3 SCALE 1:50

NOTES:

- DEPTH OF NEW PAVEMENT TO EXCEED DEPTH OF ADJACENT EXISTING PAVEMENT BY 50mm (MIN.)
- SEAL JOINT TYPES: AC TO AC - SAWCUT EXISTING AC WHERE SHOWN OR AS AGREED ON SITE TO PROVIDE A CLEAN JOINT.

CONSTRUCTION
FOR CONSTRUCTION

SCALE: A1 SHEET 1:20, A3 SHEET 1:40

SCALE: A1 SHEET 1:25, A3 SHEET 1:50

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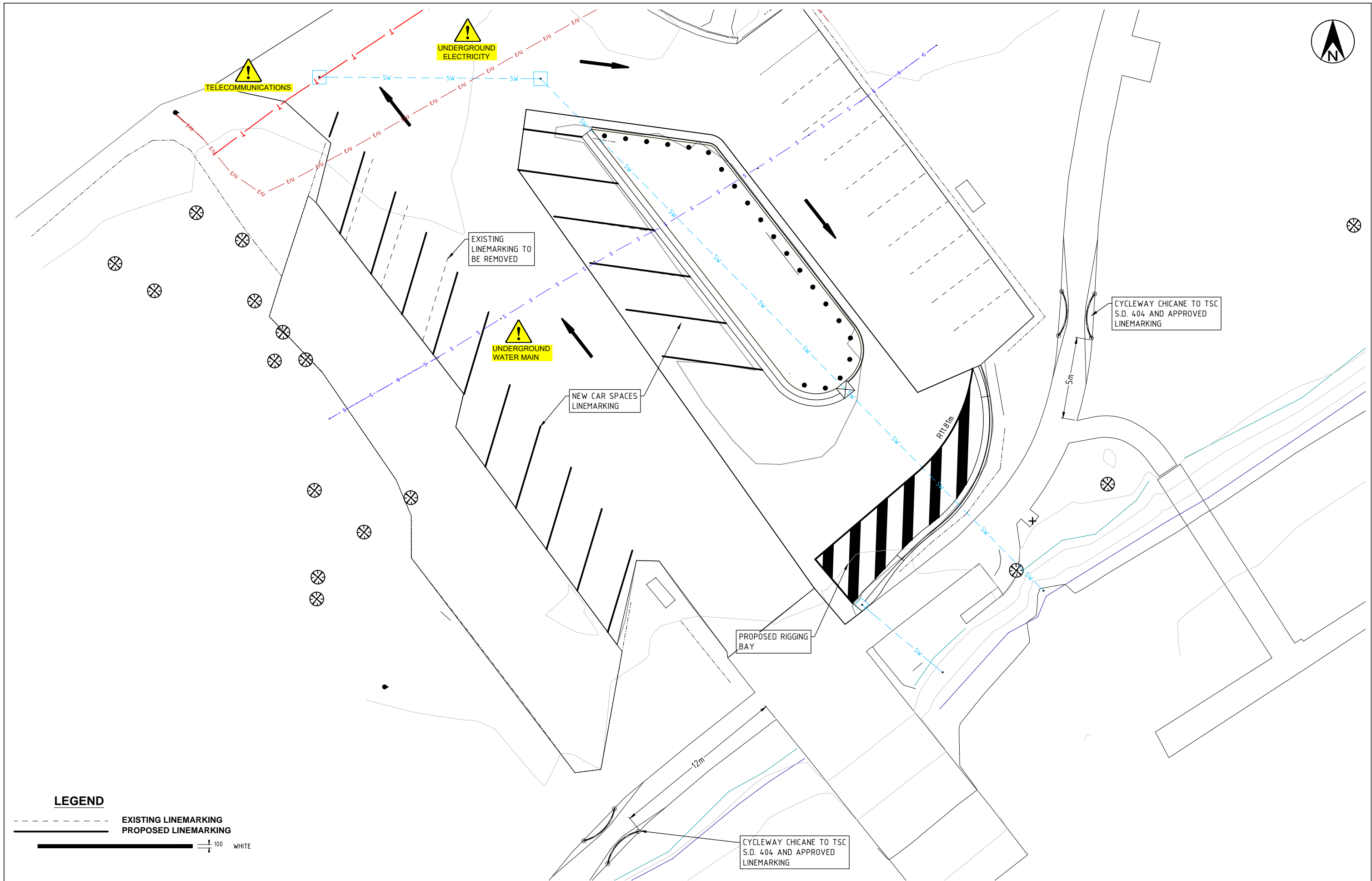
PROJECT: **KENNEDY DRIVE BOAT RAMP
CAR PARK UPGRADE**

PLAN TITLE:
**STORMWATER LONGITUDINAL SECTION &
TYPICAL SECTION**

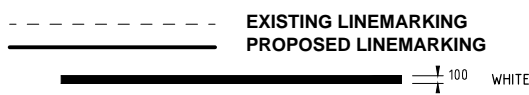
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DRAWING NUMBER
MNP23-05

ISSUE
1

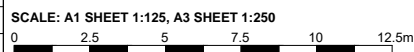


LEGEND



CONSTRUCTION FOR CONSTRUCTION

1	ISSUE FOR CONSTRUCTION	N.O.	J.M.M.	18.10.17
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VERTICAL DATUM	AHD			

PROJECT: **KENNEDY DRIVE BOAT RAMP CAR PARK UPGRADE**

PLAN TITLE: **LINEMARKING PLAN**

PROJECT NUMBER:	MNP23
DRAWING NUMBER	MNP23-06
ISSUE	1



APPROVAL
ON BEHALF OF COUNCIL
N. Cambridge
ENVIRONMENTAL SCIENTIST
DATE: 19.07.2017

- HAY BALE PIT SURROUND AS PER TSC S.D. 501
- SEDIMENT FENCE AS PER TSC S.D. 501
- FILTER SILT SOCK. SEE DETAILS DRAWING 08

CONSTRUCTION
FOR CONSTRUCTION



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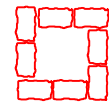
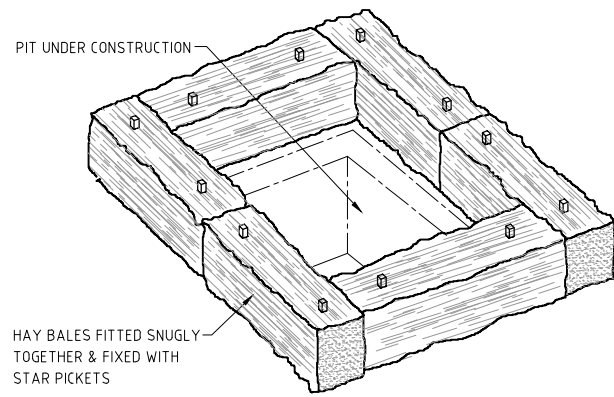
PROJECT: **KENNEDY DRIVE BOAT RAMP
CAR PARK UPGRADE**

PLAN TITLE: **EROSION AND SEDIMENT CONTROL
PLAN**

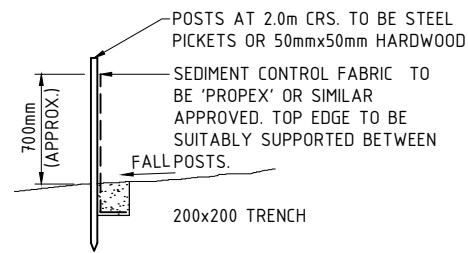
PROJECT NUMBER:
MNP23

DRAWING NUMBER
MNP23-07

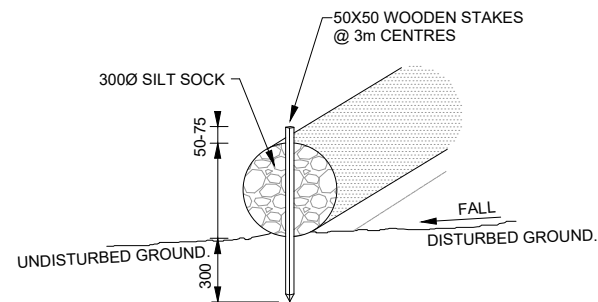
ISSUE
1



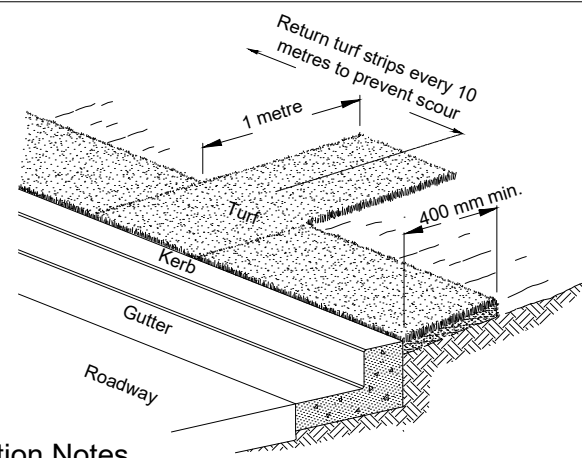
HAY BALE PIT SURROUND
NOT TO SCALE
AS PER TSC S.D. 501



TEMPORARY SEDIMENTATION CONTROL FENCE
NOT TO SCALE
SEDIMENT FENCE



FILTER SILT SOCK DETAIL
NOT TO SCALE



Construction Notes

1. Install a 400mm minimum wide roll of turf on the footpath next to the kerb and at the same level as the top of the kerb.
2. Lay 1.4 metre long turf strips normal to the kerb every 10 metres.
3. Rehabilitate disturbed soil behind the turf strip following the ESCP/SWMP.

KERBSIDE TURF STRIP SD 6-13

EROSION & SEDIMENTATION CONTROL COMMENTARY

1. MONITOR 7 DAY RAIN FORECAST TO DETERMINE TIMING OF WORK.
2. AVOID WORK IN WET WEATHER, ESPECIALLY WITHIN THE ROAD SURFACE.
3. LIMIT AREAS OF DISTURBANCE & MAINTAIN GRASSED AREAS WHERE POSSIBLE. ENSURE GUTTERS, PATHWAYS & ROADS ARE SWEEP CLEAN PRIOR TO RAIN OR AT THE END OF SHIFT. HARD SURFACES CLEAN OF SOIL WILL REDUCE EROSION & SEDIMENTATION CONTROLS & THEREFORE TRIP HAZARDS TO PEDESTRIANS & ROAD HAZARDS ETC.
4. ENSURE THAT TURF IS REPLACED AS SOON AS POSSIBLE AFTER WORKS TO AID IN SOIL STABILISATION.
5. REMOVE ESC MEASURES WHEN SITE IS CONSIDERED STABILISED E.G. ESTABLISHED TURF ON EXCAVATED AREAS, REPLACE PAVEMENT ETC.
6. ARRANGE REGULAR INSPECTIONS TO REVIEW & UPDATE CONTROL MEASURES.

EROSION AND SEDIMENT CONTROL PLANS

PROGRESSIVE *ERSED* PLANS SHALL BE DEVELOPED AND IMPLEMENTED AS REQUIRED BY THE SITE SUPERVISOR BASED ON THIS PLAN AND FOLLOWING PRINCIPLES AND STANDARD SITE CONTROL MEASURES.

- MINIMISE EXTENT AND DURATION OF DISTURBANCE
- CONSTRUCTION WORKS TO BE MANAGED SUCH THAT AREAS OUTSIDE SCOPE OF WORKS REMAIN UNDISTURBED WHERE POSSIBLE.
 - MINIMISE EXTENT OF DISTURBANCE WITHIN CONSTRUCTION SITE AT ANY ONE TIME BY STAGING THE WORKS (E.G. RIP EXISTING BITUMEN IN SECTIONS, MOVING ON TO NEW SECTIONS FOLLOWING COMPLETION OF PREVIOUS STAGE).
 - MINIMISE DISTURBANCE OF VEGETATION ALONG THE ROAD VERGE WITH SPECIAL EMPHASIS ON MANAGEMENT OF CONSTRUCTION ACTIVITIES ADJACENT TO WATERCOURSES (E.G. MAINTAIN GRASSY BUFFER WHERE POSSIBLE).

- CONTROL STORMWATER FLOWS ONTO, THROUGH AND FROM THE SITE
- SEPARATE 'CLEAN' RUN-ON WATER FROM 'DIRTY' (E.G. TURBID) CONSTRUCTION AREA RUNOFF (MAINTAIN CLEAN WATER PASSAGE THROUGH CULVERT CROSSING THROUGHOUT CONSTRUCTION WORKS).
 - CONSTRUCT PERMANENT DRAINAGE STRUCTURES EARLY IN THE PROJECT.

- USE EROSION CONTROL MEASURES TO PREVENT ON-SITE DAMAGE
- THE INSTALLATION OF ALL EROSION AND SEDIMENT CONTROLS TO OCCUR IMMEDIATELY POST CLEARING AND STRIPPING.
 - SITE STOCKPILES OF SOIL MATERIAL IN LOW-HAZARD AREAS CLEAR OF WATERCOURSES. ADDITIONAL PROTECTION TO BE AFFORDED WITH TEMPORARY VEGETATION, DIVERSION BANKS AND SEDIMENT CONTROL MEASURES, IF REQUIRED. SEED STOCKPILES WITH ANNUAL GRASS IF THEY ARE TO BE STORED LONGER THAN 10 DAYS.
 - CONSTRUCT A RANGE OF EROSION CONTROLS WITHIN THE VARIOUS ROAD SUB-CATCHMENTS TO COMPLEMENT AND INCREASE THE EFFECTIVENESS AND EFFICIENCY OF SEDIMENT CONTROLS IN THE LOWER AREAS.

- USE SEDIMENT CONTROL MEASURES TO PREVENT OFF-SITE DAMAGE
- THE INSTALLATION OF ALL EROSION AND SEDIMENT CONTROLS TO OCCUR IMMEDIATELY POST CLEARING AND STRIPPING.
 - CONSTRUCT CONTROL MEASURES AS CLOSE TO THE POTENTIAL SOURCE OF SEDIMENT AS POSSIBLE.
 - CONTROL THE DEPOSITION OF MUD AND SOIL MATERIAL ONTO LOCAL ROADS.

- STABILISE DISTURBED AREAS QUICKLY
- ALL REINSTATEMENT WORKS ADJACENT TO NEW CONSTRUCTION SHALL BE CARRIED OUT AS SOON AS POSSIBLE AFTER COMPLETION OF CONSTRUCTION WORKS.
 - ALL DISTURBED VERGES TO BE STABILISED BY REVEGETATING WITH APPROPRIATE SPECIES (E.G. ANNUAL GRASS SEED SUCH AS ANNUAL RYEGRASS OR JAPANESE MILLET, OR TURF) AS SOON AS PRACTICAL AFTER REINSTATEMENT.
 - ENSURE THE SUCCESS OF THE LATER REVEGETATION PROGRAM BY UTILISING A GOOD TOPSOIL MANAGEMENT PROGRAM.

- INSPECT AND MAINTAIN CONTROL MEASURES
- ENSURE THE PROGRESSIVE AND CONTINUAL IMPLEMENTATION AND MAINTENANCE OF TEMPORARY EROSION AND SEDIMENT CONTROLS.
 - INITIATE A PROGRAM TO ENSURE REGULAR MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL MEASURES. SEDIMENT CLEANED FROM STRUCTURES (E.G. SCRAPE AWAY ACCUMULATED SEDIMENT UPSTREAM OF CHECK DAMS AND REPLACE/REPAIR AS NECESSARY) TO MAINTAIN FUNCTIONALITY.
 - ARRANGE REGULAR INSPECTIONS BY AN ENVIRONMENTAL SCIENTIST TO REVIEW AND UPDATE CONTROL MEASURES. ADDITIONAL INSPECTIONS WILL BE CONDUCTED DURING AND/OR IMMEDIATELY FOLLOWING SIGNIFICANT RAINFALL EVENTS TO MONITOR THE FUNCTIONING OF CONTROLS.
 - ALL EROSION AND SEDIMENT CONTROLS TO BE MAINTAINED IN PLACE UNTIL ALL WORKS ARE COMPLETED AND DISTURBED AREAS HAVE STABILISED.

EXTRACT FROM LANDCOM (2004). MANAGING URBAN STORMWATER: SOIL AND CONSTRUCTION. VOLUME 2D MAIN ROADS CONSTRUCTION. 2008

This sediment and erosion control plan contains Council's minimum requirements for environmental protection; however, it is still the principal contractor's responsibility to ensure that the works and mitigation strategies are performed in a manner that complies with all relevant environmental legislation, including any development approval requirements.



CONSTRUCTION FOR CONSTRUCTION



DESIGN UNIT

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DESIGNED	N.O	19.07.17	COORDS ADOPTED	PM41250
DRAWN	N.O	19.07.17	EASTING	552145.608
CHECKED	J.M.M.	19.07.17	NORTHING	6882057.637
HORIZONTAL DATUM	MGA		R.L.	1.864
VERTICAL DATUM	AHD			

PROJECT: **KENNEDY DRIVE BOAT RAMP CAR PARK UPGRADE**

PLAN TITLE: **EROSION AND SEDIMENT CONTROL NOTES**

PROJECT NUMBER: **MNP23**

DRAWING NUMBER: **MNP23-08**
ISSUE: **1**